



Planning and Highways Committee

Date: Thursday, 6 July 2023

Time: 2.00 pm

Venue: Council Chamber, Level 2, Town Hall Extension

Everyone is welcome to attend this committee meeting.

Access to the Council Chamber

Public access to the Council Chamber is on Level 2 of the Town Hall Extension, using the lift or stairs in the lobby of the Mount Street entrance to the Extension.

There is no public access from any other entrance of the Extension.

Filming and broadcast of the meeting

Meetings of the Planning and Highways Committee are 'webcast'. These meetings are filmed and broadcast live on the Internet. If you attend this meeting you should be aware that you might be filmed and included in that transmission.

Membership of the Planning and Highways Committee

Councillors

Lyons (Chair), Shaukat Ali, Andrews, Chohan, Curley, Davies, Gartside, Hassan, Hewitson, Hughes, Johnson, Kamal, J Lovecy, Ludford and Riasat

Agenda

- 1. Urgent Business**
To consider any items which the Chair has agreed to have submitted as urgent.
- 1a. Supplementary Information on Applications Being Considered**
The report of the Director of Planning, Building Control and Licencing will follow.
- 2. Appeals**
To consider any appeals from the public against refusal to allow inspection of background documents and/or the inclusion of items in the confidential part of the agenda.
- 3. Interests**
To allow Members an opportunity to [a] declare any personal, prejudicial or disclosable pecuniary interests they might have in any items which appear on this agenda; and [b] record any items from which they are precluded from voting as a result of Council Tax/Council rent arrears; [c] the existence and nature of party whipping arrangements in respect of any item to be considered at this meeting. Members with a personal interest should declare that at the start of the item under consideration. If Members also have a prejudicial or disclosable pecuniary interest they must withdraw from the meeting during the consideration of the item.
- 4. Minutes**
To approve as a correct record the minutes of the meeting held on 1 June 2023. 7 - 22
- 5. 135028/FO/2022 & 135029/LO/2022 - Land Bound By King Street West, St Marys Parsonage, Garden Lane And Smithy Lane Manchester M3 2JP - Deansgate Ward** 23 - 114
The report of the Director of Planning, Building Control and Licensing is enclosed.
- 6. 135544/FO/2022 & 135545/LO/2022 - 466 - 472 Moss Lane East Manchester M14 4PJ - Moss Side Ward** 115 - 176
The report of the Director of Planning, Building Control and Licensing is enclosed.
- 7. 135848/FO/2022 - Site Of Former Victoria Park Probation Centre Laindon Road Manchester Rusholme Ward** 177 - 234
The report of the Director of Planning, Building Control and Licensing is enclosed.
- 8. 136541/FH/2023 - 35 Whalley Avenue, Chorlton, Manchester, M21 8TU - Chorlton Park Ward** 235 - 254
The report of the Director of Planning, Building Control and

Licensing is enclosed.

- 9. 134707/FH/2022 - 1D Cotton Lane, Manchester, M20 4GL - Withington Ward** 255 - 272

The report of the Director of Planning, Building Control and Licensing is enclosed.
- 10. 136171/FO/2023 - Land Bounded By Brigham Street, Meech Street And The Rear Of Connie Street, Manchester - Clayton & Openshaw Ward** 273 - 310

The report of the Director of Planning, Building Control and Licensing is enclosed.

Meeting Procedure

The meeting (and any site visits arising from the meeting) will be conducted in accordance with the relevant provisions of the Council's Constitution, including Part 6 - Section B "Planning Protocol for Members". A copy of the Constitution is available from the Council's website at <https://democracy.manchester.gov.uk/ecCatDisplay.aspx?sch=doc&cat=13279>

At the beginning of the meeting the Chair will state if there any applications which the Chair is proposing should not be considered. This may be in response to a request by the applicant for the application to be deferred, or from officers wishing to have further discussions, or requests for a site visit. The Committee will decide whether to agree to the deferral. If deferred, an application will not be considered any further.

The Chair will explain to members of the public how the meeting will be conducted, as follows:

1. The Planning Officer will advise the meeting of any late representations that have been received since the report was written.
2. The officer will state at this stage if the recommendation of the Head of Planning in the printed report has changed.
3. ONE objector will be allowed to speak for up to 4 minutes. If a number of objectors wish to make representations on the same item, the Chair will invite them to nominate a spokesperson.
4. The Applicant, Agent or their representative will be allowed to speak for up to 4 minutes.
5. Members of the Council not on the Planning and Highways Committee will be able to speak.
6. Members of the Planning and Highways Committee will be able to question the planning officer and respond to issues that have been raised. The representative of the Highways Services or the City Solicitor as appropriate may also respond to comments made.

Only members of the Planning and Highways Committee may ask questions relevant to the application of the officers. All other interested parties make statements only. The Committee having heard all the contributions will determine the application. The Committee's decision will in most cases be taken under delegated powers and will therefore be a final decision.

If the Committee decides it is minded to refuse an application, they must request the Head of Planning to consider its reasons for refusal and report back to the next meeting as to whether there were relevant planning considerations that could reasonably sustain a decision to be minded to refuse.

Information about the Committee

The Council has delegated to the Planning and Highways Committee authority to determine planning applications, however, in exceptional circumstances the Committee may decide not to exercise its delegation in relation to a specific application but to make recommendations to the full Council.

It is the Council's policy to consult people as fully as possible before making decisions that affect them. Members of the public do not have a right to speak at meetings but the Committee will usually allow applicants and objectors to address them for up to four minutes. If you have a special interest in an item on the agenda and want to speak, tell the Committee Officer, who will pass on your request to the Chair. Groups of people will usually be asked to nominate a spokesperson.

The Council is concerned to ensure that its meetings are as open as possible and confidential business is kept to the strict minimum. When confidential items are involved these are considered at the end of the meeting at which point members of the public are asked to leave.

Late representations will be summarised and provided in a Supplementary Information Report. Such material must be received before **noon on the Tuesday** before the meeting. Material received after this time will not be reported to the Committee, this includes new issues not previously raised during the formal consultation period. Only matters deemed to be of a highly significant legal or technical nature after consultation with the City Solicitor will be considered.

Material must not be distributed to Planning Committee Councillors by members of the public (including public speakers) or by other Councillors during the meeting. The distribution of such material should be in advance of the meeting through the Planning Service as noted above.

Joanne Roney OBE
Chief Executive
Level 3, Town Hall Extension,
Albert Square,
Manchester, M60 2LA

Further Information

For help, advice and information about this meeting please contact the Committee Officer:
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This agenda was issued on **Wednesday, 28 June 2023** by the Governance and Scrutiny Support Unit, Manchester City Council, Level 2, Town Hall Extension (Library Walk Elevation), Manchester M60 2LA

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Planning and Highways Committee

Minutes of the meeting held on Thursday, 1 June 2023

Present: Councillor Lyons (Chair)

Councillors: Shaukat Ali, Andrews, Curley, Davies, Gartside, Hassan, Hewitson, Hughes, Kamal, J Lovecy and Riasat

Apologies: Councillor Chohan, Johnson and Ludford

Also present: Councillor Good (Ward Councillor Ancoats and Beswick) – application 133324/FO/2022 & 133323/LO/2022 only

PH/23/33. Supplementary Information on Applications Being Considered

A copy of the late representations received had been circulated in advance of the meeting regarding applications 135419/FO/2022, 133324/FO/2022 and 133323/LO/2022, 135419/FO/2022, 136551/FO/2023, 135647/FO/2022 and 135936/FO/2023

Decision

To receive and note the late representations.

PH/23/34. Minutes

Decision

To approve the minutes of the meeting held on 13 April 2023 as a correct record.

PH/23/35. 135662/FO/2022 - Laystall Street / Great Ancoats Street Manchester M4 6DE - Piccadilly Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that presented an application relating to the erection of a 20 storey building to create a 154 bedroom hotel (Class C1) above 2 basement levels with ancillary café / bar / restaurant and gym and other associated works including highway improvements, cycle parking and creation of accessible parking bay following removal of on site structures.

Seven letters of objection had been received (including three from the same party) and one anonymous letter. The grounds of objections were concerning the design, traffic impacts of reconfiguring the Laystall Street junction, inadequate pre-application consultation and the prejudicial impact of developing this site in isolation of the adjoining site.

The Planning Officer did not have anything to add to the report or late representations received.

The applicant's agent addressed the Committee stating the design of the building proposed made efficient use of the site whilst not compromising any development on adjacent land. The applicant had an excellent track record and reputation for delivering and operating hotel development across the UK. Proposals were designed to deliver a high quality building, developed in close consultation with Council officers. The proposals had been subject to rigorous townscape and heritage assessments and would meet highest of sustainable construction standards and would reduce the demand for alternative form of visitor accommodation in the city.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Hewitson addressed the Committee and sought clarity on the proposed amendment to the road layout and direction exiting Laystall Street. In connection to this Councillor Davies sought clarity as to whether the proposed change had come from the Council's Highways department as part of a wider programme of changes to road layouts or whether any consultation with local residents had taken place. Councillor S Ali also expressed his concern in relation to the proposed traffic remodelling.

The Planning Officer confirmed that at present traffic exiting Laystall Street could turn left or right. Within the proposals submitted, traffic would only be able to turn left. This proposal had been subjected to traffic modelling and it had been determined that this proposal would have no adverse effect on traffic and would improve the environment for pedestrians around the site. In addition, it was confirmed that the proposed change had been submitted by the applicant and discussed and whist agreed by the Council's Highways Department. the proposal would still need to a formal Section 278 agreement and if it did not pass, alternative proposals would need to be considered, however, this should not affect the application going forward.

Councillor Andrews sought clarity on whether the application would need to be reconsidered by the Committee should the Section 278 agreement not be passed.

The Planning Officer advised that if the Section 278 Agreement was not passed, the application could still go forward subject to a minor modification to the application in relation to the proposed traffic modelling.

Councillor Curley enquired as to whether there was any possibility of increasing the number of proposed disabled parking bays.

The Planning Officer advised that in addition to the proposed disabled bay, the applicant would also be providing a valet parking service a spart of the operational management plan.

Councillor Andrews proposed a motion to approve the application.

Councillor Hughes seconded Councillor Andrews's proposal.

Decision

The Committee Approves the application as set out in the report submitted.

PH/23/36. 135675/FO/2022 - Tariff Street Manchester - Piccadilly Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that presented an application regarding the erection of two residential apartment buildings (Use Class C3) comprising Block 1 -part 9, part 10 and Block 2-12 storey building (comprising of 261 dwellings in total), with ground floor commercial units (Use Class E), associated residents amenity space, cycle parking, landscaping, access, street loading and other associated works following demolition of the existing building on site.

30 letters had been received from three rounds of neighbour notification from a total 24 objectors. The objections related to design, heritage, amenity, servicing, sunlight and daylight, wind impacts on external spaces, highways and non-compliance with the Piccadilly Basin SRF.

The Planning Officer did not have anything to add to the report.

An objector attended and addressed the Committee on the application, raising concerns fire safety, specifically in relation to the proposed Block Two, which proposed only one staircase which was non-compliant. Concerns were also made around the wind report, that the application deviated from the SRF, no consultation had been given to local heritage assets and removable of public realms, loss of day light to neighbouring residential properties and overdevelopment of the site

The applicant's agent attended and addressed the Committee, stating that the proposal before committee represented positive discussions with Planning Officers and was in line with key principles within the Piccadilly Basin SRF. The proposals met and exceeded design standards and the proposed scale and massing responded to the historic mills and would deliver well designed accommodation that would be sympathetic to the area. The proposal was consistent with the strategic vision for the area and there would be an initial £250k contribution to affordable housing with a further viability assessment secured to allow this to be reassessed . It was stated that the current site made little contribution to the heritage of the area and the proposal would contribute to the delivery of new homes in the city.

The Planning Officer provided clarification on the issues raised by the objector. Specifically in relation to fire safety, he advised the Committee that this was not an issue for the planning process. It was for the Committee to determine on land use planning issues. It would be for Building Control to determine on fire safety and if changes were needed, this would result in a new application which could be in the form of a non material change, material change or new application, which may need to be subjected to consideration by the Committee again.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Curley addressed the Committee, welcoming the securing of the Section 106 agreement toward affordable housing. He proposed a motion to approve the officer's recommendation of Minded to Approve subject to the signing of a section 106 agreement in relation to an initial off site affordable housing contribution, with a future review of the affordable housing position

Councillor S Ali seconded Councillor Curley's proposal.

Decision

The Committee is Minded to Approve the application subject to the signing of a Section 106 agreement in relation to an initial off site affordable housing contribution, with a future review of the affordable housing position.

PH/23/37. 133324/FO/2022 & 133323/LO/2022 - Ancoats Works Pollard Street Norfolk Street Manchester M4 7DS - Ancoats & Beswick Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that proposed the erection of two, part 8, part 4 storey buildings and refurbishment of the southern part of the Ancoats Works building to Pollard Street to form 183 residential apartments and 10 duplex apartments (Use Class C3a) together with flexible commercial space (Use Class E/Sui Generis) (274 sqm) with associated landscaping, car and cycle parking and associated works following demolition and partial demolition of existing buildings.

Listed building consent was also sought for removal of an existing roof structure between Hope Mill and Ancoats Works, the replacement of existing gates fronting Pollard Street, and associated works in connection with the residential led development of Ancoats Works.

Nine letters of objection, and one letter of support had been received from surrounding residents and businesses within Hope Mill. The objections related to, but were not restricted to, a lack of parking, loss of daylight to local businesses, scale and massing, loss of heritage assets and a lack of S106 contribution.

The Planning Officer did not have anything to add to the report and late representations received.

An objector attended and addressed the Committee on the application, raising concerns in relation to the size of the development and the impact it would have on the local community. It was stated that the application would remove a local historic landmark and the proposed development had non-descript features. Concern was also raised in relation to size of the development and associated loss of daylight to existing residents and the impact the development would have on the local infrastructure, including increased traffic that the proposed development would have.

The applicant's agent attended and addressed the Committee, advising that the proposals would be respectful of nearby listed buildings to ensure heritage assets in the area remained dominant. It was stated that the proposed development met and exceeded design standards and would result in £35m investment into the local

economy. Significant mitigation would be undertaken to protect existing commercial businesses that neighboured the site and extensive landscaping would also take place, proving attractive, safe communal areas for residents

Councillor Good (Ward Councillor Ancoats and Beswick) attended and addressed the Committee. He raised concerns about the lack of affordable housing in the development. The development proposed 193 units with non being affordable, which did not accord with the Council's policy around affordable housing

Further, he raised concerns that there was no proposed parking provision and he also felt that the sustainable transport element was not sufficient as there was little connected cycle infrastructure to the development.

He requested that the Committee rejected the planning application in its current form. He stated that to meet Council policy the application should at a minimum provide 20% affordable housing units, or the applicant contributed made an equivalent financial contribution (20%) for off-site affordable housing.

The Planning Officer provided clarification on the issues raised by the objector. He stated that the application was not a large development compared to surrounding developments and that the area needed to change as the impact of growth of the city centre continued to move outwards. He advised that the site was unappealing in its current form and contributed little to the area. In relation to affordable housing, he assured the Committee that the Council rigorously tested the viability assessments to all housing development proposals. The profit margin for the development was 17.5% and regardless of what this equated to in monetary terms, Government had set a minimum profit margin of 20% on site, therefore the Council wasn't able to secure a Section 106 Agreement that gave a financial contribution upfront. There would however, a clawback mechanism put in place.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Lovecy addressed the Committee and sought clarity as to whether the conditions attached do the application would ensure that all of the properties would be effective against becoming AirB&B type usage. She also sought confirmation as to who would have access to the new proposed public realm and what steps were being taken in relation to acoustic and noise mitigation

The Planning Officer advised that the conditions attached to the application would protect against the properties being used as AirB&B. He confirmed that the proposed public realm would be for residents only and acknowledged that the issue around acoustics had been challenging and work had been undertaken to ensure those neighbouring businesses could still operate

Councillor Curley enquired as to whether there was any opportunity for additional disabled parking provision and what mechanism was being used to exclude residents in this develop from having to apply and purchase parking permits from the existing scheme.

The Planning Officer advised that a condition could be included to review additional disabled parking if the Committee was minded to agree this. He added that Officers were working with the City Solicitor to identify a mechanism that would exclude residents in this development from applying for a parking permit. This could not be achieved through a Section 106 Agreement but possible a Section 111 Agreement.

Councillor Andrews proposed a motion to approve the officer's recommendation of Minded to Approve subject to the completion of the legal agreement associated with planning application 133324/FO/2022 and the inclusion of a condition to review additional disabled parking provision.

Councillor Curley seconded Councillor Andrew's proposal.

Decision

The Committee is Minded to Approve the application subject to the completion of the legal agreement associated with planning application 133324/FO/2022 and the inclusion of a condition to review additional disabled parking provision.

PH/23/38. 135419/FO/2022 - One Medlock Street Manchester M15 5FJ - Deansgate Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the demolition of the existing hotel building and structures and redevelopment of the site to comprise two separate buildings: one 13 storey office building with commercial unit (Use Class E) at ground floor; a part 11, part 38 storey building comprising 1,014 purpose built student accommodation units (sui generis) with ground floor office/community uses (Use Class E, F1 or F2); and associated ancillary internal and external amenity space, hard and soft landscaping and associated highway works.

There had been 11 representations received objecting to the proposed development. The objections related to, but were not restricted to, increased noise and disturbance, scale and massing, over-development, loss of daylight, lack of suitable infrastructure and loss of privacy.

The Planning Officer did not have anything to add to the report and late representations received.

The applicant's agent attended and addressed the Committee. He stated that the site occupied a key location to the southern gateway of the city centre. The application supported the positive change of the wider area as part of the First Street Regeneration Framework. The proposals had been developed through local engagement and working with local teams. The proposed design would provide an improved street level experience, which would be greener and work better for pedestrians and cyclists. The office building proposed would provide over 2200 jobs and there would also be a community hub available for all of the community. The application would also provide high quality purpose built student accommodation for approximately 1000 students. Positive conversation had taken place with

Universities who supported the proposals and would form part of the PBSA pipeline identified by the Council

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Lovecy addressed the Committee and sought clarification as to how the affordable low market rent level was set in relation to the proposed student accommodation

The Planning Officer advised that there was no Council policy position for affordable student accommodation but this would be picked up as part of the review of the Council's Core Strategy. It was reported that 20% of the proposed student accommodation would be at 80% of the market rate with equal access to all facilities.

Councillor Curley commented on responses received from Sport England and use of facilities and asked if any provision could be made to address these.

The Planning Officer advised that there was no policy position that required the Council to address the comments received from Sport England.

Councillor Davies welcomed the proposed landscaping and sought clarification as to whether appropriate traffic modelling had been undertaken in connection to safe cycling provision in the area. She also asked if consideration had been given to the potential increase in traffic arising from the use of Uber and online food delivery companies that could be attributed to student accommodation

The Planning Officer confirmed that the Council was looking at an Active Travel Scheme along the whole length of Medlock Street but this was not yet funded. The proposed development would help make a significant improvement to the local environment in terms of tree planting, the widening of pavements and better use of the site. In addition he advised that travel plans had been updated to account for the potential increase use of ridesharing companies and online food delivery companies

Councillor Davies requested the Committee be provided with a note on how travel plans had been updated to account for the potential increase use of ridesharing companies and online food delivery companies

Councillor S Ali proposed a motion to approve the officer's recommendation of Minded to Approve subject to a legal agreement for the provision of on-site affordable accommodation, waste management to be provided by a private contractor and a financial contribution towards off site tree planting.

Councillor Kamal seconded Councillor S Ali's proposal.

Decisions

The Committee:-

- (1) Is Minded to Approve the application subject to a legal agreement for the provision of on-site affordable accommodation, waste management to be provided by a private contractor and a financial contribution towards off site tree planting.
- (2) Requests a note on how travel plans had been updated to account for the potential increase use of ridesharing companies and online food delivery companies

PH/23/39. 136170/FO/2023 - Land Bounded By River Street To The North, River Street And Vacant Lane To The East, Hulme Street to The South And Plot 10A Of The First Street Masterplan To The West Manchester - Deansgate Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the erection of a 14-storey building comprising of purpose-built student accommodation (PBSA) (Sui Generis) and ground floor Food Hall (Sui Generis Use), and other associated works including external amenity spaces, public realm, secure cycle parking, access and servicing arrangements (Plot 10B).

No objections had been received.

The Planning Officer did not have anything to add to the report.

The applicant's agent attended and addressed the Committee. He stated that the proposed development had received no objections from local residents, statutory or non-statutory consultees. The development would provide high quality student accommodation to meet the demand in the area from students. It also aligned to the Council's pipeline of further PBSA and would help draw students out of main stream homes, freeing up these properties and reduce rent pressure for the city's residents. The development would also offer 15% of the total accommodation at an affordable rate. The development was also significantly lower in height than that envisaged in the SRF. Designed wise the development would successfully transition from the modern developments of First Street to the traditional mill buildings of Macintosh Village.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Davies addressed the Committee. She welcomed that the proposed development would be sympathetic to the surrounding area. She sought clarification that the proposed 15% of accommodation being at an affordable rate would be for the perpetuity of the development and asked what impact the development would have on traffic in relation to the potential increase in the use of ridesharing companies and online food delivery companies.

The Planning Officer confirmed that the proposed 15% of accommodation at an affordable rate would be required through a Section 106 Agreement and would last for the perpetuity of the development. He also agreed to provide information on how

travel plans had been updated to account for the potential increase use of ridesharing companies and online food delivery companies.

Councillor Kamal proposed a motion to approve the officer's recommendation of Minded to Approve subject to a Section 106 to secure affordable student housing and commercial waste disposal.

Councillor Hewitson seconded Councillor Kamal's proposal.

Decision

The Committee is Minded to Approve subject to a S106 to secure affordable student housing and commercial waste disposal.

PH/23/40. 135834/FO/2022 - Albert Bridge House Bridge Street Manchester M3 5AH - Deansgate Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the creation of a mixed use development comprising two separate components in the form of an office building of up to 19 storeys with ground floor commercial, leisure, food and drink uses (All Use Class E (g)) and/ or drinking establishment (Sui Generis), and, a residential building up to 45 storeys (Use Class C3a) with additional roof top plant, basement car parking, cycle parking, landscaping and public realm, servicing and access arrangements, highway alterations and other associated works following demolition of the existing building complex.

Seven letters of objection and one neutral comment had been received. The objections related to, but were not restricted to, loss of daylight and overbearing, traffic congestion,

The Planning Officer did not have anything to add to the report.

The applicant's agent attended and addressed the Committee. He stated that the design of the development offered a welcoming and thriving new city centre destinations. The proposed development aligned with the Council's Parsonage Gardens SRF which identified Albert Bridge House as significant redevelopment opportunity for high density commercially led mixed use accommodation. The development would provide approximately 3000 full time jobs and had been designed to deliver best in class, inclusive employment space. The proposed residential accommodation would meet the Home Quality mark standards and the scale and massing of the buildings had been informed by the SRF with consideration to local heritage assets.

The Chair invited the Committee to make comments or ask questions to the Planning Officer. The development would support the ongoing economic regeneration of the area and form a key part of the city's blue and green infrastructure, providing a 20% biodiversity net gain

Councillor Davies addressed the Committee. She welcomed the success of the Tree Preservation Orders but raised concern in relation to the percentage of parking spaces proposed. She sought clarification as to whether there had been a decision as to whether all the proposed residential accommodation would be for rent or would some be for purchase and whether the proposed ground floor independent retail proposals could be guaranteed as these types of businesses could not often commit to long term leases.

The Planning Officer clarified that there were 12 accessible spaces overall, but if Committee was minded, a condition could be included to review this provision. It was confirmed that all of the proposed residential accommodation would either be for rent or purchase and insofar as the ground floor retail proposals, the applicant would be offering a profit rent or turnover rent to ensure an independent business occupied the space. If not already within the conditions, the Planning Officer proposed a suitable condition could be included in the application

Councillor S Ali proposed a motion to approve the officer's recommendation of Minded to Approve subject to the signing of a section 106 agreement in relation to a future review of the affordable housing position, to secure monies associated with highway improvement works along Bridge Street and secure the retention of the project architect and the inclusion of conditions to review the overall provision of residential parking spaces and the rental arrangements for the proposed independent ground floor retail offering.

Councillor Hewitson seconded Councillor S Ali's proposal.

Decision

The Committee is Minded to Approve the application, subject to the signing of a Section 106 agreement in relation to a future review of the affordable housing position, to secure monies associated with highway improvement works along Bridge Street and secure the retention of the project architect and the inclusion of conditions to review the overall provision of residential parking spaces and the rental arrangements for the proposed independent ground floor retail offering.

PH/23/41. 136551/FO/2023 - 393 Wilmslow Road Manchester M20 4WA - Withington Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the retention of use of former Hotel (C1) as Temporary Living Accommodation for Single Homeless People (Sui Generis). The applicant proposed to retain the use of the property as short term residential accommodation providing 30 en-suite rooms to single homeless people.

Objections had been received from 24 local residents, Fallowfield Community Guardians and South East Fallowfield Residents Group.

Councillors Wills, Gartside and Chambers had indicated their support for the proposal in principle, subject to consideration of the issues and the attachment of appropriate conditions, as did Withington Civic Society.

The Planning Officer did not have anything to add to the printed report.

An objector attended and addressed the Committee on the application. She stated that she had requested Planning Officers to defer consideration of this application to enable a more in-depth review as to whether the area was the most suitable location to deliver the type of accommodation being proposed. She stated that there was already 12 supported living units within 200 meters of the proposed development and only 21 properties had been consulted on this planning application, all of them bar two, housed mostly students and at least two of them were other supported living accommodation. She felt that more information was needed in relation to police and ambulance call outs in connection with the existing supported living premises in the area before an informed decision could be made. There was also concern about the additional pressures that this development would place on the local infrastructure, such as access to GP surgeries.

The applicant's agent attended and addressed the Committee. He stated that the applicant had worked in partnership with the Council's homelessness department since March 2020, which had worked very well and wished for this to continue. The site had previously been utilised as nursing home and more recently as a hotel use. The property was currently set up to provide 24 hour support to all residents to help those seeking permanent accommodation. There was a good relationship with direct neighbours and residents. Security staff were on site 24 hours a day seven days a week. The concerns raised by local neighbours were acknowledged and the applicant would seek to minimise any impact. The application would also help reduce the use of emergency temporary accommodation, such as Bed and Breakfast accommodation.

The Planning Officer responded to issues raised by the objector. He advised the Committee that the notification process had gone beyond the Council's statutory requirements and comments received had been taken into account. He added that it was important for the Committee to assess the application on its individual merits and land use planning issues. The existing use of the premises was as a hotel with 30 beds which could be used to home homeless people without the need for planning permission and it was the care package and management facilities to support the occupiers meant that planning permission was now required. The Officer also stated that comings and goings associated with the proposed use would be very similar to a 30 bed hotel but the hotel could also be used for multiple occupancy in each room. As part of the conditions, there would be a requirement for a management regime to be in place which would require the premises to be staffed at all times and occupation would be by referral only.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Lovecy addressed the Committee. She commented that in its current designation the situation could be worse for local residents and felt that with the information provided to the Committee, Members were in a position to make a decision on the application before them.

Councillor Curley leant his support to the application and requested that Officers ensured that a strong management team was put in place to manage the facility. In addition, Councillor Davies sought clarification as to whether there was any condition that could be put in place to ensure the applicant worked closely with the Council's Homelessness team.

The Director of Planning and Building Control advised that there was already a strong relationship between the Council's Homelessness Department and the Operator of the premises and agreed to feedback the Committee's views to officers within the Council's Homelessness Department.

The Planning Officer confirmed that there was a condition in place for a management plan to be submitted and agreed which would include occupancy, arrangements for staffing and accommodation referrals, timings for moving in and out and contracts between occupants and the operator.

Councillor Lovecy asked if it was possible to strengthen this existing condition.

The Director of Director of Planning and Building Control proposed that, if minded, the Committee could approve the application subject to her being able to have discussions with colleagues Homelessness as to how best to strengthen this condition and the subsequently approve the application in consultation with Chair

Councillor Kamal proposed a motion to approve the officer's recommendation of Approve for the application.

Councillor S Ali seconded Councillor Kamal's proposal.

Decision

The Committee is Minded to Approve the application as set out in the report, subject to the Director of Planning and Building Control discussing with colleagues in the Council's Homelessness Department as to how best to strengthen the condition for a suitable management plan.

(Councillor Gartside declared an interest in this application as she had fettered her discretion by making her views known as part of the consultation with Ward Councillors. She left the meeting during consideration of the application).

PH/23/42. 135647/FO/2022 - 550 Mauldeth Road West Manchester M21 7AA - Chorlton Park Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the erection of a new Lidl foodstore (Use Class E) with associated car parking and landscaping.

This application was deferred at the meeting of the Committee held on 16 March 2023; where the Committee resolved to be 'minded to refuse' the proposal and requested that Officers bring a report to a future meeting to address their concerns.

which related to highways safety and specifically to traffic management and the impact that this would have on pedestrian and cycle users of the area.

In response to issues raised at the previous meeting, additional information had been submitted by the applicant in order to further address these concerns. The Planning Officer provided a brief outline of the additional measures proposed and advise that both the Council's Highways department and Transport for Greater Manchester were satisfied with what was now being proposed and would add an additional layer of safety for all users of the highway and footway. On this basis, Planning Officers could not provide appropriate planning grounds for refusal.

An objector attended and addressed the Committee on the application. She raised concerns that the amendments to the highways safety had not and could not make the site suitable for a large supermarket. The proposed development was in the middle of a four school campus and would have an adverse impact on pedestrian and child safety as well as an increase in traffic within the locality. It was felt that the updated highway safety proposals still did not mitigate the concerns already raised. The proposed development was expected to increase traffic by up to 300 cars per hour at peak times and it was felt that the new proposals still did not address the concerns raised by the Committee when it first considered the application.

The applicant's agent attended and addressed the Committee. He stated that the original application had been amended following concerns raised around highways safety. The measures now proposed were in addition to existing safety measures proposed. The applicant was also willing to review traffic patterns in the first three months of operation by way of a condition with a view to ensure it operated in a safe and appropriate manner. He commented that the Council's Highways Department and Transport for Greater Manchester were now satisfied with the additional safety proposals. As previously presented the development would deliver a significant number of tangible benefits to the local community.

Councillor Midgley (Ward Councillor Chorlton Park) state that whilst she recognised the different views on the application from local residents, in her view the additional highways safety mitigations now addressed concerns previously raised. If approved, she hoped the applicant would work with the local community to ensure they were responsible and responsive neighbours.

The Planning Officer reminded the Committee that the current office building on the site provided 105 car parking spaces that could be brought back into use or changed under to retail units under permitted development without the need for the proposed highway safety measures now being proposed, which was a material consideration.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Hughes addressed the Committee. He sought confirmation that the Council's Highways Department were now satisfied with the safety proposals that had been put forward.

The Planning Officer confirmed that the Highways Department was satisfied with the safety measure now being proposed.

Councillor Andrews proposed a motion to approve the officer's recommendation of Approve for the application.

Councillor S Ali seconded Councillor Andrews's proposal.

Decision

The Committee approves the application as set out in the report.

PH/23/43. 135936/FO/2023 - Bignor Street Park Heywood Park Manchester - Cheetham Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the erection of part single, part two storey building to form purpose-built primary school (Class F1) with associated open space, access, landscaping, boundary treatment and other infrastructure works.

The proposals were subject to notification by way of 395 letters to nearby addresses, a site notice was posted at the site and an advertisement placed in the Manchester Evening News. In response to the neighbour notification four comments were received, two objections to the proposals, one comment in support and one neutral comment.

The Planning Officer advised that the mitigation measures proposed by the applicant following comments received from Sport England, now resulted in the recommendation of the Director of Planning and Building Control being altered to Minded to Approve, subject to the signature of an appropriate legal agreement and conditions, and the signing of a Section 106 agreement securing offsite mitigation for re-provision of play. As such, the application would no longer be required to be referred to the Secretary of State.

The applicant's agent attended and addressed the Committee. She stated that the proposal would contribute to additional primary school places in the city. The proposed mitigation package addresses the loss of the playing field on site. There would be no significant on the highways network and the proposed development was in accordance with the relevant policies within the Council's Development Plan.

The Planning Officer clarified that in the late representations received, condition 33 was to be removed as this was a repetition of condition 32 and a slight amendment to the wording of condition 4 was also required.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Riasat addressed the Committee, stating that he welcomed the securing of the Section 106 Agreement and was in full support of the application now that all concerns had been addressed. He proposed a motion to approve the officer's

recommendation of Minded to Approve subject to the signing of a Section 106 agreement securing offsite mitigation for reprovision of play and the deletion of condition 33 and rewording of condition 4 as outlined by Officers.

Councillor Andrews seconded Councillor Riasat's proposal.

Decision

The Committee is Minded to Approve the application, subject to the signing of a Section 106 agreement securing offsite mitigation for reprovision of play and the deletion of condition 33 and rewording of condition 4 as outlined by Officers.

(Councillors S Ali and Hassan declared interests in this application as they had fettered their discretion by having a pre meeting with the applicant at which they made their views on the application known. They left the meeting during consideration of the application).

PH/23/44. 135576/FO/2022 - 88-90 Carmoor Road Manchester M13 0FB - Ardwick Ward

The Committee considered the report of the Director of Planning, Building Control and Licensing that related to the demolition of a number of existing buildings, the erection of part three storey, part six storey purpose-built student accommodation (sui generis) with 172 beds in a mix of studio and cluster units, together with ancillary facilities, shared amenity space, site access and other associated works following demolition of existing buildings.

130 representations had been received, 129 of which objected to the proposed development, along with a third-party objection on behalf of Afro Caribbean and Friends Community Association (ACFCA). A further letter of objection had been received from a patron of the community centre following the submission of revised details and a further period of re-notification.

The Planning Officer did not have anything to add to the printed report.

The applicant's agent attended and addressed the Committee. She stated that the development would deliver high quality student accommodation and already had over 10,000 student beds under its management across the UK. The applicant had met with a number of community representatives and as a result of these discussion he proposed scheme had been significantly reduced to mitigate the impact on the local community centre. The site had been identified as part of the Council future student accommodation pipeline and would be of a high quality design protected by secure access and 24 hour site management. A travel plan commitment to sustainable travel had also been made by the applicant. The development would also look to provide 20% of all bed spaces being advertised below market rent level in each academic year.

The Chair invited the Committee to make comments or ask questions to the Planning Officer.

Councillor Andrews proposed a motion to approve the officer's recommendation of Minded to Approve subject to a legal agreement containing affordable rent obligations for up to 20% of all bed spaces being advertised as being below market rent level in each academic year.

Councillor Kamal seconded Councillor Andrew's proposal.

Decision

The Committee is Minded to Approve the application, subject to a legal agreement containing affordable rent obligations for up to 20% of all bed spaces being advertised as being below market rent level in each academic year.

(Councillor Hewitson declared an interest in this application as she had fettered her discretion by having a pre meeting with the applicant at which she made her views on the application known. She left the meeting during consideration of the application).

Application Number	Date of Appln	Committee Date	Ward
135028/FO/2022	23 Sep 2022	6 July 2023	Deansgate Ward
135029/LO/2022			

Proposal Full Planning Permission for the erection of 14 storey building and the refurbishment of the existing buildings at 3 Smithy Lane and Carriage Works on Garden Lane / St Mary's Parsonage with ground floor extension to form office accommodation (Class E) with front of house and commercial floorspace at ground floor (Class E) with associated landscaping and other works following demolition of existing buildings at 27-29 King Street West and the Grade II Listed 31-33 King Street West and link bridge from Carriage Works on Garden Lane / St Mary's Parsonage to 3 St Mary's Parsonage

LISTED BUILDING CONSENT for the demolition of 31-33 King Street West and the refurbishment of the existing buildings at 3 Smithy Lane and Carriage Works on Garden Lane / St Mary's Parsonage with ground floor extension with associated landscaping and other works following demolition of existing buildings and link bridge from Carriage Works on Garden Lane / St Mary's Parsonage to 3 St Mary's Parsonage in association with the erection of a 14-storey building

Location Land Bound by King Street West, St Marys Parsonage, Garden Lane and Smithy Lane, Manchester, M3 2JP

Applicant Mr Alex Russell, Property Alliance Group, Alliance House, Westpoint Enterprise Park, Clarence Avenue, Manchester, M17 1QS,

Agent Miss Ellie Philcox, Euan Kellie Property Solutions, Landmark House, Station Road, Cheadle Hulme, Cheadle, SK8 7BS

EXECUTIVE SUMMARY

The proposal is for 4,849 sqm of office floorspace and the retention and improvement of the Grade II Listed 3 Smithy Lane and the Carriage Works, following the demolition of part of the listed complex and other buildings on site. Objections have been received from Historic England and Save Britain's Heritage about the loss of 31-33 King Street West and the impact of the new build on the retained listed buildings and the surrounding Conservation Area. The applications will need to be referred to the Secretary of State for a decision if Members are minded to approve this proposal.

Key Issues:

Principle of the proposal and the schemes contribution to regeneration: The proposal accords with national and local planning policies, and would deliver significant economic, social, and environmental benefits. The site is part of the St Mary's Parsonage SRF. The development would have no car parking spaces parking spaces.

The commercial unit would provide active street frontages and the public realm includes tree planting, improves permeability and increases visibility of the Grade II Listed buildings. The demolition would allow full access to be provided and would support the refurbishment and improvements to the retained listed buildings.

Impact on the historic environment. The proposal would impact on listed buildings on the site and the setting of nearby listed buildings and structures and on the Parsonage Gardens Conservation Area. Historic England object to the proposal.

Economic: The proposal would create 135 full time equivalent (FTE) jobs over the 2.5-year build period. Total net GVA from construction would generate around £25 million. Once occupied, it could support 410 FTE jobs, with GVA of £35.8m each year, wages of £13.1m and contribute £2.7m of national insurance and income tax. The development would deliver additional business rates of £6.6m every ten years.

It would create a range of jobs, including highly skilled roles, graduate opportunities, and entry level roles helping to drive economic growth and productivity and support inclusive growth objectives.

Social: A local labour agreement would maximise the recruitment of local residents in construction and related jobs. This would extend to end use job opportunities. Commercial units would bring active frontages and natural surveillance. The development would be fully accessible. The public realm would address changes making it fully accessible. Crime and anti-social behaviour would be minimised with an effective lighting scheme.

Environmental: This would be a low carbon development in a highly sustainable location. The development would include enhanced fabric parameters to limit heat losses, maximised glazing ratios to provide natural lighting, electrification of the systems and renewable technologies to offset some outstanding energy consumption. There would be no unduly harmful impacts on traffic and local air quality. Where impacts do arise, these can be mitigated. New planting, trees and bird and bat boxes would improve biodiversity. A drainage scheme includes sustainable principles and would include SuDS features such as rain gardens within the public realm. The ground conditions are not complex or unusual. The height, scale and appearance would contribute positively to the Parsonage Gardens SRF Area. Secured by Design principles would ensure the development is safe and secure. Waste management would prioritise recycling to minimise the amount of waste going to landfill.

Impact on local residents and local businesses: The impact on daylight/sunlight and overlooking would be acceptable. Construction impacts would not be significant and can be managed to minimise the effects on local businesses.

Description of the Site

This 0.26 ha site is bounded by King St West, Smithy Lane, Garden Lane and St Mary's Parsonage and is in the Parsonage Gardens Conservation Area. It includes: a four-storey carriage workshop on Garden Lane, a 4-storey warehouse at 3 Smithy

Lane and 31-33 King St West (Reedham House), all of which comprise a listed complex and 3-storey 27-29 King St West, which is not part of the listed complex.



The complex was a carriage works and provides considerable evidence of the character and evolution of smaller scale industry in nineteenth and twentieth century Manchester. It is listed and comprises a manufacturing block built in 1840, a commercial warehouse built in 1855, which functioned separately, but was in the same ownership as the carriage works. It also includes a showroom (Reedham House) built in 1849, which was heavily altered in 1926 and largely re-constructed after Second World War bomb damage.

Reedham House was in separate occupation and activity by the late 1870s. It was totally rebuilt behind its retained and re-modelled façade in 1926. The buildings have a function and interrelationship that remains legible. It is a rare building typology, and only five listed carriage work complexes survive across the country.



The Listed complex has been altered and has blocked up openings, lay in grid suspended ceilings, UPVC windows and doors, screed applied to floors and solid partitions. Most historic fabric other than some brickwork is concealed. There are serious damp issues in the basement. The courtyard contains air-conditioning units, and features such as lightwells, cobbles, glazed pavement lights, drainpipes and lightwells have been neglected and/or replaced/covered with modern materials.



Concrete staircases and other structures have been built on stone paving and cobbles and a bridge link interrupts the underpass/entrance to the courtyard. None of the buildings are accessible to disabled people.



The site is adjacent a Grade II Listed Building at 3 St Mary's Parsonage. Nearby Listed Buildings and structures include Arkwright House on Parsonage Gardens, the former Kendal Milne department store, Century Buildings on St Mary's Parsonage, the Masonic Hall on Bridge Street, The Sawyers Arms, Albert Bridge, and the former Hydraulic Power Station in Grounds of City College.

Other nearby approved developments:

A three-storey rooftop extension to Kendals (98 to 116 Deansgate) to provide offices with flexible commercial spaces on the ground, lower ground and basement and the erection of 14 storey office building on the site of the MSCP (129251/FO/2021 and 129252/LO/2021). The erection of a 19-storey office following the demolition of Alberton House on St Marys Parsonage (134287/FO/2022). Surrounding uses include shops, offices, hotels, bars, restaurants, financial and professional services, café, and museum. The site is close to Parsonage Gardens.

The proposal

Planning permission and listed building consent are sought for a 14-storey office building fronting Motor Square following the demolition of part of the Grade II Listed complex (Reedham House, 31-33 King Street West). It would provide 4,849 sqm (net) of floorspace with an external roof terrace at floor 10.



The base would have a colonnade of circular textured columns and active street frontages. The main body of the facade is clad in an anodised aluminium rainscreen cladding with extruded fins.

The green colour seeks to complement the reddish/brown brickwork at the retained Carriage Works and Warehouse. The base would be a pigmented concrete lintel to complement existing brickwork, influenced by the stonework of the warehouse on Smithy Lane.



The Grade II listed Carriage Works on Garden Lane and Warehouse at 3 Smithy Lane, would be fully refurbished including:

- Restoration and repair of all elevations and rooves.
- Replacement of modern insertions such as white plastic windows with alternatives that complement the character and original features.
- Removal of items such as roller shutters.
- Reinstatement of the Garden Lane entrance to the Carriage Works to provide an entrance to the historic courtyard where there would be a lift to all levels and the main building reception.
- Decluttering and repair of the inner courtyard to expose its original features and architectural detail.
- Two lifts in the courtyard would provide access to all levels, most of which don't currently have full access.
- Installation of a glazed roof over the courtyard to use the space as an extension of the reception space.
- Repointing of brickwork

- Retention and repair of all internal original and historic structures and features such as columns.
- Removal of modern wall linings to reveal original brickwork and sensitively repair where needed.
- Removal of modern ceilings to allow the floorboards and supporting structures to be revealed.
- Installation of lighting and services, which would be exposed.
- Reinstatement of the historic Carriage Works entrance from Garden Lane with new glazed entrance doors to the archway. This new entrance would create a north/south connection through the building.
- Cleaning of the historic lintels of the Garden Lane elevation.
- Installation of anodised aluminium fire escape doors within existing openings.
- Installation of feature lighting to the Courtyard entrance on Garden Lane.
- Replacement of the entrance door on Smithy Lane to create a fire escape.

The following works are also proposed:

- Demolition of the link bridge between the Carriage Works and 3 St Mary's Parsonage with opening reinstated as a sash window.
- Use of 140 sqm of the ground floor of the new build as a commercial unit (Class E use) on St Mary's Parsonage to provide an active street frontage.
- Provision of a gym (Class E use) and secure cycle storage (92 spaces) and shower facilities with accesses off Garden Lane and Smithy Lane.
- Creation of public realm on King Street West, St Marys Parsonage and Garden Lane / Smithy.
- Biodiversity enhancements through the provision of bat and bird boxes and inclusion of rain gardens in the landscaping. A variety of pollinator-friendly plant species including grasses and herbaceous species.
- All accesses would be level.
- Five passenger lifts.
- Creation of a loading bay on St Mary's Parsonage, shared with the recently implemented taxi rank, such that the bay becomes loading only during daytime hours, when the taxi rank is typically not used, and taxi rank in the evening.
- No car parking provided.
- An internal waste store accessed from Garden Lane.
- An Internal service riser.

A Framework Travel Plan has been provided.

The planning and Listed Building applications have been supported by the following information: - Drawings, Landscape Plans; Design and Access Statement, Air Quality Assessment, Archaeological Assessment, Biodiversity Net Gain Assessment, Construction Management Plan, Crime Impact Assessment, Daylight/Sunlight Report, Economic Impact Assessment, Energy and Environmental Standards Statement, Local Labour Agreement, Wind Assessment, Planning Statement, Statement of Community Involvement, Townscape and Heritage Visual Impact Statement, Ventilation Statement, Noise Assessment, Ecology Report, Flood Risk and Drainage Report, Heritage Asset Statement, Waste Management Statement and Transport Statement.

Consultations

Publicity – The occupiers of adjacent premises have been notified and the applications have been advertised in the local press as a major development, being of public interest, affecting the setting of a conservation area and the setting of listed buildings, as an application for listed building consent and as a development affecting a public right of way. Notification letters have been sent to an extensive area and 1 objection, 1 letter of support and 1 comment have been received.

Save Britain's Heritage object to the demolition of the grade II listed building at 31-33 Kings Street West which constitutes substantial harm and is unjustified in heritage and environmental terms.

A) Unjustified loss of a grade II listed building Reedham House, a local landmark of high historic significance. The harm is substantial and unjustified and would cause irreversible harm. Reedham House is listed for its architectural interest because of its high architectural interest as a surviving 1840s coach works, comprising a manufacturing block and a showroom. The proposal would harm the architectural interest and significance of the grade II listed 3 Smithy Lane and surrounding heritage assets and compromise the collection of buildings' original plan form. The relationship between the former showroom and warehouse and how their fabric relates to one another is of particular architectural interest, however the proposal involves the total loss of the fabric where the two phases meet. The loss of one section would therefore have a substantial effect on the other, losing the synergy between the extant structures. The destruction of this property would therefore result in the complete loss of both the historic and architectural interest of this building.

B) The loss of Reedham House and the proposed 14 storey office would substantially harm the character and significance of the Parsonage Gardens Conservation Area. The harm to the heritage significance of the Conservation Area would be substantial, with no clear or convincing justification provided. The increased scale would severely alter the historic façade that acts as an important character boundary to the conservation area, reinforcing the dominance of new tall buildings that circle the boundary to the southwest. The 14-storey tower is at odds with the conservation area's setting and long-established special character, which is characterised by 19th century low to mid-rise commercial buildings. The Council's legal duty outlined in Sections 66 and 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 includes the preservation and enhancement of Reedham House, a building which is a clear positive contributor to its overall historic character and significance.

C) The demolition is not sustainable development in environmental terms. It would have an unnecessary negative carbon cost. Retaining and retrofitting historic buildings and the considerable amount of embodied carbon they contain, is of paramount importance if Manchester City Council is to become carbon neutral by 2038. Regardless of heritage value, the needless demolition of a building which is perfectly capable of reuse is fundamentally at odds with this commitment.

Cadent Gas – no objection.

Historic England - Historic England objects to the application on heritage grounds, and don't consider that the application meets the requirements of the NPPF, in particular paragraphs 189, 199, 200 and 202.

31-33 King Street West is of historic and architectural interest in its own right, and its value is considerably greater when appreciated as part of the wider historic carriage works complex, which is designated as a collective whole.

The loss of one element would therefore have a pronounced effect on the other, and the impact on the significance of the listed building would be high. More widely, the building provides important evidence for both the spread of small-scale industry across Manchester, and a rare insight into the form and functioning of carriage works nationally. The diminishment of this complex would therefore have wider harmful implications. Its loss would also deprive the conservation area of a handsome, if altered, building, that is not only an attractive element of the street scene, but also contributes to our understanding of the history and evolution of the area.

The impact on the conservation area is less considerable than on the listed building but would still be harmful and strikingly noticeable. The negative impact would be exacerbated by the scale and design of the replacement tower. This would distract from the retained elements of the complex and reinforce the loss by obscuring the legibility of the rest of the historic site. Its scale, verticality and design would be out of keeping with the character and appearance of the conservation area, converting the site from one which forms the gateway into the conservation area, to one that further reinforces the prevalence of ad hoc tall buildings, of indifferent design, which form a ring outside the south and west edge of the conservation area.

The proposals would therefore cause considerable harm to the significance of both of these assets. The scale of the impact on the significance of the listed building would be high and the impact on the conservation area would be lower, but still considerable.

If the Local Planning Authority is minded to grant consent in its current form, in light of this objection they should treat this letter as a request to notify the Secretary of State of this application, in accordance with the relevant Direction.

The LPA should take advice from their own advisors to assess the proposed works to convert the former manufacturing building, and to conclude if this would produce heritage benefits. However, even if it did so, it is not clear that the achievement of this potential benefit is intrinsically linked to the demolition and replacement of 31-33 King Street West.

There are no concerns with the loss of 27-29 King Street West.

Head of Highways no objections subject to conditions about off-site highways works, a servicing strategy, provision and adoption of a Travel Plan and a Construction Management Plan.

Head of Regulatory and Enforcement Services (Street Management and Enforcement) No objection and recommends conditions relating to plant and equipment, the management of construction, opening hours, the hours during which

deliveries can take place, acoustic insulation of the premises, the external roof terrace, the storage and disposal of refuse, air quality and the mitigation / management of any contaminated land.

Flood Risk Management Team Advised that invert levels were only partially provided, and the critical storms were not provided and recommend conditions relating to surface water drainage works and maintenance and management.

Greater Manchester Police (Design for Security) Recommend a condition to reflect the physical security recommendations in the Crime Impact Statement.

Work and Skills Recommend local labour conditions for construction and in use.

Environment Agency No objection subject to conditions.

Greater Manchester Ecology Group No objections subject to conditions to protect bats and birds and confirm that the net gain assessment has demonstrated that the site will achieve a sufficient gain in biodiversity units.

United Utilities No objections subject to a condition about surface water drainage.

GMAAS recommends that advice is sought regarding the impact of the proposals on the other listed structures at the site, along with other Designated Heritage Assets and their settings (including the Conservation Area), and the necessity for appropriate mitigation. A scheme of historic building recording should be undertaken prior to any demolition works through a condition. A Level 1 to 2 historic building survey should be undertaken on retained buildings prior to refurbishment.

This would enable a point-in-time descriptive and photographic record of the exteriors and interiors of the structures, complete with floor plans that indicate survival of historic fabric, fixtures and fittings. This could inform whether further watching briefs should be maintained during works that might reveal concealed historic fabric, such as any strip-out works and demolition.

Health and Safety Executive (Fire Safety) No comments.

Transport for Greater Manchester – suggest a condition requiring the submission, implementation, and monitoring of a Full Travel Plan within 6 months of occupation.

Manchester Airport Safeguarding Office – No objections

National Amenity Societies – No comments

Salford City Council – No comments

Issues

The proposals would be consistent with Core Strategy Policies SP1 (Spatial Principles), EC1 (Land for Employment and Economic Development), CC1 (Primary Economic Development Focus (City Centre and Fringe)), CC4 (Visitors – Tourism,

Culture and Leisure), CC5 (Transport), CC6 (City Centre High Density Development), CC7 (Mixed Use Development), CC8 (Change and Renewal), CC9 (Design and Heritage), CC10 (A Place for Everyone), C1 (Centre Hierarchy), Policy EC1 (Land for Employment and Economic Development), Policy EC3 (The Regional Centre), T1 (Sustainable Transport), T2 (Accessible Areas of Opportunity and Need), EN1 (Design Principles and Strategic Character Areas), EN3 (Heritage), EN4 (Reducing CO2 Emissions), EN6 (Target Framework for CO2 Reductions), EN8 (Adaptation to Climate Change), EN9 (Green Infrastructure), EN11 (Quantity of Open Space, Sport and Recreation), EN14 (Flood Risk), EN15 (Biodiversity and Geological Conservation), EN16 (Air Quality), EN17 (Water Quality), EN18 (Contaminated Land), EN19 (Waste) and DM1 (Development Management).

The Core Strategy Development Plan Document 2012-2027 was adopted in July 2012 and is the key document in the Local Development Framework. It sets out the long-term strategic planning policies for Manchester. A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents. The adopted Core Strategy contains a number of Strategic Spatial Objectives that form the basis of the policies contained therein, as follows:

SO1. (Spatial Principles) – The development would be in a highly accessible location and reduce the need to travel by private car and would support sustainable growth and help to halt climate change.

SO2. (Economy) – The scheme would provide construction jobs along with permanent employment in a highly accessible location. It would also provide 4,849 sqm of office floorspace. This would support the City's role as the main employment location and primary economic driver of the City Region.

SO5. (Transport) – The development would be highly accessible, reducing the need to travel by private car and would make the most effective use of public transport. This would improve physical connectivity and help to enhance the functioning and competitiveness of the city and provide access to jobs, education, services, retail, leisure, and recreation.

SO6. (Environment) – The development would protect and enhance the natural and built environment and ensure the sustainable use of natural resources in order to: mitigate and adapt to climate change; support biodiversity and wildlife; improve air, water, and land quality; improve recreational opportunities; and help to ensure that the City is inclusive and attractive to residents, workers, investors, and visitors.

Policy SP1 (Spatial Principles) – The development would be sustainable and would deliver economic and commercial development, including office, retail and leisure uses. It would be consistent with the City Centre Strategic Plan and the St Mary's Parsonage SRF. The development would be close to sustainable transport and maximise the use of the City's public transport infrastructure. It would enhance the built and natural environment and provide office accommodation; creating a well-designed place; creating character and reducing the need to travel.

Policy C1 (Centre Hierarchy) - The development would assist the City Centre in remaining the regional focus for commerce, culture, leisure, and tourism.

Policy EC1 (Land for Employment and Economic Development) - The office space could generate employment and help to spread the benefits of growth across the City and reduce economic, environmental, and social disparities. The site is close to transport infrastructure and the proposal would promote walking, cycling and public transport use. Jobs would be created during construction and in operation. The design would use the site efficiently and a range of transport modes are nearby.

Policy EC3 (The Regional Centre) - The proposal would deliver high quality office floorspace in an area for employment growth on a highly accessible site. This would help to spread the benefits of growth across the City, help to reduce economic, environmental, and social disparities and create inclusive sustainable communities. The site is connected to nearby transport infrastructure in an appropriate location for office development. It would maximise walking, cycling and public transport use.

Policy CC1 (Primary Economic Development Focus (City Centre and Fringe)) - The development would complement existing uses and would be a high-density, mixed-use scheme that would provide active uses in addition to office accommodation.

Policy CC4 (Visitors – Tourism, Culture and Leisure) – The proposal would improve facilities for visitors, including the provision of improved public realm and an improved contribution to the quality of Motor Square. It would better reveal the significance of the retained heritage assets. It would improve permeability and the public realm at Garden Lane and Smithy Lane, St Mary's Parsonage and King Street West / Motor Square.

Policy CC5 (Transport) - The proposal would provide secure accommodation and increase natural surveillance. It would be accessible by a variety of modes of transport and help to improve air quality and reduce carbon emissions.

Policy CC6 (City Centre High Density Development) – The extension would use the site efficiently through the construction of the 14-storey office building.

Policy CC7 (Mixed Use Development) - The development would incorporate active frontages, create activity, and increase footfall.

Policy CC8 (Change and Renewal) – The proposal would support the employment role of the City Centre and improve accessibility and legibility. It would create jobs during construction and in operation and would maximise the use of the site.

Policy CC9 (Design and Heritage) – The design would be appropriate in its context. The scale, height, massing, alignment, and material of the new build are acceptable. The development would cause less than substantial harm to heritage assets. The loss of Reedham House would allow significant improvements to the interior and exterior of the retained fabric including the courtyard. It would allow full access to the retained listed buildings.

Policy CC10 (A Place for Everyone) – Full access would be provided to all levels. This is a highly accessible, sustainable location. Permeability around the site would be improved and high-quality public realm would be provided on the streets surrounding the complex including Garden Lane and Smithy Lane.

Policy T1 (Sustainable Transport) – The proposal would encourage modal shift away from car travel to more sustainable alternatives through its location, the implementation of a travel plan and the provision of cycle parking spaces.

Policy T2 (Accessible Areas of Opportunity and Need) – The proposal would be accessible by a variety of sustainable transport modes. Permeability around the Site would also be improved with the provision of new high-quality public realm on the streets surrounding the site.

Policy EN1 (Design Principles and Strategic Character Areas) - The design would be high-quality and would enhance the character of the area. The design responds positively at street level with the provision of active street frontages, improved permeability and an improved frontage to St Mary's Parsonage and King Street West/Motor Square.

Policy EN3 (Heritage) - The development would have a positive impact on the character and appearance of the site. The retained buildings would be refurbished and extended improving their appearance, increasing maintenance, and securing the future for the building. The loss of Reedham House would cause less than substantial harm given that Reedham house is one element of a listed complex and is not a listed building in its own right. This harm would be justified by significant public benefits. The heritage issues are addressed in full in the main body of the report

Policy EN4 (Reducing CO2 Emissions by Enabling Low and Zero Carbon Development) - The proposal would follow the principle of the Energy Hierarchy to reduce CO2 emissions.

Policy EN6 (Target Framework for CO2 reductions from low or zero carbon energy supplies) – The development would comply with the CO2 emission reduction targets set out in this policy.

Policy EN8 (Adaptation to Climate Change) - This is a highly sustainable location. The site is in flood risk zone 1 and generally has a low risk of flooding. The proposal includes a drainage strategy, is supported by an energy statement, and would comply with Part L 2021. A BREEAM pre assessment report confirms that the development could achieve 'Very Good.'

Policy EN9 (Green Infrastructure) – Four street trees would be planted on King Street West, and one would be removed from St Mary's Parsonage. The landscaping would lose 0.02 biodiversity units and deliver 0.24 biodiversity units, giving a Biodiversity Net Gain score of +0.22 biodiversity units.

Policy EN11 Quantity of Open Space, Sport and Recreation – The proposal would improve King Street West, St Mary's Parsonage, Garden Lane and Smithy Lane and

provide a function for the courtyard, which would include lifts and a reception. It would improve permeability and improve the contribution of the site to Motor Square. It would increase connectivity between spaces to allow better links for disabled people, pedestrians, and cyclists, and enhance biodiversity through the provision of street trees and rain gardens and bat housing.

Policy EN14 (Flood Risk) – A Flood Risk Assessment and drainage strategy identifies that the site is within Flood Zone 1 (low probability) and proposes a drainage strategy.

EN15 (Biodiversity and Geological Conservation) – The landscaping would lose 0.02 biodiversity units and deliver 0.24 biodiversity units, giving a BNG of +0.22 biodiversity through the raingardens, planting and street trees. Four bat slates and two integrated bat boxes are proposed.

Policy EN16 (Air Quality) - The proposal would be highly accessible by all forms of public transport and reduce reliance on cars and minimise traffic emissions. An air quality assessment has been provided. The proposals do not include car parking.

Policy EN17 (Water Quality) - The proposal would not have an adverse impact on water quality. Surface water run-off and ground water contamination would be minimised.

Policy EN18 (Contaminated Land and Ground Stability) - A site investigation, which identifies possible risks arising from ground contamination has been provided.

Policy EN19 (Waste) - The development would be consistent with the principles of waste hierarchy and a Waste Management Strategy has been submitted.

Policy DM1 (Development Management) – This policy sets out the requirements for developments and outlines a range of general issues that all development should have regard to. Of these the following issues are of relevance to this proposal:

- appropriate siting, layout, scale, form, massing, materials, and detail.
- design for health.
- adequacy of internal accommodation and amenity space.
- impact on the surrounding areas in terms of the design, scale, and appearance of the proposed development.
- that development should have regard to the character of the surrounding area.
- effects on amenity, including privacy, light, noise, vibration, air quality and road safety and traffic generation.
- accessibility to buildings, neighbourhoods, and sustainable transport modes.
- impact on safety, crime prevention and health; adequacy of internal accommodation, external amenity space, refuse storage and collection, vehicular access, and car parking; and
- impact on biodiversity, landscape, archaeological or built heritage, green Infrastructure and flood risk and drainage.

The application is considered in detail in relation to the above issues within this report and is considered to be in accordance with this policy.

Saved Unitary Development Plan Policies

Policy DC10.1 (Food and Drink Use) – The proposal could include a restaurant or café which would be appropriate in this location.

DC18.1 (Conservation Areas) – This is addressed in full in the report.

DC19.1 (Listed Buildings) – This is addressed in full in the report.

DC20 (Archaeology) – A condition would require the submission and agreement of a written scheme of investigation and a programme of archaeological works.

DC26.1 and DC26.5 (Development and Noise) – Acoustic assessments have been submitted.

DM2 (Aerodrome Safeguarding) – Manchester Airport have no objections

Guide to Development in Manchester Supplementary Planning Document and Planning Guidance (April 2007)

This Supplementary Planning Document supplements guidance within the Adopted Core Strategy with advice on development principles including on design, accessibility, design for health and promotion of a safer environment. The proposals would comply with these principles where relevant.

Relevant National Policy

The National Planning Policy Framework sets out Government planning policies for England and how these are expected to apply. The NPPF seeks to achieve sustainable development and states that sustainable development has an economic, social and an environmental role (paragraphs 7 & 8). Paragraphs 10, 11, 12, 13 and 14 of the NPPF outline a "presumption in favour of sustainable development". This means approving development, without delay, where it accords with the development plan. Paragraphs 11 and 12 state that:

“The presumption in favour of sustainable development does not change the statutory status of the development plan as the starting point for decision making. Where a planning application conflicts with an up-to-date development plan (including any neighbourhood plans that form part of the development plan), permission should not usually be granted. Local planning authorities may take decisions that depart from an up-to-date development plan, but only if material considerations in a particular case indicate that the plan should not be followed.”

The proposed development is considered to be consistent with sections 6, 7, 8, 9, 11, 12, 14, 15 and 16 of the NPPF for the reasons outlined below:

Section 6 – (Building a strong and competitive economy) – The proposal would support economic growth to create jobs and prosperity through construction and through the use of the completed development, building on an area's inherent

strengths. The proposal would support economic growth and create jobs and prosperity

Section 7 (Ensuring the Vitality of Town Centres) - promotes competitive town centres and recognises that office and leisure uses can ensure the vitality of centres. The City Centre is the focus for economic and commercial development, leisure and cultural activity and city living. The proposal would be part of an area which would attract and retain a diverse labour market. It would be a high-quality development on an underused site that would benefit from the improvements proposed. Development of the site, which is within a regeneration area and in need of redevelopment, restoration and repair would improve the character and appearance of the area and increase the amount of office space within the City Centre.

Section 8 (Promoting healthy and safe communities) – This recognises that the planning system can facilitate social interaction and create healthy, inclusive communities. The development would incorporate a mix of uses and provide active street frontages and would be safe and accessible. It would be fully integrated into the wider area and would relate well to and complement the nearby uses around Parsonage Gardens. It would assist in activating Motor Square.

Section 9 (Promoting Sustainable Transport) – This site is close to Victoria and Salford Central railway stations and Shudehill bus, coach and Metrolink Interchange and Exchange Square Metrolink Station. Route 1 of the Manchester City Centre 'Metroshuttle' free bus has a stop on Bridge Street. The closest local bus stops to the Application Site are located at Bridge Street. Additional bus stops are available at Deansgate and John Dalton Street. There would be 96 no. secure cycle spaces provided within the basement of the development, which is in excess of the guidance set out by MCC within the Core Strategy, along with shower and changing facilities and lockers. This would be sustainable development and contribute to wider sustainability and health objectives and would give people a choice about how they travel.

Section 11 (Making Effective Use of Land) - Planning should promote the effective use of land, safeguard and improve the environment and ensure safe and healthy living conditions. The development would use the site effectively due to the construction of a 14-storey office building.

Section 12 (Achieving Well-Designed Places) – Promotes the high quality and inclusive design of individual buildings, public and private spaces and wider development schemes. The building would respect the historic site and the character of the existing buildings and would be fully accessible. The proposals would also include high quality and inclusive public space.

Section 14 (Meeting the challenge of climate change, flooding and coastal change) – The site is in flood risk zone 1 and therefore has a low risk of flooding. It would be a low carbon development and a BREEAM pre assessment report confirms that the development could achieve 'Very Good.'

Section 15 (Conserving and enhancing the natural environment) - The potential risks of various forms of pollution, including ground condition and noise, has been

considered. It is considered that the proposals, with appropriate mitigation measures, would not have any significant adverse impacts on the natural environment and includes measures to protect and enhance the natural environment.

Section 16 (Conserving and Enhancing the Historic Environment) - The site is within the Parsonage Gardens Conservation Area and includes a Grade II Listed Carriage works complex and there are other nearby Grade II Listed Buildings. The applicant has submitted a heritage statement that assesses the impact on the Carriage Works, including the loss of the showroom and on the surrounding heritage assets. The proposal would not have an unjustified detrimental impact on the Grade II Listed Carriage Works, the Parsonage Gardens Conservation Area, or nearby listed buildings. The loss of Reedham House would cause less than substantial harm given that Reedham house is one element of a listed complex and is not a listed building in its own right. This harm would be justified by significant public benefits.

Other Relevant City Council Policy Documents

Our Manchester Strategy 2016-25 – sets out the vision for Manchester to become a liveable and low carbon city which will:

- Continue to encourage walking, cycling and public transport journeys.
- Improve green spaces and waterways including them in new developments to enhance quality of life.
- Harness technology to improve the city’s liveability, sustainability, and connectivity.
- Develop a post-2020 carbon reduction target informed by 2015's intergovernmental Paris meeting, using devolution to control more of our energy and transport.
- Argue to localise Greater Manchester's climate change levy so it supports new investment models.
- Protect our communities from climate change and build climate resilience

This strategy sets the ambitions for the City for the next decade. It sets out a vision for Manchester to be in the topflight of world-class cities by 2025, when the City will: have a competitive, dynamic and sustainable economy that draws on our distinctive strengths in science, advanced manufacturing, culture, and creative and digital business – cultivating and encouraging new ideas; possess highly skilled, enterprising and industrious people; be connected, internationally and within the UK; play its full part in limiting the impacts of climate change; be a place where residents from all backgrounds feel safe, can aspire, succeed and live well; and be clean, attractive, culturally rich, outward-looking and welcoming.

Through its objective of being a progressive and equitable city, from a development and regeneration point of view, this not only means creating and enabling jobs and growth it also demands a smart and thoughtful approach to how development is executed. This should ensure that residents living in nearby areas and circumstances of disadvantage are connected to employment, skills, and training opportunities, and given the support and empowerment necessary to make the most of them. The ways in which the development would play a part in the realisation of these objectives is set out below.

Manchester: A Certain Future (MACF) is the city-wide climate change action plan, which calls on all organisations and individuals in the city to contribute to collective, citywide action to enable Manchester to realise its aim to be a leading low carbon city by 2020. Manchester City Council (MCC) has committed to contribute to the delivery of the city's plan and set out its commitments in the MCC Climate Change Delivery Plan 2010-20.

Manchester Climate Change Board (MCCB) Zero Carbon Framework - The Council supports the Manchester Climate Change Board (MCCB) to take forward work to engage partners in the city to address climate change. 1.3 In November 2018, the MCCB made a proposal to update the city's carbon reduction commitment in line with the Paris Agreement, in the context of achieving the "Our Manchester" objectives and asked the Council to endorse these ambitious new targets.

The Zero Carbon Framework - outlines the approach which will be taken to help Manchester reduce its carbon emissions over the period 2020-2038. The target was proposed by the Manchester Climate Change Board and Agency, in line with research carried out by the world-renowned Tyndall Centre for Climate Change, based at the University of Manchester. Manchester's science-based target includes a commitment to releasing a maximum of 15 million tonnes of CO₂ from 2018-2100. With carbon currently being released at a rate of 2 million tonnes per year, Manchester's 'carbon budget' will run out in 2025, unless urgent action is taken. Areas for action in the draft Framework include improving the energy efficiency of local homes; generating more renewable energy to power buildings; creating well-connected cycling and walking routes, public transport networks and electric vehicle charging infrastructure; plus, the development of a 'circular economy', in which sustainable and renewable materials are reused and recycled as much as possible.

Climate Change and Low Emissions Implementation Plan (2016-2020) -This Implementation Plan is Greater Manchester's Whole Place Low Carbon Plan. It sets out the steps we will take to become energy-efficient and investing in our natural environment to respond to climate change and to improve quality of life. It builds upon existing work and sets out our priorities to 2020 and beyond. It includes actions to both address climate change and improve Greater Manchester's air quality. These have been developed in partnership with over 200 individuals and organisations as part of a wide-ranging consultation. The alignment of the proposals with the policy objectives set out above is detailed below.

'Powering Recovery: Manchester's Recovery and Investment Plan' – This sets out what Manchester is doing to respond to the COVID-19 pandemic and reinvigorate its economy, with plans to protect and create jobs, and support new business opportunities in the city's economy. It sets out how Manchester can play a leading role in the levelling-up agenda, with ambitious plans to build on recent investment in economic assets and infrastructure and accelerate the growth in high-productivity sectors including the Digital, Creative, Technology and Health Innovation Sectors alongside the well-established financial and professional services sectors. This includes support for major job-generating investment with high-growth sectors, new-starts and scale-up. The office space would create high quality office space in a highly sustainable location with excellent transport links. This would support the aim to secure a highly skilled and knowledge intensive workforce within the City.

The development would intensify the levels of economic activity associated with this part of the City Centre and would as part of the wider delivery of the Parsonage Garden SRF, build on the success of the adjacent Spinningfields Area.

The Greater Manchester Strategy, Stronger Together, - This is the sustainable community strategy for the Greater Manchester (GM) Region. The proposal would deliver the comprehensive refurbishment and repurposing of an underutilised site within the City Centre in order to bring a new high standard of office accommodation to the City. The proposed development would support and align with the overarching programmes being promoted by the City Region via the GM Strategy helping to achieve a number of its key growth priorities including the reshaping of the economy to meet global demand and building Manchester's global brand.

The Greater Manchester Strategy (2017) ("Our People, Our Place") - was produced the Greater Manchester Combined Authority (GMCA) and replaces the former "Stronger Together: Greater Manchester Strategy" published in 2009. It sets out a very clear vision for the City-Region, stating that Manchester will be:

"A place where all children are given the best start in life and young people grow up inspired to exceed expectations.

A place where people are proud to live, with a decent home, a fulfilling job, and stress-free journeys the norm. But if you need a helping hand, you'll get it.

A place of ideas and invention, with a modern and productive economy that draws in investment, visitors and talent.

A place where people live healthy lives and older people are valued.

A place at the forefront of action on climate change with clean air and a flourishing natural environment.

A place where all voices are heard and where, working together, we can shape our future."

Delivery of a new office block and associated commercial accommodation would create a substantial amount of employment opportunities that range from contributing to the supply chain indirectly in addition to direct job creation through new commercial office floorspace. This new office block would contribute directly to creating an environment that attracts investment into local and regional centres within Greater Manchester and in Manchester, which is seen as the heart of the region.

The proposed office accommodation would support and align with the overarching programmes being promoted by the City Region via the GM Strategy.

The St Mary's Parsonage SRF (2020) - This was formally endorsed by the Executive in July 2020 and is a material consideration. It aims to create a commercially led, mixed-use district, set around high-quality public space.

This could deliver significant regeneration benefits and support the growth of the City Centre. The SRF should build on its asset base, and its proximity to the City Centre, to successfully host an eco-system of best-in-class employment – from global businesses to start ups - and other high-quality uses including residential, hotel, retail, and leisure. A key principle is to retain and celebrate the area's special

qualities and assets, including its heritage assets, public spaces, and green and blue infrastructure.

The SRF sets out a clear vision to enhance and regenerate the area and identifies sites that could catalyse investment and drive change. The proposal is a component of this strategy and is in an area that has the capacity to accommodate taller buildings, whilst delivering improved permeability and enhanced public realm. The Site is recognised as a 'Development Opportunity', within the 'King Street West Zone'.

For reasons set out below the proposal would be consistent with the height, density and development principles set out above and would help to deliver the SRF.

Manchester City Centre Strategic Plan- The Strategic Plan 2015-2018 updates the 2009-2012 plan and seeks to shape the activity that will ensure the City Centre continues to consolidate its role as a major economic and cultural asset for Greater Manchester and the North of England. It sets out the strategic action required to work towards achieving this over the period of the plan, updates the vision for the City Centre within the current economic and strategic context, outlines the direction of travel and key priorities over the next few years in each of the City Centre neighbourhoods, and describes the partnerships in place to deliver those priorities. The site sits within the Area designated as the Retail Core at a key location on St Mary's Parsonage and there are several heritage assets that would be impacted by the proposal. As detailed above the St. Marys Parsonage Strategic Regeneration Framework (SRF) area, of which the site forms part of, seeks to refocus the priorities of the area in order to establish St Mary's Parsonage as a clearly definable and cohesive City Centre neighbourhood, with a distinctive character and sense of place. MCC have recognised that regeneration opportunities of parts of the Retail Core beyond a traditional retail focus are available on the fringes of this neighbourhood.

Conservation Area Declarations

Parsonage Gardens Conservation Area Declaration

The Parsonage Gardens Conservation Area is bounded by Blackfriars Street, Deansgate (a common boundary with the St Ann's Square Conservation Area), Bridge Street (a common boundary with the Deansgate/Peter Street Conservation Area) and St Mary's Parsonage. The River Irwell forms the western boundary of the area along the line of the administrative border of the City of Salford.

It contains several Grade II listed buildings, including Blackfriars Bridge, plus more recent buildings such as Alexandra House and Century Buildings (modern element). At the centre of the Conservation Area is Parsonage Gardens which is bordered by large buildings. Most are orange-red brick or terracotta, although one modern-style steel and glass structure merges well into its surroundings. Parsonage Gardens is surrounded by a rich mixture of buildings of various ages and styles which are relatively harmonious in their relationships with one another.

The Grade II listed Arkwright House is a significant 7 storey office block in the conservation area. The Conservation Area includes frontage to the River Irwell and includes part of the Grade II listed bridge on Blackfriars Street built around 1820.

The architectural emphasis of corners is a characteristic of Manchester buildings which contributes to the urban design character of the city centre. It is evident in the Parsonage Gardens area and its use in new developments is encouraged.

St Ann's Square Conservation Area Declaration

The St Ann's Square Conservation Area is in the commercial heart of the City, where almost all buildings accommodate shops on the ground floor. It was the first conservation area to be designated by the City Council on 29 July 1970.

St Ann's Square is the focal point of this conservation area. It was laid out in the Georgian period, however the Grade I Listed St Ann's Church is the only surviving building of this time. The remaining buildings are later replacements that continue to enclose the square in a satisfactory and coherent manner, constructed in various styles over a long period and creating a rich tapestry of built form.

Each new building has been designed with due regard for the existing buildings and together they create an imposing street wall. This is also true of other areas of the conservation including King Street, which has a rich variety of buildings due to the renewal and repair of individual properties over a long time period. John Dalton Street however has been subject to more radical development and few of the buildings now have the narrow frontage that characterises the remainder of the conservation area.

Encouragement of variety will help to maintain the character of the area. Where buildings need to be replaced, high quality modern designs, taking cues from the remaining buildings are promoted. Building proposals should be designed to enhance the existing quality of the built environment.

Albert Square Conservation Area Declaration

The Albert Square Conservation Area is bounded by Princess Street, Cooper Street, Kennedy Street, Clarence Street, Bow Lane, Tib Lane, Cross Street, John Dalton Street, Deansgate, Lloyd Street, Jackson's Row, Central Street, Manchester Central Library and Manchester Town Hall Extension.

It contains many listed buildings, including the Grade I Listed Town Hall, but also contains a number of more recent buildings such as Heron House. There is much variety in the building materials used in Albert Square. Generally, buildings on the eastern side of the Square are built of yellow stone whilst those on the west side, opposite the Town Hall, are finished in red brick. This helps to emphasise the civic importance of the Town Hall.

The principal characteristic of the conservation area is the view looking east along Brazennose Street which focuses on the dominant tower of the Town Hall, framed by commercial buildings on either side.

A large amount of the Conservation Area in particular around the Town Hall and on Brazennose Street is pedestrianised.

The architectural emphasis of corners is a characteristic of Manchester buildings which contributes to the urban design character of the city centre. It is evident in the Albert Square area and its use in new developments will therefore be encouraged.

Designers should respect the architectural character of the existing historic buildings and create proposals which harmonise with them. This does not mean producing pastiche or a copy of an old building, since each building should have a vitality of its own and reflect the period in which it is built.

Deansgate Conservation Area Declaration

Deansgate Conservation Area includes much of the area surrounding Peter Street and the junctions of Deansgate with both Quay Street and Bridge Street. The area is situated on ground, which is mostly flat, although there is a gentle slope down Peter Street in a westerly direction towards the river.

Peter Street, and its continuation into Quay Street, is the most important junction in the area. Acute and oblique angles affect the plan form of buildings; since land in the city centre is at a premium, buildings totally cover their site and as a result more interesting buildings occur, many with corner entrances which are typical of Manchester.

Generally, buildings in the area display the Manchester characteristic of a tri-partite subdivision of the elevations, consisting of an over-large ground floor, a less highly modelled middle section and a varied top level seen against the sky.

Buildings on Peter Street, Quay Street and part of Deansgate are of different ages and styles but retain a positive relationship with one another. Where redevelopment proposals are put forward, the City Council will seek designs which are consistent with the character of surrounding buildings.

Legislative requirements

Section 66 of the Listed Building Act 1990 provides that in considering whether to grant planning permission for development that affects a listed building or its setting the local planning authority shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

Section 72 of the Listed Building Act provides that in the exercise of the power to determine planning applications for land or buildings within a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.

S149 (Public Sector Equality Duty) of the Equality Act 2010 provides that in the exercise of all its functions the Council must have regard to the need to eliminate

discrimination, advance equality of opportunity and foster good relations between person who share a relevant protected characteristic and those who do not. This includes taking steps to minimise disadvantages suffered by persons sharing a protect characteristic and to encourage that group to participate in public life. Disability is among the protected characteristics.

S17 Crime and Disorder Act 1998 provides that in the exercise of its planning functions the Council shall have regard to the need to do all that it reasonably can to prevent crime and disorder.

Regeneration

Regeneration is an important planning consideration as the City Centre is the primary economic driver of the region and is crucial to longer term economic success. There has been some regeneration in and around Parsonage Gardens over the past 20 years through private and public sector investment with the refurbishment of Arkwright House.

More recently, permission has been granted for the refurbishment and extension of Kendals, the construction of a 19-storey building at Alberton House and the construction of a mixed-use building of between 34 and 45 storeys on the Albert Bridge House site. The development would contribute to the area's transformation and regeneration.

There is a high demand for commercial space in the City Centre and demands for workspace are changing. This includes a greater focus on communal and amenity spaces, health and wellbeing and sustainability. There are office schemes offering over 10,000 sq. ft floor plates with good sustainability credentials but there is a gap in the market for smaller occupiers who want the same sustainability credentials.

The proposal would help meet this identified demand, offering greater flexibility on floor plate sizes with enhanced sustainability. It offers tenants the opportunity to move to different spaces in the building if their business grows.

The St Mary's Parsonage SRF identifies this as a prime location, close the River Irwell, heritage assets and Parsonage Gardens. However, the area does not maximise its potential. Development has occurred on a piecemeal basis and has not addressed the constraints and issues that affect the whole area. It proposes a commercially led mixed use neighbourhood, with high quality public space and leisure and retail facilities which create a vibrant destination. It will be a place where people live, work, relax, socialise.

The site contains historic buildings that have been changed and altered. The proposal would address these issues, restore the retained part of the listed building, and provide a positive use that benefits the surrounding area. The ground level activity and improved connectivity would integrate the proposal into the urban grain and enhance its contribution to Motor Square. The improvements would create a more vibrant and safer pedestrian environment and improve the impression of the area for visitors.

The development would deliver significant economic and social benefits. It would create 250 FTE jobs over the 2.5-year build period and a further 80 indirect and induced FTE jobs, equivalent to an average of 135 FTEs. Total net GVA from construction would generate around £25 million in the local economy through the direct, indirect and induced impacts. A condition for a local labour agreement should ensure that the full benefits of the proposal are realised.

Once occupied, the employment space could support 410 FTE jobs, generating a GVA contribution of £35.8m each year, wages of £13.1m and contribute £2.7m of national insurance and income tax.

The scheme would deliver additional business rates of £6.6m every ten years. There would be a range of jobs, including highly skilled roles, graduate-relevant opportunities, and entry-level roles helping to drive economic growth and productivity and deliver inclusive growth.

The proposal would use the site efficiently and effectively in line with Paragraph 119, 120(d) and 124 of the NPPF. It would deliver modern commercial space for which there is known demand. It would be close to major transport hubs and promote sustainable economic growth. It would complement and build upon the City Council's current and planned regeneration initiatives

Design Issues, relationship to context and the effect on the Historic Environment.

The effect of the proposal on key views, listed buildings, conservation areas, scheduled Ancient Monuments, archaeology and open spaces has been addressed.

Section 16 of the NPPF establishes the criteria by which planning applications involving heritage assets should be assessed and determined. Paragraph 197 identifies that in determining applications Local Planning Authorities should take into account the following considerations:

- The desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation.
- The positive contribution that conservation of heritage assets can make to sustainable communities, including their economic viability.
- The desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 189 states that heritage assets are an irreplaceable resource, and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations

Paragraph 199 advises that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight

should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 200 advises that any harm to or loss of a designated heritage asset requires clear and convincing justification. Substantial harm or loss should be exceptional and substantial harm to or loss of designated heritage assets of the highest significance, including grade I and II* listed buildings should be wholly exceptional.

Paragraph 202 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

Impact on the Grade II Listed former Carriage Works Complex

The listed complex at 31-33 King Street West includes a carriage works on Garden Lane, warehouse at 3 Smithy Lane and a showroom 31-33 King Street West known as Reedham House. They are listed as a complex under one listing. It also includes a non-listed building at 27-29 King Street West and a bridge link between the buildings and to an adjacent building.

27-29 and 31-33 King Street West would be demolished meaning part of the listed complex would be lost. 27-29 King Street West doesn't make a positive contribution to the Conservation Area, is not listed and its loss would not cause harm to any of the heritage assets affected.

Reedham House has some historic and architectural interest in its own right and is part of the listed complex. Its loss could have a detrimental effect on the retained buildings and would affect the significance of the listed complex. The extent of this harm and the public benefit of the scheme therefore needs to be carefully assessed.



The complex provides evidence for the spread of small-scale industry across Manchester, and an insight into the form and functioning of carriage works nationally. Reedham House contributes to understanding of the history and evolution of the area.

The proposed 14 storey building could affect the legibility of the rest of the former carriage works complex.

Reedham House is historically part of the whole complex and was constructed in 1849. However, it has been in separate occupation and activity since the late 1870s. It is understood that after 1870 it was used as a brass works, leather goods factory and tea & coffee warehouse.

It was totally rebuilt behind its remodelled façade in 1926. Much of the fabric was subsequently damaged and repaired in the post-war period. Some more modern reconstruction has affected the rear elevation.

Reedham House has been changed extensively and almost nothing remains of the pre-1926 structure. Floor levels, window openings and other items were changed in 1926 and subsequent refurbishments have further altered the interior and exterior. The extent to which the original use of this part of the listed building complex operated cannot now be readily understood.

The interior is not original and is void of features. The elevations have been altered extensively and a majority of the windows and brickwork and stonework of the upper levels are the only remaining historic features (1926 or earlier). Windows were repositioned in 1926 when the height of the building was increased so are no longer in their original openings. The basement fabric appears historic. The shop fronts are modern and are not in keeping with the character of the listed building and have a detrimental impact.

The rear of Reedham House faces an internal courtyard and has been recently substantially rebuilt at ground floor level. It is in poor condition with several large air-conditioning units, UPVC windows, plastic rainwater goods and solid externally mounted roller shutters.

All courtyard elevations have suffered from previous works that adversely affect the listed complex with blocked up windows, concrete fire escape stairs that obscure historic cobbles and paving, cobbles lost or covered and UPVC windows and doors. Many features are lost, damaged or obscured. A non-original bridge interrupts the arched underpass entrance into the courtyard.

All buildings are almost entirely inaccessible for disabled people and the lift in Reedham House does not access all floors. There are internal level changes on most floors.

A poor-quality brickwork extension on St Mary's Parsonage obscures original fabric and features.

The interior of all buildings in the listed complex have suffered from inappropriate works that obscure almost all historic fabric including walls, floors, and ceilings. However, the carriage works, and warehouse and the courtyard between the buildings retain original and historic features and features that can be reinstated and repaired. All retained buildings have also been neglected and would require reuse and restoration. The proposals include the full restoration of these retained buildings, works that would greatly improve the appearance and condition of the courtyard and increase usability by providing disabled access.

An options appraisal has assessed the retention of Reedham House, including retaining its façade. Retaining and extending Reedham House at roof level with an extension to the side would require alterations and demolition to the original fabric of the carriage works, would obscure original fabric, and would result in a larger more prominent extension.



Retaining the façade of Reedham House but demolishing the interior would result in a much bulkier extension and could require alterations to window proportions to

ensure level access between buildings further changing the appearance of the retained elevation. None of the five alternative options assessed met all of the objectives for the development as none were considered to be viable or of appropriate scale and form and quality.



The proposals would cause a high level of less than substantial harm to the Grade II Listed complex as a building within the complex would be lost. The building that would be lost, although part of the original complex as a showroom, has been altered significantly and its loss would allow the restoration and reuse of the retained buildings of the complex.

The impact of the proposal on the setting of surrounding listed buildings and the Parsonage Gardens and Deansgate Conservation Areas would also be at the higher end of less than substantial and the impact on the St Anns Square and Albert Square Conservation Areas would be minimal.

There are no World Heritage Sites nearby. Sections 66 and 72 of the Listed Buildings and Conservation Areas Act 1990 requires members to give special consideration and considerable weight to the desirability of preserving the setting of listed buildings and to the desirability of preserving the setting or preserving or enhancing the character or appearance of a conservation area when considering whether to grant planning permission for proposals that affect it.

Development decisions should also accord with the requirements of Section 16 of the National Planning Policy Framework which notes that heritage assets are an irreplaceable resource and emphasises that they should be conserved in a manner

appropriate to their significance. Of particular relevance to the consideration of this application are paragraphs 189, 197, 199 and 202.

The NPPF establishes a clear hierarchy of significance for heritage assets, derived from their designated status. The fundamental objective is to avoid compromising designated heritage assets, such that any potential 'harm' from a development must be balanced against the potential advantages of the public benefits that may outweigh any harm (paragraphs 201-202).

The NPPF (paragraph 193) stresses that when considering the impact of a proposal on the significance of a designated heritage asset, great weight should be given to the asset's conservation. This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm. Significance of an asset can be harmed or lost through alteration or destruction or by development within its setting. As heritage assets are irreplaceable, any harm or loss should clearly and convincingly be justified.

Paragraph 202 requires this to be weighed against the public benefits including, where appropriate, securing its optimum viable use. Where a proposal would have an adverse impact on the historic environment the of harm must be outweighed by the public benefits brought of the scheme.

Paragraph 20 of the NPPF Planning Practice Guidance on the Historic Environment states that public benefits may follow from many developments and could be anything that delivers economic, social or environmental progress as described in the National Planning Policy Framework (paragraph 127).

Impact on Designated and Non-Designated Heritage Assets and Visual Impact Assessment

The impact of the proposal has been assessed from a number of agreed viewpoints. Nine were identified as Verified Views, to help to understand the impact of the proposals on the immediate and wider environment around the site.

Each verified view is shown as a baseline condition, followed by an image showing the proposal and a cumulative image with nearby consented and proposed developments.

This Heritage Assessment Townscape and Visual Impact Assessment (TVLA) used Historic England's updated policy guidance on the Setting of Heritage Assets (Historic Environment Good Practice Advice in Planning Note 3, Second Edition). (December 2017). The magnitude of the impacts (both beneficial and adverse) are identified in the assessment as high, medium, low, negligible or neutral.

TVLA and Heritage Assessment viewpoint locations

9 views have been provided and analysed to provide a qualitative assessment of the effects of the proposal on heritage assets. Cumulative impacts are shaded in colours with wire lines.

View 1: Looking North from Bridge Street



Existing view



cumulative view



Proposed view

Baseline – There are a variety of architectural styles and forms, with medium rise buildings (13 storeys) and smaller buildings (2-4 storeys) with open sky views. No building dominates or makes more than a neutral contribution to the Conservation Area. Many around Motor Square were not originally built to surround a public square. Motor Square includes mature trees, and a tree partly obscures Reedham

House. Other post-war and modern buildings fill the backdrop, and none are of heritage value.

Townscape Impact -The view would alter substantially with open views of the sky reduced. The proposal would appear more prominent than existing and proposed developments. It would create a landmark building with a large open space in front of it. The chamfered edge reflects the historic bend in the street. The ground floor treatment would provide active street frontages. It would be no taller than the 19 story Alberton House of the 18 storey Albert Bridge House. The colour palette would reflect surrounding buildings. The impact would be Minor Beneficial.

Impacts on Heritage Assets – The loss of Reedham House would be highly noticeable and a much taller element introduced on King Street West. It would be seen as part of the continuing developments of the St Mary’s Parsonage SRF area. It would alter the character and appearance of the Parsonage Gardens Conservation Area due to the loss of Reedham House and would have an Adverse impact.

Verified View 2: Looking west along King Street West



Existing view

Cumulative view



Proposed view

Baseline – This is from Southgate along King Street West with the carpark on the right. Albert Bridge House is in the distance. On the left is a red brick 19th century commercial building.

The front elevation of Reedham House is on the right and has more character than the buildings to either side. The townscape is dominated by the car park, with a combination of ages, styles, massing, and form.

Townscape Impacts – The eastern elevation could appear prominently in this view; but would be much less prominent should the Kendals scheme be implemented. The 10th floor terrace would line through with the Kendals building and it would sit in a newly created context.

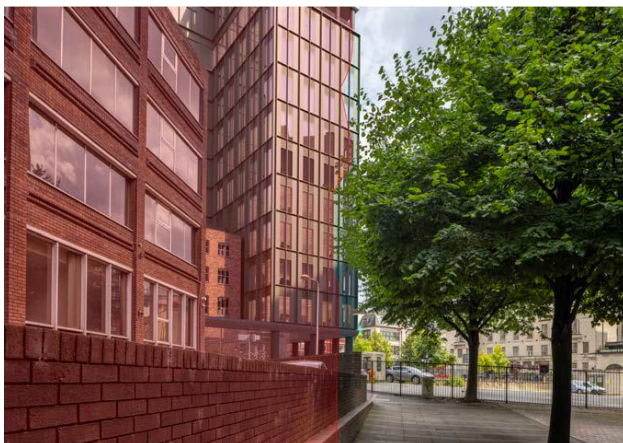
The proposals would be Minor Beneficial, providing a quality building in keeping with proposed development.

Impact on Heritage Assets – Reedham House would be lost, and the impact of the building would be Neutral as views are limited. The heritage asset on the left would remain the most prominent feature.

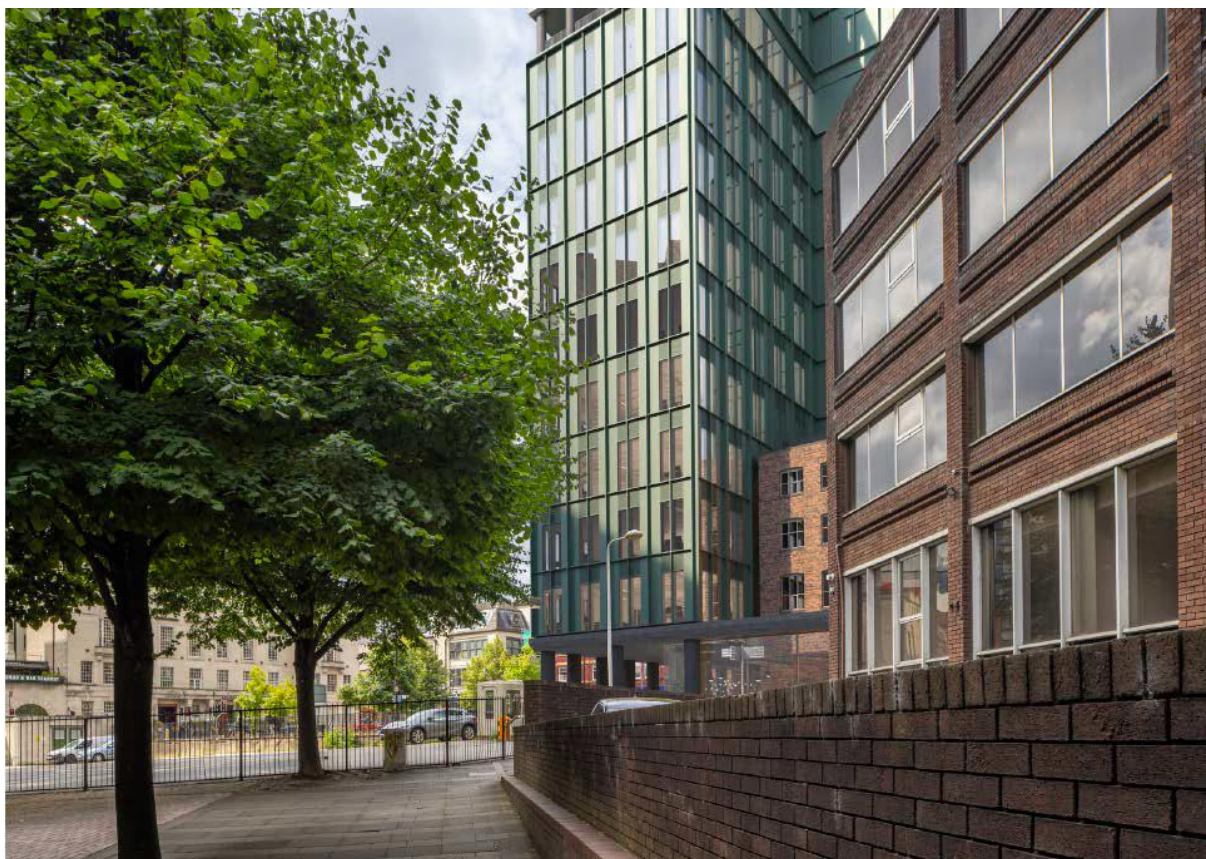
View 3: Looking southeast alongside Alberton House



Existing view



Cumulative view



Proposed view

Baseline - This view is dominated by the mature trees to the right and Albertton House to the left with Motor Square and the Grade II listed Masonic Hall visible. The corner of 27-28 King Street West and the carriage works is glimpsed behind the tree on the left in the middle distance.

It looks into the Parsonage Gardens conservation area. Other than Albertton House, building heights are around 3-5 storeys.

Townscape Impacts – The building would be the tallest until Albertton House is implemented. It would provide an active frontage to St Marys Parsonage and Motor Square. It would be in keeping with the aspirations of the St Mary's Parsonage SRF and would be Minor Beneficial due to its quality

Impact on Heritage Assets- the proposal would replace the 1950s extension to the Listed complex and would have a Minor Beneficial impact on the Parsonage Gardens Conservation Area and the Listed Carriage Works and the setting of the Masonic Hall.

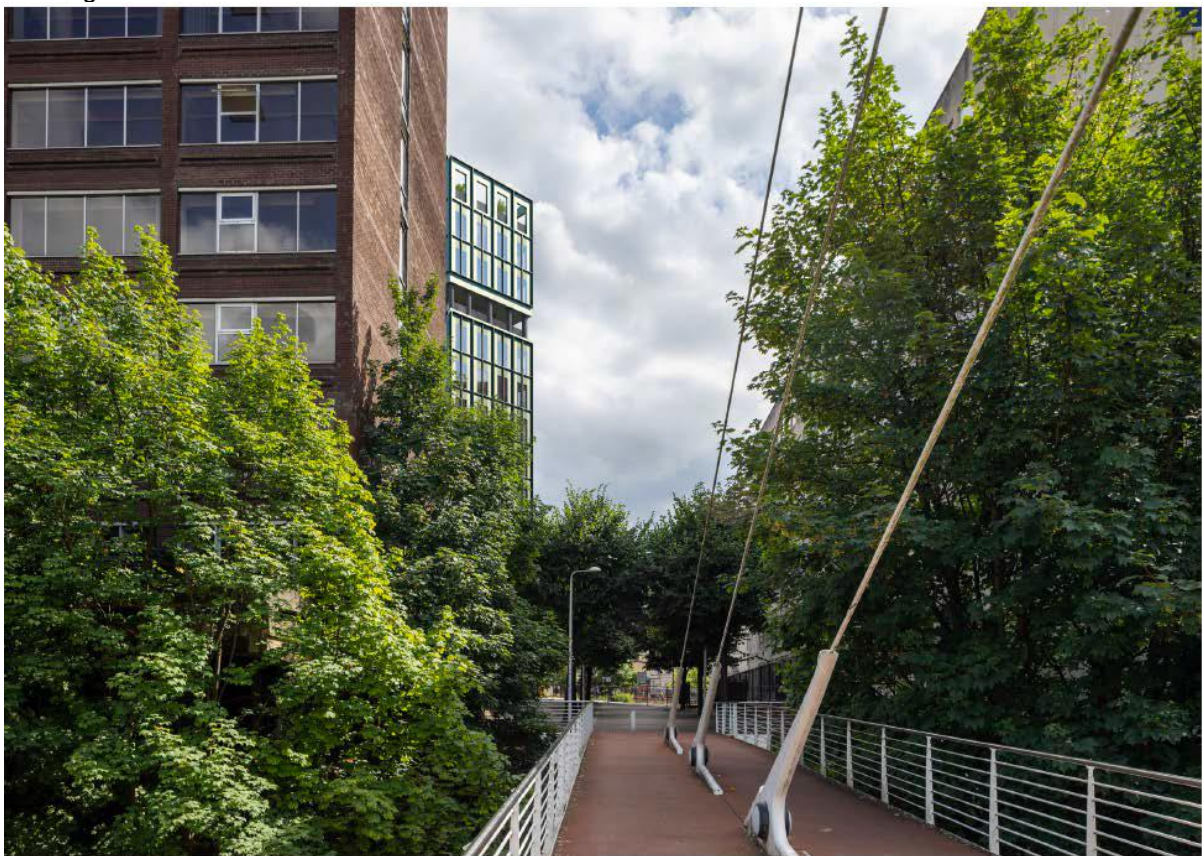
Verified View 4: Looking east from Trinity Bridge



Existing View



Cumulative view



Proposed View

Baseline – This view is dominated by the bridge, Alberton House and trees. The proposals would appear smaller than the existing and proposed Alberton House.

Townscape Impacts- The proposal could be hidden by the Alberton House development but would add interest to this view if visible. The effect would be Moderate Beneficial or Neutral depending on whether it is visible.

Impacts on Heritage Assets- None

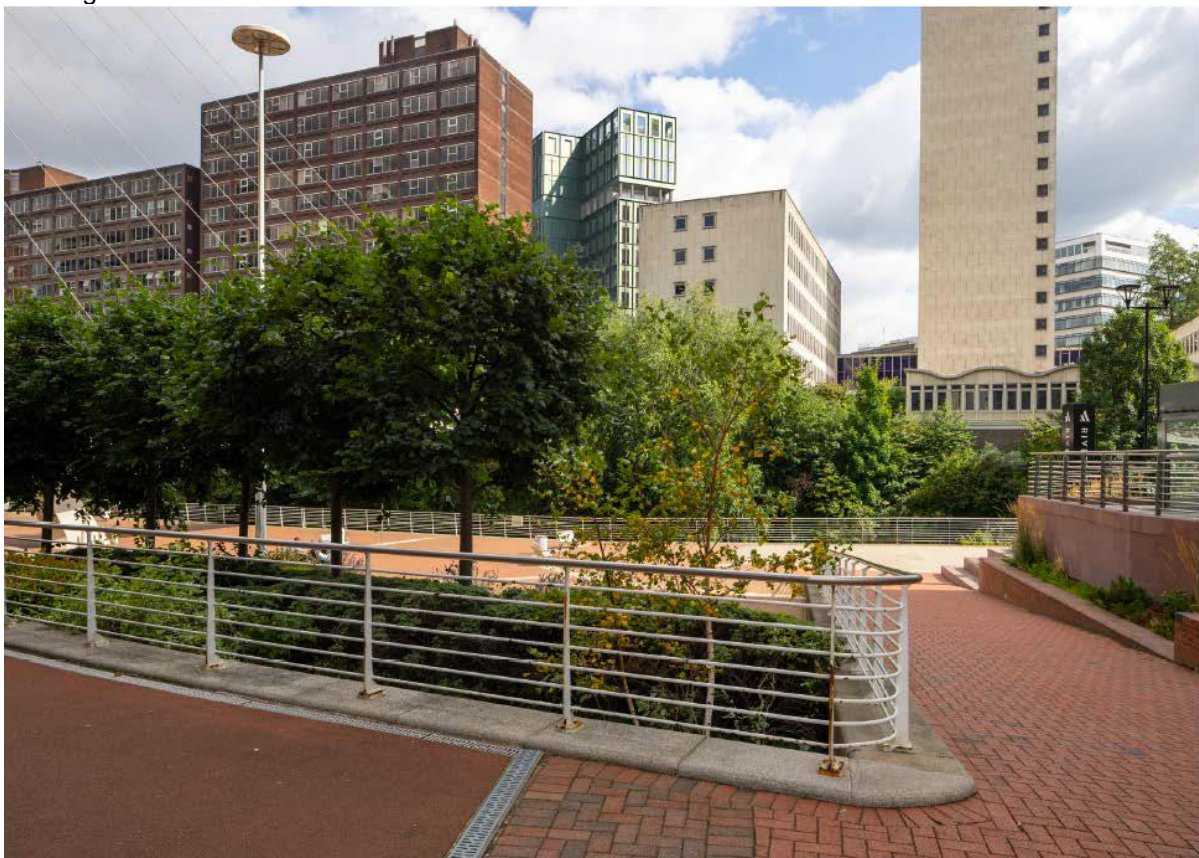
Verified View 5: View looking east from beyond the River Irwell



Existing View



Cumulative View



Proposed View

Baseline - The cables of Trinity Bridge are visible to the left in front of Cardinal House with Alberton House, and Albert Bridge House, all post-war buildings.

Townscape Impacts - The proposal would be in the backdrop in keeping with existing massing and heights and would add a building of high-quality. The redevelopment of Alberton House would obscure the proposal, but when visible it would make a positive contribution.

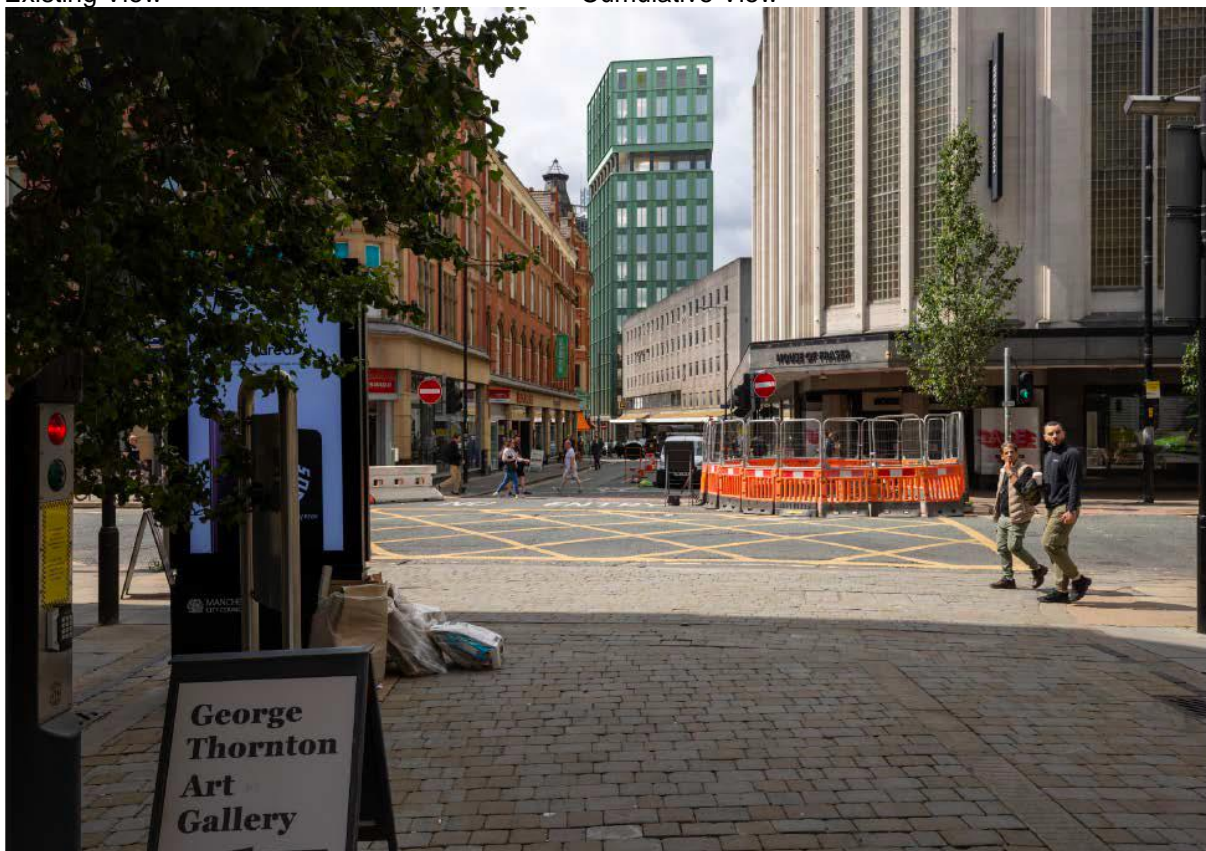
Impact on Heritage Assets - The overall effect would be Moderate Beneficial or Neutral depending on whether it is visible.

Verified View 6: Looking west from King Street



Existing View

Cumulative View



Proposed View

Baseline – The proposal would be visible in the centre, partly concealed by the MSCP.

Townscape Impacts - The proposal would not be out of context with Kendals in the foreground. The 10th floor terrace breaks up the massing. The mass of Kendals helps to minimise its impact and the consented Kendals scheme would largely conceal it.

Impact on Heritage Assets - The listed building would be lost but the impact of the proposal would be a neutral on historic assets and the Conservation area as Reedham House is barely visible. Kendals would remain the most prominent feature.

Verified View 7: Looking south from St Mary's Parsonage



Existing View



Cumulative View



Proposed View

Baseline – The Grade II Listed 3 St Mary's Parsonage is in the foreground and Alberterton House and Albert Bridge House are visible.

Townscape Impacts – The proposal would be prominent in the backdrop to the listed building and would change the townscape.

Impact on Heritage Assets – The architectural qualities of the listed building would be readable, and a quality building would be introduced. The setting of the listed building would be altered. The quality of design and the colours would minimise the impact but there would be an adverse impact on the listed building and conservation area.

View 8: New Bridge Street looking southwest



Existing View



Cumulative View



Proposed View

Existing Baseline – 3 St Mary’s Parsonage is in the centre. On the left is a car park and to the right is Alberton House and Albert Bridge House. The setting of the listed buildings contains a variety of styles and massing.

Townscape Impacts – This townscape has old and new buildings and different quality. The listed buildings mean that the quality of townscape is good. The proposal would appear behind two listed buildings and would alter their setting. The materials and tones should help to minimise its impact and ensure the listed buildings remain prominent. It shows the re-opening of the Garden Lane entrance, the removal of the roller shutter and the improved listed warehouse. The cumulative view demonstrates how the area could change with a new scale.

Impact on Heritage Assets – The improvements to the listed warehouse are visible and the listed buildings would remain prominent, However, the proposal would have an adverse impact.

View 9: Looking south from Parsonage Gardens



Existing View



Cumulative View

Existing Baseline – The Grade II Listed Arkwright House dominates the view and Alberton House can be seen across Parsonage Gardens.

Townscape Impacts – The proposal would not be visible

Impact on Heritage Assets – None.

The proposal could enhance Motor Square and the surrounding townscape and conservation area, providing a landmark building on an open space with active frontages wrapping around the site and improvements to the public realm.

There are instances where the proposal would have a large presence in the backdrop of listed buildings, but this is mitigated in part through its design which aims to ensure that the red brick listed buildings remain prominent.

The proposal would add a building of high quality in the context of conservation areas to an already varied area and would enhance Motor Square.

Public Benefits

The high level of less than substantial harm caused would be outweighed by the following public benefits:

- The development would create 250 full time equivalent jobs over the 2.5-year build period plus and a further 80 indirect and induced FTE jobs, equivalent to an average of 135 FTEs. Total net GVA from the construction phase would generate around £25 million in the local economy through the direct, indirect and induced impact of the construction phase.
- Once fully occupied, the employment space could support 410 FTE jobs, generating a GVA contribution of £35.8m each year, wages of £13.1m and contribute £2.7m of national insurance and income tax. The scheme would deliver additional business rates of £6.6m every ten years.
- The complete restoration and repair of all elevations and roof of the retained elements of the listed building with modern insertions replaced in a manner that complement the character and original features.
- The reinstatement of the Garden Lane entrance to the Carriage Works with a connection through the historic courtyard to the main building reception.
- The existing brickwork would be repointed where necessary.
- The harmful clutter and non-original alterations in the inner courtyard would be removed to expose its original features and restore its architectural detail.
- Internally, all listed structures including existing columns would be retained and sensitively repaired. Modern wall linings would be removed, and the original brickwork revealed and sensitively repaired where needed.
- Modern ceilings would be removed to allow the existing floorboards and supporting structures to be revealed.
- New lighting and services would be exposed and carefully threaded through the existing fabric minimising damage by using existing openings where possible.
- The re-opening of the courtyard as the centre of the complex and reactivating Garden Lane with the re-opening of the entrance on the northern side.

- Restoring the western elevation of the complex with the replacement of the ground floor extension which detracts from the listed complex.
- The improved public realm would provide routes and views through to the Carriage Works and Warehouse.
- A new feature lift and glazed roof would be provided to the courtyard, greatly improving access and enabling the space to be used to extend the reception space. The upper levels of the buildings are not currently accessible by lift.
- The development would introduce a long term economically viable use.
- It would activate Motor Square and surrounding streets.
- The development would support Manchester's Zero Carbon ambitions and deliver a sustainable development
- The office space and the ground floor commercial uses would deliver opportunities and amenities that would benefit local people
- The public realm would improve permeability with new public realm at Garden Lane and Smithy Lane, and improved frontages to St Mary's Parsonage and King Street West / Motor Square and improve the SRF area.
- The applicant is committed to maximising the recruitment of local residents and a Local Labour Agreement would be provided.
- 4,849 sqm of office floorspace would make an important contribution to the pipeline of office floorspace in the City.

It is considered that the benefits of the proposal would outweigh the less than substantial harm caused to the affected heritage asset by the above-described works and are consistent with paragraphs 196 and 197 of the NPPF and address sections 66 and 72 of the Planning Act in relation to preservation and enhancement. The benefits of the proposals are described above.

Given the above, the works are considered to be consistent with the above identified policies.

Viability

A viability report has been made publicly available through the Council's public access system and has been independently assessed, on behalf of the Council. It includes an assessment of whether the development would be viable if the Grade II Listed former carriage showroom at 31-33 King Street West was retained.

Five options were prepared, and two were assessed. Option A included the retention and refurbishment of 31-33 King Street West and Option B included the demolition of 31-33 King Street West.

The conclusion of the independent assessment was that Option A results in a negative profit and would not be viable. Option B results in a profit margin of 9.9% which is slightly below the 15% target. If a yield of 4.75% is adopted, which the applicant has included in their appraisal, it results in a profit margin of 15.88% on Cost. The applicant's conclusions are therefore agreed, and Option B is the only viable option.

CABE/ English Heritage Guidance on Tall Buildings

One of the main issues to consider is whether a 14-storey building is appropriate in this location. This would be a tall building and should be assessed against the relevant policies in the NPPF and Core Strategy that relate to Tall Buildings and the Tall Buildings Guidance of English Heritage and CABE.

Design Issues, relationship to context, including principle of tall building in this location and the effect on the Historic Environment This considers the design in relation to context and its effect on key views, listed buildings, conservation areas, scheduled Ancient Monuments, Archaeology and open spaces. The key issues is its appropriateness and its impact on the setting of the conservation areas and affected listed buildings and non-designated heritage assets which lie within 500m of the site. The design has been discussed with Historic England and Places Matter and public engagement has taken place

Tall buildings can play an important role in shaping perceptions of an area. The Core Strategy supports tall buildings that are of excellent design quality, are appropriately located, contribute positively to sustainability and place making and deliver significant regeneration benefits. However, they should relate sensitively to their context and make a positive contribution to a coherent city/streetscape. Sites in the City Centre are considered to be suitable where they are viable and deliverable, particularly where they are close to public transport nodes. These parameters have informed the SRF's which have promoted regeneration in the City Centre over the past 20 years. Taller buildings should; relate to key nodal points and gateways, key vistas and public spaces, positively contribute to the skyline and deliver significant, high quality public realm to create a high quality, sustainable neighbourhood.

The area has a broad range of building heights and styles representing different architectural eras. There are historic buildings including Kendals, The Freemason's Hall and Arkwright House. Albert Bridge House is a more modern office development. There are large historic former warehouses around Parsonage Gardens and there are lower mainly Victorian buildings around Bridge Street and King Street West, representing remnants of an older denser urban grain. The area is characterised by a mix of building scales and contexts. Parsonage Gardens is surrounded by a diverse range of buildings and styles, but the feels dislocated from more vibrant areas nearby.

The SRF indicates that the site could accommodate taller buildings in high-quality public realm. The works to the retained listed structures would positively contribute towards the character and quality of the Parsonage Gardens Conservation Area and introduce long term viable uses. The tall new build element would not be as tall as the current Albert Bridge House and immediately adjacent would be the approved 14-

storey Kendals development. The redevelopment of Alberton House would be 19-storeys.

Part of the listed building complex would be demolished, to enable the retention and restoration of the remainder of the original elements of the complex. Part of the site has been vacant for some time and the structures have been significantly modified, with some features removed and are in a poor condition.

The design parameters in the SRF require tall buildings to respond to the historic environment through a visual impact analysis and assessment and ensure that micro-climatic effects in terms of wind and sunlight / daylight, do not have an adverse effect on safety, comfort, or amenity. The location of the tower would replace the less significant parts of the listed complex, allowing the reopening of the courtyard, allowing the retention and restoration of the oldest and most significant buildings within the complex and by wrapping around to the west at ground floor to create an open, glazed space that would better reveal the listed building behind. Site specific considerations have informed the design including surrounding developments, accessibility and listed buildings and the relationship with existing and future context.

The Core Strategy requires tall buildings to help to create a unique, attractive, and distinctive City. They should enhance the character and distinctiveness of an area without adversely affecting valued townscapes or landscapes or intruding into important views. Parts of the site such as the toilet block on St Mary's Parsonage, the deteriorating condition of the listed buildings and some surrounding streets undermine the quality and character of the townscape and the area lacks coherence.



The proposal would improve the area and use the site efficiently. The interface with the public realm and Motor Square would be improved through landscaping and adjacent clear glazed active ground floor frontages.

The elevations aim to respond to the surrounding context. The base includes a colonnade of circular textured columns, with inset glazing to create active frontages. The glazing would respond to the direction of travel to Trinity Bridge. The façade would be clad in an anodised aluminium rainscreen cladding and extruded fins create depth. The green colour would relate to the reddish/brown brickwork at the Carriage Works and Warehouse. The tone of the concrete lintel aims to set off the existing brickwork, and the anodised metal cladding to the circular columns would have an embossed pattern to tie-in with stonework to the warehouse on Smithy Lane.

The proposal would cause 'less than substantial harm' to the Grade II Listed Carriage works that would be justified by the public benefits of the proposals described above.

Architectural Quality

The key factors to evaluate are the building's scale, form, massing, proportion and silhouette, materials, and its relationship to other structures.

The proposal would transform the complex. The character of Carriage works, and Warehouse buildings would be restored with concealed features revealed. 31-33 King Street West which is part of the listing would be demolished. However, it is not as significant as the buildings that would be retained and has been altered very significantly.

It would not be viable to refurbish and extend it and its retention would prevent the quality of form of the new build and the full restoration of the retained listed buildings.

The ground floor of the new build has been set back to create a colonnade along King Street West. Street trees would be planted, and rain gardens would enliven Smithy and Garden Lane. St Mary's Parsonage and King Street West would be improved. The building would be a landmark on the Square.

The area contains different forms of architecture, with red brick and buff stone being the main material, mixed with more contemporary buildings in concrete. The green tones of the anodised rainscreen cladding and the earthy pigment of the precast concrete would have a positive relationship with existing buildings.

The building has a clear top, middle and bottom. The base has a colonnade of circular textured columns with clear glazing to create active frontages. Extruded fins would create depth in the façade. The glazing line is set back to respond to the direction of travel to Trinity Bridge. The main body of the facade is clad in the green anodised aluminium rainscreen cladding. The precast concrete lintel to the base wraps around the ground floor with a tapered wall forming a single storey glass extension which connects to the 1840's Carriage Works.

The fully glazed extension on St Mary's Parsonage would provide an active frontage to the ground floor of the Carriage Works Building, which has no window openings. It

would connect to the Grade II listed building via a structural glazed roof light and would be clad in precast concrete.



The brickwork of the carriage works would be re-pointed, historic lintels would be cleaned, and PVC windows would be removed and replaced, and roller shutters and other detritus would be removed. The historic Carriage Works entrance would be reinstated, with glazed entrance doors to the archway.

The layout and transparency of the ground floor glazing would maximise daylight and allow views into ground floor areas increasing passive surveillance and improving security whilst animating the street and would improve the streetscape.

Credibility of the Design

Proposals of this nature are expensive to build so it is important to ensure that the design and architectural intent is maintained through the design, procurement, and construction process. The design team has extensive experience in restoring listed

buildings and delivering office buildings in similar locations and would ensure that the development is high quality and deliverable.

The detailing is well-considered, the materials are high quality materials, with good quality natural light, ventilation and acoustics, active frontages and welcoming entrances and communal spaces including external terraces and public realm at ground level which includes public accessibility and fully accessible buildings.

Contribution to Improving Permeability, Public Spaces and Facilities and Provision of a Well-Designed Environment (including Age Friendly Provision):

Proposed public realm layout

The Core Strategy requires tall buildings to create an attractive, pedestrian friendly environment. The proposal would secure the successful regeneration of the site and deliver placemaking objectives. There would be wide pavements with natural stone paving to King Street West and St Mary's Parsonage.

The hard and soft landscaping, including trees, would improve biodiversity. Four street trees incorporating SUD activated tree pits, would be planted on King Street West.

The tree planting on King Street West and within the rain gardens on Smithy Lane and Garden Lane would offer shade and reduce the effects of urban heat island and the permeable surfaces would contribute to a sustainable drainage strategy. Pedestrian routes would be clearly defined and well lit.

The design would promote health & wellbeing and be suitable for all due to the level access throughout the site, the areas of shade, the provision of lighting and a variety of seating. The final details would be agreed by condition.

Conclusions in relation to CABA and English Heritage Guidance and Impacts on the Local Environment.

On balance, it is considered that the applicant has demonstrated that the proposal would meet the requirements of the CABA and HE guidance as well as the Core Strategy policy on Tall Buildings.

Relationship to Transport Infrastructure and cycle parking provision

This is a sustainable location close to free bus, bus, tram, and train. There are multi storey car parks within 600m, and leaseholds can be arranged for contract spaces. The nearest City Car Club bay is within 50m on St Mary's Parsonage. No parking spaces are proposed, and none exist. A Travel Plan would make occupiers aware of sustainable options. The Transport Statement concludes that the overall impact on the local transport network would be minimal.

92 secure cycle parking spaces would be provided in the basement of the buildings and would be accessed off Garden Lane and Smithy Lane. Drop off, servicing and loading would be from a dedicated loading bay on St Mary's Parsonage.

Sustainability / Climate Change: Building Design and Performance (operational and embodied carbon)

There is an economic, social, and environmental imperative to improve the energy efficiency of buildings. Larger buildings should attain high standards of sustainability because of their high profile and impact.

The energy strategy responds to the City's Climate Emergency declaration and has set out how the scheme contributes to Net Zero Carbon targets through operational and embodied carbon.

An Environmental Standards assessment of physical, environmental, social, and economic effects in relation to sustainability objectives sets out measures that could be incorporated across the lifecycle of the development to ensure high levels of performance and long-term viability and ensure compliance with planning policy. Energy use would be minimised through good design in line with the Energy Hierarchy to improve the efficiency of the fabric and use passive servicing methods.

Operational Carbon

The Core Strategy requires developments to achieve a minimum 15% reduction in CO₂ emissions. The proposals would achieve and exceed the 15% reduction with a 39.24% betterment over the current Part L.

The proposals would comply with Part L 2013, would achieve an Energy Performance Certificate ("EPC") Target A rating of A15 and would have a Building Emission Rate of 2.09 kgCO₂/m²/year (against a target of 3.44 kgCO₂/m²/year) and a Building Primary Energy Rate of 22.47 kWh/m²/year (against a target of 36.73 kWh/m²/year), achieve BREEAM 'Very Good' and NABERS UK 5 stars.

47.1% of all energy use would be provided by renewables as follows:

Photovoltaics - 35.34%

Air Source Heat Pumps - Heating - 8.46% and Domestic Hot Water - 3.29%

The new build would follow the principles of the Energy Hierarchy with enhanced fabric parameters to limit heat losses: External Walls – New fabric 0.15 Watts-per-metre-square-kelvin (W/m²K); Existing fabric 1.70 W/m²K; Ground/ Exposed Floors – New fabric 0.12 W/m²K; Existing Fabric 1.00 W/m²K; Windows – New fabric 1.20 W/m²K | 0.35 grams; Existing fabric 5.70 W/m²K; Doors – New fabric 0.50 W/m²K. Existing fabric 2.00 W/m²K; Air Permeability – New fabric 3.00 m³/hr/m² @ 50 Pascals; and, Existing fabric 25 m³/hr/m² @ 50 Pascals

The lighting and controls would be energy efficient and would incorporate Air Source heat pumps and Solar Photo Voltaic units to offset a portion of the outstanding energy consumption.

Building Location and Operation of Development and Climate Change Adaptation and Mitigation

Features that would contribute to achieving overall sustainability objectives would include that this is a highly sustainable location; recycling facilities would divert material from landfill and SuDs would be installed in the public realm to help to mitigate flood risk.

A net zero carbon-built environment means addressing all construction, operation and demolition impacts to decarbonise the built environment value chain. Embodied carbon is a relatively new indicator and the availability of accurate data on the carbon cost of materials and systems is evolving.

To reduce the Whole life Embodied emissions, the emphasis is on minimising the use of energy intensive materials, using local suppliers where possible, reducing traffic, and improving vehicle efficiency. Further consideration should be given to embodied carbon benchmarking relating to Circular Economy principles. This will be detailed further at the next design stage.

The proposal would make a positive contribution to the City's objectives and, subject to the ongoing decarbonisation of the grid is capable of becoming Net Zero Carbon in the medium to long term whilst achieving significant CO₂ reductions in the short term.

Whole Life Embodied emissions includes embodied carbon emissions related to demolition, materials and construction process throughout the lifespan of the building, including upfront emissions during constructions, construction transport, replacements/repairs during the operational phase and end-of-life.

During operation transport, mitigation is focused on active travel and encouraging the use of public transport through measures in the Travel Plan. To mitigate against operational energy emissions, the focus has been on improving the energy efficiency of buildings using a fabric first design approach and providing an all-electric development.

It is estimated that the whole-life embodied carbon emissions of the proposal would comply with the RIBA 2025 Climate Challenge The annual energy consumption complies with the RIBA 2025 Climate Challenge target With mitigation for the operational phase, the residual impacts would be minor.

Effect on the Local Environment/ Amenity

This examines the impact that the scheme would have on nearby and adjoining occupiers and includes issues such as microclimate, daylight, sunlight and overshadowing, air quality, noise and vibration, construction, operations and TV reception and sunlight, daylight and overshadowing

Construction

Effects would vary throughout the demolition and construction phase and the effects would be less in the completed scheme.

Operational Effects

Daylight, Sunlight and Overshadowing

The nature of high-density City Centre development means that amenity issues, such as daylight, sunlight, and the proximity of buildings to one another have to be dealt with in a manner appropriate to their context.

An assessment of daylight, sunlight and overshadowing has used specialist software to measure the amount of daylight and sunlight available to windows in neighbouring buildings. The assessment made reference to the BRE Guide to Good Practice – Site Layout Planning for Daylight and Sunlight Second Edition BRE Guide (2011). This assessment is not mandatory but is generally accepted as the industry standard and helps local planning authorities consider these impacts. The guidance does not have 'set' targets and is intended to be interpreted flexibly, acknowledging that locational circumstances need to be considered, such as a site being within a town or City Centre where higher density development is expected and obstruction of light to buildings can be inevitable.

The BRE Guidelines suggest that homes have the highest requirement for daylight and sunlight and states that the guidelines are intended for use for rooms where natural light is required, including living rooms, kitchens, and bedrooms. The daylight and sunlight at 65 Bridge Street could be affected. Other homes have been scoped out due to their distance and orientation from the site. The latest planning permission on the Council's website in relation to room layouts has informed the analysis. The Sunlight and Daylight Assessment has set out the current site condition VSC levels (including impacts from adjacent approved schemes) and how the proposal would perform against the BRE VSC targets.

Daylight Impacts

The Guidelines provide methodologies for daylight assessment, namely VSC (vertical sky component) and NSL (no skyline). VSC considers how much Daylight can be received at the face of a window by measuring the percentage that is visible from its centre. The less sky that can be seen means less daylight is available. Thus, the lower the VSC, the less well-lit the room would be. In order to achieve the daylight recommendations in the BRE, a window should attain a VSC of at least 27%.

As VSC does not consider window size, the guidance states that internal daylight distribution is also measured. This assesses how light is cast into a room by examining the parts of the room where there would be a direct sky view. Daylight may be adversely affected if, after the development, the area in a room which can receive direct skylight is reduced to less than 0.8 times its former value. Any reduction below this would be noticeable. NSL assess daylight in a whole room

rather than just that reaching an individual window and more accurately reflects daylight loss.

VSC diminishes rapidly as building heights increase relative to separation distances. As such, the adoption of the 'standard target values' is not the norm in a City Centre and the Guide recognises that different targets may be appropriate. It acknowledges that if a building stands close to a common boundary, a higher degree of obstruction may be unavoidable and is common in urban locations and in a City Centre, or an area with modern high-rise buildings, a higher degree of obstruction may be unavoidable if new developments are to match the height and proportions of existing buildings.

Sunlight Impacts

For Sunlight, the Guide should be applied to all main living rooms which have a window which faces 90 degrees of due south. Kitchens and bedrooms are less important, although care should be taken not to block too much sunlight. Sunlight may be adversely affected if the centre of the window receives less than 25% of annual probable sunlight hours, or less than 5% of annual probable sunlight hours between 21 September and 21 March; receives less than 0.8 times its former sunlight hours during either period; and, has a reduction in sunlight received over the whole year greater than 4% of annual probable sunlight hours (APSH). A scheme would be considered to comply with the advice if the base line values and those proposed are within 0.8 times of each other as an occupier would not be able to notice a reduction of this magnitude. This only applies to living areas.

BRE Targets

The Guidance states that a reduction of VSC to a window of more than 20% or of NSL by 20% does not necessarily mean that the room would be inadequately lit, but there is a greater chance that it would be more apparent. A scheme would comply if figures achieved are within 0.8 times of baseline figures. Similarly, winter targets of APSH of 4% and an annual APSH of 20% are acceptable. For the purposes of the sensitivity analysis, these values are a measure against which a noticeable reduction in daylight and sunlight would be discernible and are referred to as the BRE Alternative Target. (BRE Target within the Environmental Statement). The impacts of the development in this context are set out below.

Baseline

All impacts have been assessed against a baseline of the current site condition with any adjacent approved schemes taken into account. No consented schemes could be affected, and none have been included in the assessment (cumulative impacts). The impact on 65 Bridge Street have been assessed as follows:

Daylight Impacts

With the development in place and the results weighted to allow for the 20% reduction which would not be noticeable, the impact would be:

10 windows have been appraised to 6 rooms. In the baseline condition none achieve VSC target, because of their proximity to adjacent buildings.

6 windows would achieve a VSC that is of at least 0.8x the baseline VSC. 4 windows to 3 second floor rooms (1 LKD; 2 bedrooms) would achieve VSC values of between 14% and 16%. Whilst this is less than the BRE's aspiration target, the impact of the proposal on daylight is appropriate and balanced for the following reasons:

These impacts need to be considered in the context of the following:

- The City Centre location, emerging height and density anticipated in the locality
- None of the rooms affected achieve the BRE's VSC target in the baseline condition
- All rooms served by these windows would pass the BRE's NSL test with negligible reductions in the internal distribution of skylight.
- One room is also served by a south facing window that would not be impacted.
- All four windows would retain VSC values of between 0.74x and 0.79x baseline values. The BRE consider a reduction in VSC to 0.8x baseline figures unlikely to be noticed by room occupants.
- 0.74x is likely to be indistinguishable from the 0.8x target.

All 6 rooms appraised would pass the BRE's NSL test, experiencing no more than a negligible reduction in distribution of skylight.

Sunlight Impacts

65 Bridge Street is located to the south and its site-facing windows face north so do not receive direct sunlight over the site. The proposal would have no impact.

Overshadowing and Privacy

Motor Square is to the south of the site and receives no direct sunlight over the site.

Wind

Changes to wind can impact on how comfortable and safe the public realm is. Any changes that cannot be designed out should be minimised by mitigation. A Wind Microclimate report focused on the impact on people using the site and surrounding area. This has been assessed based on the meteorological data for the area, a terrain assessment and extensive experience of wind flow in the urban environment. The assessment used the Lawson Comfort Criteria, which seek to define the reaction of an average pedestrian to wind. The assessment excludes landscaping to provide a worst case (i.e., windiest) scenario. Potential impacts are considered on amenity spaces at and in the site including areas immediately outside any building entrances, throughfares around the building, ground level and upper-level terrace amenity.

Baseline

The site contains 3 and 4 storey buildings, shorter than those to the south. Therefore, stronger, higher-level winds would pass over the site without being brought down to ground level, resulting in relatively calm conditions. Standing wind conditions would be expected nearby with calmer sitting use wind conditions to the north and east. Strong winds are not expected to occur currently.

Proposals

Taller developments to the south (i.e., 1 Spinningfields, 3 Hardman Street, Sunlight House) would continue to provide shelter from stronger, higher-level winds and conditions are expected to be mostly suitable for the intended uses. However, it would be expected that amenity areas at ground level and upper-level terraces would be windier than desired in summer and strong wind could occur with the potential for safety concerns to more vulnerable pedestrians at upper-level amenity areas. Therefore, the following mitigation measures would be provided:

Ground level:

- 4no. trees at least 4m tall on King Street West.

Upper-Level Terrace at levels 10 and 13:

- 1.5m tall solid or 50% porous balustrade surrounding terraces.

Construction phase impacts would be negligible. Following completion and with mitigation in place, conditions would be suitable for their intended use. The final details of this can be secured by a condition.

Air quality

An air quality assessment (AQA) has considered whether air quality would change during the construction and operational phases. The site is in an Air Quality Management Area (AQMA) where air quality is known to be poor as a result of surrounding roads. Roads which may be used for construction traffic and post development are in the AQMA. Residents could experience poor air quality and vehicles travelling to and from the site could increase pollution in this sensitive area.

The site was previously developed and is close to homes. There are homes, businesses, and recreational areas (Parsonage Gardens and Motor Square) which could be affected by construction traffic and that associated with the completed development.

The potential effects during construction of dust and particulate emissions from site activities and materials movement has been assessed based on a qualitative risk assessment method based on the Institute of Air Quality Management's (IAQM) 'Guidance 2014. The assessment of potential air quality impacts from the completed scheme has focused on the predicted impact of changes in ambient nitrogen dioxide (NO₂) and particulate matter with an aerodynamic diameter of less than 10 µg/m³ (PM₁₀) and less than 2.5 µg/m³ (PM_{2.5}). Various scenarios were tested to assess

both the construction and operational impacts on air quality including construction and when the entire development is complete.

The main contributors to air quality would be from construction from dust, particulate matter and pollution concentrations generated on site, particularly from exhaust emissions from traffic, plant, and earthworks. Nearby homes could experience impacts from dust from construction. There would be emissions from construction traffic entering the site. There are also likely to be cumulative impacts from other nearby developments which will be under construction at the same time. Detailed dispersion modelling has determined whether the site is suitable for the proposal due to its roadside location in an AQMA. Good on-site practices would ensure dust and air quality impacts are not significant and should remain in place during the construction period and should be included in a condition. Subject to compliance with this condition the impacts are considered to be negligible.

The operation the impact of the development would be 'negligible' and the following mitigation measures are included in the scheme:

The development would not contravene The City Council's Air Quality Action Plan or render any of the measures unworkable.

It would extend the street canyon on King Street West but is surrounded by narrow rectilinear streets and due to this the effects are negligible.

The location of the proposal would encourage sustainable behaviours such as active travel and public transport.

Fumes would be extracted at roof level.

The energy strategy would be all electric.

The development is car free. 92 cycle spaces are proposed.

A travel plan would aim to reduce vehicle trips, traffic congestion, noise and air pollution, and greenhouse gas emissions.

The implementation of these measures would ensure that the residual effects would not be significant. Pollutant concentrations would be within the relevant health-based air quality objectives. Building users would be exposed to acceptable air quality and the site is suitable for the proposed use.

Noise and Vibration

A report concludes that internal noise levels would be acceptable subject to appropriate acoustic design and mitigation. The mitigation measures required for any externally mounted plant and ventilation should be a condition.

Delivery and service vehicles would be restricted to daytime hours to mitigate any impact on residential accommodation. During the operational phase the proposal would not produce noise levels or vibration that would be significant.

The applicant and their contractors would work and engage with the local authority and local communities to seek to mitigate any impacts and minimise disruption.

The potential noise impact in external areas would be negligible with mitigation in the form of noise management in place which would be controlled by a condition.

Cumulative impacts would be negligible with mitigation in place.

Telecommunications (TV and Radio reception and Broadband provision)

It is recommended that any reported television or radio interference during the construction phase and following completion should be investigated by means of a reception measurement taken prior to construction, within 6 months of the commencement of construction works and at post-construction stage. Should there be any impact a series of mitigation measures would be identified which could be controlled by a condition.

Crime and Disorder - The increased footfall, additional residents and improved lighting would improve security and surveillance. Greater Manchester Police have provided a crime impact assessment and the scheme should achieve Secured by Design accreditation. A condition is recommended.

Archaeological issues - GMAAS believe that there could be below ground remains. The site should be subject to intrusive archaeological investigation in advance of development with an initial phase of evaluation trenching, followed, if necessary, by open-area excavation and recording. This investigation can be secured through a condition granted.

Biodiversity and Wildlife Issues/ Contribution to Blue and Green Infrastructure (BGIS) / Climate change adaptation and mitigation from Green Infrastructure -The site is currently occupied by buildings and hardstanding and contains no statutory nature conservation sites. The habitats and plant species recorded at the site are widespread and common throughout the UK and Greater Manchester.

The baseline habitat survey identified a single small non-native street tree in poor condition at the site on St Mary's Parsonage which would be lost. Using the Urban Tree Helper within Metric 3.1, this urban tree was estimated to have a value of 0.02 biodiversity units. The remainder of the site was classified as hardstanding or building and, therefore, had no allocated biodiversity units. The site was not considered to be of strategic importance for biodiversity.

The Site does provide bat roosts, and a licence would be required from Natural England. Details of the measures to provide replacement bat roosting features have been submitted as part of the application. The proposed and retained buildings could support roosting bats. It is possible that birds would nest in/on the buildings and trees therefore an informative should be applied to the application to make the applicant aware of the requirements in relation to breeding birds.

The proposal would provide green infrastructure including tree cover. This could secure ecological enhancement for both flora and fauna. Measures to mitigate habitat loss and improve biodiversity are included in the Ecology Report. The scheme would provide a Biodiversity Net Gain of +22 biodiversity units. This net gain leads to a very large percentage increase (much greater than +100%) due to the very small amount of habitat currently present on site.

The rain garden would contain pollinator-friendly plant species including grasses and herbaceous species. The design of the public realm aims to mitigate impacts on climate change and improve biodiversity. Soft landscaping can provide climate change benefits in its own right: carbon sequestration (CO₂ offsetting) from the planting of new trees, a net 17no. addition. Planting and provision of public amenity space would support the Sustainable Drainage Systems (SuDS). The trees would increase shade and evapotranspiration from the trees and planting would mitigate the urban heat island effect.

Waste and Recycling –

Office

There would be a dedicated refuse store in the carriage works accessed via Garden Lane. It would be mechanically ventilated with louvres. The total weekly volume is divided by percentage into the following waste streams: General refuse = 30% = 6435 L = 6no. x 1100L Euro bins; Pulpable refuse = 50% = 10725 L = 10no. x 1100L Eurobins; Co-mingled Refuse = 15% = 3218 L = 3no. x 1100L Eurobins ; and Organic = 5% = 1073 L = 1 no. x 1100L Eurobins

Retail

Additional provisions have been allowed for in the Refuse store. The refuse generated by the retail units should be collected daily. The refuse store has been sized in line with 'GD 04 Waste Storage and Collection Guidance for New Developments based a twice weekly collection. Staff would arrange for the daily refuse collections and the collection company would retrieve the bins from the store and return them to the store immediately after collection.

Flood Risk and Sustainable Urban Drainage Strategy (Suds) - The site is in Flood zone 1 and is low risk site for flooding. A range of flood risk mitigation measures have been proposed including raising ground levels to fall away buildings and new drainage to reduce the risk of flooding from surface water and sewer. Due to the risk of groundwater flooding, any proposed basements would be required to provide the necessary degree of watertightness.

The hierarchy of drainage options has been considered in line with local and national policy. A connection to the public combined sewer in Garden Lane, north of the site is being considered, to retain the site's existing method of disposal. The existing drainage catchments have been identified and existing peak runoff rates calculated using the Modified Rational Method.

Contaminated Land Issues – A Phase 2 Ground Investigation has been prepared based on desktop / published sources and on-site sampling. Contaminants have been identified and remediation measures would be a condition.

Disabled access – The building and public realm is inclusive regardless of disability, age, or gender. The main pedestrian access is off King Street West. The proposal would provide fully accessible entrances from street level with automated doors. The design and layout would be inclusive. Passenger lifts in the courtyard provide full access.

Local Labour – A condition would require the Council's Work and Skills team to agree the detailed form of the Local Labour Agreement.

Airport Safeguarding - Given the scale of the development, the potential impacts on aerodrome safeguarding has been assessed. Aerodrome safeguarding have found no conflict with any safeguarding criteria.

Construction Management – The site is in an air quality management area and air quality measures are required. Dust suppression, minimising piling, and use of screenings to cover materials would minimise the impact on residents. With appropriate management measures in place the impacts of construction management on surrounding residents and the highway network can be mitigated to be minimal.

Socio- Economic Impacts / Human Health

The development would create 135 full time equivalent (FTE) jobs over the 2.5-year build period plus jobs connected to additional supply chain expenditure. Total GVA from construction would be around £25 million in Manchester. A local labour agreement would fully realise the benefits of the proposal. Around 24 jobs would be supported on site on completion. This would create an estimated £1.12 million in GVA. These impacts would have a minor to moderate beneficial impact. Local expenditure would also increase during the construction phase.

The employment space could support up to 410 FTE jobs. Their expenditure should have a positive economic impact and help to sustain the viability of local services and facilities. On completion the proposal would generate a net GVA of £35.8 million per annum, wages of £13.1 million for the Manchester economy and contribute £2.7 million of national insurance and income tax. It would deliver business rates of £6.6m every ten years.

The construction and the use of the completed development would create a range of jobs including highly skilled roles, graduate roles and entry level jobs including jobs for local people helping to drive economic growth and productivity and supporting inclusive growth objectives.

No significant adverse socio-economic are expected during the Operational Phase and therefore no specific mitigation is required.

Social Value from the Development

The proposal would support the creation of a strong, vibrant and healthy community. In particular, the proposal would:

- Promote regeneration in other areas of the City Centre and beyond.
- Not harm the natural environment and reduce carbon emissions through design.
- The local labour agreement would provide job opportunities for local people.
- Create jobs.
- Help to reduce crime with increased passive surveillance from active ground floor uses and overlooking from occupiers.
- Improve linkages between areas of the City Centre.
- Provide access to services and facilities via sustainable transport.
- Not result in any adverse impacts on air quality, flood risk, noise or pollution and there will not be any adverse contamination impacts.
- Not have a detrimental impact on protected species.
- Regenerate previously developed land with limited ecological value in a highly efficient manner.
- Through the public realm and Class E use bring a new place for people to gather in which to relax, socialise and enjoy.

Response to Objectors Comments

The response to the objections received is included in the heritage related parts of this report.

CONCLUSION

The proposal conforms to the development plan taken as a whole and there are no material considerations which would indicate otherwise. All material considerations have been addressed above.

The proposal would create a range of jobs through construction and in operation including highly skilled roles, graduate roles and entry level jobs including jobs for local people helping to drive economic growth and productivity and supporting inclusive growth objectives.

Significant concerns have been raised by Historic England and Save Britain's Heritage about the loss of part of the Grade II complex. The proposals cause a high level of less than substantial harm but would be outweighed by the public benefits. It is considered, therefore, that, notwithstanding the considerable weight that must be given to preserving the setting of the adjacent listed buildings and preserving or enhancing the character of the adjacent conservation areas as required by virtue of the Listed Buildings Act, the overall impact of the proposed development including the impact on heritage assets would meet the tests set out in paragraphs 189, 197, 199, 200 and 202 of the NPPF and that the harm is outweighed by the benefits of the development.

The building would be tall in its current context, but this would change significantly if proposals for larger buildings at Kendals, Alberton House and Albert Bridge House are developed. The design aims to minimise the impact on the surrounding heritage

assets and the harm would be mitigated and would not amount to a reason to refuse this planning application.

The regeneration of the site would be beneficial, and the building would be highly sustainable. It would be energy efficient offering long terms solution to energy supply and carbon reductions. Public realm improvements would promote pedestrian and cycle movements. There would be no unduly harmful impacts on noise, traffic generation, air quality, water management, wind, solar glare, contamination or loss of daylight and sunlight.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved polices of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation MINDED TO APPROVE

Article 35 Declaration

Officers have worked with the applicant in a positive and pro-active manner to seek solutions to problems arising in relation to dealing with the applications. This has included ongoing discussions regarding changes to the proposals and the need for more information and has included extensive pre application advice. This has resulted in an acceptable development.

Application referenced 135028/FO/2022 - Condition(s) to be attached to decision:

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents:

The site location plan referenced 107_B1_00_0000 received by the City Council as Local Planning authority on 23 September 2022

The drawings referenced:

107 B1_05_2200
107 B1_05_2201
2951-PLA-GF-XX-DR-L-0001
2951-PLA-GF-XX-DR-L-2002

received by the City Council as local planning authority on 23 September 2022

2951-PLA-GF-XX-SK-L-2004 Rev P01

received by the City Council as local planning authority on 30 November 2022

107_B1_02_0099 Rev A
107 B1_02_0100 Rev B
107 B1_02_0101 Rev A
107 B1_02_0102 Rev A
107 B1_02_0103 Rev A
107 B1_02_0104 Rev A
107 B1_04_2200 Rev A
107 B1_04_2201 Rev A
107 B1_04_2202 Rev B
107 B1_04_2204 Rev B
107 B1_04_2205 Rev B
107 B1_04_2206 Rev B
107 B1_04_2210 Rev A
107 B1_04_2211 Rev B
107 B1_10_4201 Rev A
107 B1_10_4202 Rev A
107 B1_10_4203 Rev A

received by the City Council as local planning authority on 17 April 2023

2951-PLA-GF-XX-DR-L-0001 Rev P04

received by the City Council as local planning authority on 24 May 2023

107 B1_02_2201 Rev C
107 B1_02_2202 Rev B
107 B1_02_2203 Rev B

received by the City Council as local planning authority on 13 June 2023

107 B1_02_2204 Rev A
107 B1_02_2210 Rev A
107 B1_02_2211 Rev A

107 B1_02_2213 Rev A
 107 B1_02_2214 Rev A
 107 B1_04_2203 Rev C

received by the City Council as local planning authority on 20 June 2023

107 B1_04_2207 Rev C
 107 B1_02_2199 Rev C
 107 B1_02_2200 Rev F

received by the City Council as local planning authority on 22 June 2023

The Condition Report and Repair Schedule prepared by stephenson, hamillton, risely STUDIO received by the City Council as local planning authority on 13 June 2023

The letter from Penny Anderson Associates Ltd Consultant Ecologists dated 15 November 2022 received by the City Council as local planning authority on 15 November 2022

The Bins Provision Calculation received by the City Council as local planning authority on 24 November 2022

The Flood Risk Assessment and Outline Drainage Strategy (Rev P03) prepared by WSP received by the City Council as local planning authority on 21 November 2022

The BREEAM Pre-assessment for the Refurbishment of the Carriage Works Rev B prepared by Turley Sustainability received by the City Council as local planning authority on 17 April 2023

The BREEAM Pre-assessment for the New Construction V6 Rev B prepared by Turley Sustainability received by the City Council as local planning authority on 17 April 2023

The Design and Access Statement prepared by John Matthews Architects received by the City Council as Local Planning authority on 23 September 2022

The Air Quality Assessment prepared by Redmore Environmental received by the City Council as Local Planning authority on 23 September 2022

The Archaeological Desk-Based Assessment prepared by the University of Salford received by the City Council as Local Planning authority on 23 September 2022

The letter from Penny Anderson Associates Ltd Consultant Ecologists dated 9 September 2022 received by the City Council as Local Planning authority on 23 September 2022

The Construction Management Plan and Outline Method Statement prepared by Property Alliance Group Ltd

received by the City Council as Local Planning authority on 23 September 2022

The Crime Impact Statement prepared by Greater Manchester Police Design for Security received by the City Council as Local Planning authority on 23 September 2022

The Daylight and Sunlight Amenity Impact Assessment Report prepared by Grayscanlanhill Chartered Building Surveyors received by the City Council as Local Planning authority on 23 September 2022

The Energy and Environmental Standards Statement prepared by CWC received by the City Council as Local Planning authority on 23 September 2022

The Local Labour Agreement received by the City Council as Local Planning authority on 23 September 2022

The Pedestrian Level Wind Desk-Based Assessment prepared by RDWI received by the City Council as Local Planning authority on 23 September 2022

The Planning Statement prepared by Euan Kellie Property Solutions received by the City Council as Local Planning authority on 23 September 2022

The Statement of Community Involvement prepared by Counter Context Ltd received by the City Council as Local Planning authority on 23 September 2022

The Planning Report - Ventilation Strategy prepared by CWC received by the City Council as Local Planning authority on 23 September 2022

The Noise Survey and Plant Noise Egress Limits prepared by Sandy Brown Consultants in Acoustics; Noise & Vibration received by the City Council as Local Planning authority on 23 September 2022

The Extended Phase 1 Habitat Survey, Building Inspection for Bats and Bat Activity Surveys prepared by Penny Anderson Associates Ltd Consultant Ecologists dated 9 September 2022 received by the City Council as Local Planning authority on 23 September 2022

The Phase I Geoenvironmental Site Assessment prepared by e3p received by the City Council as Local Planning authority on 23 September 2022

The Heritage Asset Statement prepared by Jon Matthews Architects received by the City Council as Local Planning authority on 17 October 2022

The Transport Statement prepared by Property Alliance Group Ltd received by the City Council as Local Planning authority on 23 September 2022

The Framework Travel Plan prepared by Property Alliance Group Ltd received by the City Council as Local Planning authority on 23 September 2022

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to Core Strategy SO1, SO2, S05, S06, SP1, C1, EC1, EC3, CC1, CC4, CC5, CC6, CC7, CC8, CC9, CC10, T1, T2, EN1, EN3, EN4, EN6, EN8, EN9, EN11, EN14, EN15, EN16, EN17, EN18, EN19 and DM1 saved Unitary Development Plan polices DC10.1, DC18.1 DC19.1, DC20, DC26.1 and DM2.

3) No development approved by this planning permission (excluding demolition works) shall commence until a remediation strategy to deal with the risks associated with contamination of the site in respect of the development hereby permitted, has been submitted to, and approved in writing by, the local planning authority (approval to be in consultation with The Environment Agency). This strategy will include the following components:

a) A site investigation scheme, based on the E3P Phase I desk top study reference 15-644-R1-2 dated August 2022, to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off-site.

b) The results of the site investigation and the detailed risk assessment referred to in (a) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.

c) A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy in (b) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance, and arrangements for contingency action.

d) Prior to any part of the permitted development being occupied/brought into use, a verification report demonstrating the completion of works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to, and approved in writing, by the local planning authority. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met.

e) In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development in each phase is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected, and appropriate remedial action is taken in the interests of public safety. Pursuant to policies DM1 and EN18 of the Core Strategy.

4) Prior to the commencement of development (excluding demolition works), a scheme shall be submitted to and approved in writing by the City Council as local planning authority. The scheme shall include:

- A finalised drainage layout showing all components, outfalls, levels, and connectivity.
- Details of surface water attenuation that offers a reduction in surface water runoff rate in line with the Manchester Trafford and Salford Strategic Flood Risk Assessment, i.e., at least a 50% reduction in runoff rate compared to the existing rates with the aim of reducing to the Greenfield runoff rates, as the site is located within Conurbation Core Critical Drainage Area.
- An existing and proposed impermeable areas drawing to accompany all discharge rate calculations.
- Runoff volume in the 1 in 100-year, 6 hours rainfall shall be constrained to a value as close as is reasonably practicable to the greenfield runoff volume for the same event, but never to exceed the runoff volume from the development site prior to redevelopment.
- Evidence that the drainage system has been designed (unless an area is designated to hold and/or convey water as part of the design) so that flooding does not occur during a 1 in 100-year rainfall event with allowance for 45% climate change in any part of a building.
- Assessment of overland flow routes for extreme events that is diverted away from buildings (including basements). Overland flow routes need to be designed to convey the flood water in a safe manner in the event of a blockage or exceedance of the proposed drainage system capacity including inlet structures. A layout with overland flow routes needs to be presented with appreciation of these overland
- flow routes with regards to the properties on site and adjacent properties off site.

- Where surface water is connected to the public sewer, agreement in principle from United Utilities is required that there is adequate spare capacity in the existing system taking future development requirements into account. An email of
- acceptance of proposed flows and/or new connection will suffice.
- Where an application is part of a larger site which already has planning permission it is essential that the new proposal does not compromise the drainage scheme already approved.
- Hydraulic calculation of the proposed drainage system.
- Construction details of flow control and SuDS elements.
- Foul water flows should be kept separate from surface water flows, in accordance with Part H of the Building Regulations.

The approved details shall be implemented and completed as part of the development.

Reason: To prevent the increased risk of flooding, to improve and protect water quality and ensure future maintenance of the surface water drainage system to manage the risk of flooding and pollution pursuant to policies EN17 and DM1 of the Core Strategy.

5) Prior to the commencement of the construction phase of the development a detailed construction and demolition management plan outlining working practices during development shall be submitted to and approved in writing by the local planning authority, which for the avoidance of doubt should include;

- Method Statements and risk assessments for construction and demolition works.
- Display of an emergency contact number to be displayed in a publicly accessible location.
- Details of Wheel Washing.
- Dust and dirt suppression measures.
- Compound locations where relevant.
- Location, removal, and recycling of construction waste.
- Details of the vehicular activity associated with the construction, including a strategy for the routing of construction traffic, and swept path analysis.
- Details of the location and arrangements for all parking of all construction related vehicles and staff.
- Sheeting over of construction vehicles.
- Construction and demolition methods to be used, including the use of cranes.
- The identification of all vehicular access points into the site.
- Communication strategy with residents and businesses which shall include details of how there will be engagement, consult and notify residents during the works.
- A highway dilapidation survey including photographs and commentary on the condition of carriageway/footways on construction vehicle routes surrounding the site.

Development shall be carried out in accordance with the approved construction management plan.

Reason - To safeguard the amenities of nearby residents and highway safety, pursuant to policies SP1, EN9, EN14, EN15, EN17, EN19 and DM1 of the Manchester Core Strategy (July 2012).

6) No development or demolition shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by the local planning authority. The WSI shall cover the following:

- a) Informed by the updated Northwest Regional Research Framework, a phased programme and methodology of investigation and recording to include:
 - i. historic building recording to Historic England Level 3 for Reedham House (31-33 King Street West)
 - ii. historic building recording to Historic England Level 1 to 2 (No.3 Smithy Lane and the Carriage Works at junction of Garden Lane/St Mary's Parsonage)
 - iii. informed by (i) and (ii), strip-out and/or demolition watching brief(s) to allow recording of previously concealed historic fabric etc
 - iv. a scheme of targeted post-demolition archaeological evaluation trenching
 - v. informed by (iv), more detailed targeted excavation (subject of a new WSI)
- b) A programme for post investigation assessment to include:
 - i. analysis of the site investigations records and finds
 - ii. production of a final report(s) on the investigation results.
- c) Deposition of the final report(s) with the Greater Manchester Historic Environment Record.
- d) Dissemination of the results commensurate with their significance.
- e) Provision for archive deposition of the report and records of the site investigation.
- f) Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason: In accordance with NPPF Section 16, paragraph 205: To record and advance understanding of the significance of any heritage assets to be lost in part and to investigate the archaeological interest of the site and record and preserve any remains of archaeological interest and to make this evidence (and any archive generated) publicly accessible, pursuant to policy DM1 of the Core Strategy for the City of Manchester and saved policy DC20.1 of the Unitary Development Plan for the City of Manchester.

7) Notwithstanding the details submitted with the application, prior to the commencement of development the following shall be submitted for approval in writing by the City Council, as Local Planning Authority:

- (a) Samples and specifications of all material to be used on all external elevations and rooves of the development including the alterations to the heritage asset, drawings to illustrate details of full-sized sample panels and mock-ups that will be

produced (The panel to be produced shall include jointing and fixing details between all component materials and any component panels), details of external ventilation requirements, details of the drips to be used to prevent staining and details of the glazing and frames, a programme for the production of the full sized sample panels and a strategy for quality control management.

(b) Submission of a Construction Environmental Management Plan (CEMP) (Materials) to include details of the strategy for securing more efficient use of non-renewable material resources and to reducing the lifecycle impact of materials used in construction and how this would be achieved through the selection of materials with low environmental impact throughout their lifecycle.

(c) The sample panels and quality control management strategy shall then be submitted to and approved in writing by the City Council as local planning authority.

The approved materials shall be used in the construction of the development.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

8) i) Prior to the commencement of the development, details of a Local Labour Proposal, in order to demonstrate commitment to recruit local labour for the duration of the construction of the development, shall be submitted to and approved in writing by the City Council, as Local Planning Authority. In this condition a Local Labour Proposal means a document which includes:

- a. the measures proposed to recruit local people including apprenticeships
- b. mechanisms for the implementation and delivery of the Local Labour Proposal
- c. measures to monitor and review the effectiveness of the Local labour Proposal in achieving the objective of recruiting and supporting local labour objectives

The approved document shall be implemented as part of the construction of the development.

(ii) Within one month prior to construction work being completed, a detailed report which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted and approved in writing by the City Council as Local Planning Authority.

Reason - The applicant has demonstrated a commitment to recruiting local labour pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy (2012).

9) Full details of the method of support and protection of all historic fabric adjacent to the location of construction work shall be submitted to and approved in writing by

Manchester City Council as Local Planning Authority prior to the commencement of the development hereby approved. The approved method of support and protection shall be fully implemented prior to development commencing on site.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

10) Prior to the commencement of above ground works, final details of the following shall be submitted and approved in writing by the City Council as Local Planning Authority. The programme shall include an implementation timeframe:

- a) Details of measures to create potential opportunities to enhance and create new biodiversity within the development to include bat boxes and bricks, bird boxes, appropriate planting, tree pits and rainwater harvesting; and
- b) An assessment of the possibility of providing a green roof, brown roof or blue/green roof and full justification of the results. If this is possible full details of proposals and a management and maintenance strategy for the chosen type of roof.

The approved scheme shall be implemented not later than 12 months from the date the proposed building is first occupied.

Reason - To ensure that a satisfactory measures to enhance biodiversity are incorporated within the development in accordance with policies R1.1, I3.1, T3.1, S1.1, E2.5, E3.7 and RC4 of the Unitary Development Plan for the City of Manchester and policies SP1, DM1, EN1, EN9 EN14 and EN15 of the Core Strategy.

11) Notwithstanding the information detailed within Condition 2 of this application, full details of the junction of the new build to the listed building, including the interface between new lift shaft and the courtyard fabric shall be submitted to and approved in writing by the City Council as local planning authority prior to the commencement of any above ground works.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

12) Prior to the commencement of any works to the retained buildings (identified in the Extended Phase 1 Habitat Survey, Building Inspection For Bats And Bat Activity Surveys by Penny Anderson Associates Limited (dated September 2021) the following information shall be submitted to and approved in writing by the City Council as local planning authority:

- a) a licence issued by Natural England pursuant to Regulation 55 of The Conservation of Habitats and Species Regulations (Amendments) (EU Exit) 2019 authorizing the specified activity/development to go ahead; or
- b) a statement in writing from the relevant licensing body to the effect that it does not consider that the specified activity/development will require a licence.

Reason - To ensure wildlife habitats are not adversely affected and to be consistent with policies EN15 and DM1 of the Manchester Core Strategy.

13) No removal of or works to any trees or works to or demolition of buildings or structures that may be used by breeding birds shall take place during the main bird breeding season 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of the tree or structures for active birds' nests immediately before the vegetation is cleared or works commence and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation shall be submitted to and approved by the City Council as local planning authority prior to the removal of or works to any trees or works to or demolition of buildings or structures.

Reason - To ensure wildlife habitats are not adversely affected and to be consistent with policies EN15 and DM1 of the Manchester Core Strategy.

14) No development hereby permitted shall be occupied until details of the implementation, maintenance and management of the sustainable drainage scheme have been submitted to and approved by the local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details. Those details shall include:

- a) Verification report providing photographic evidence of construction as per design drawings.
- b) As built construction drawings if different from design construction drawings.
- c) Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

Reason: To manage flooding and pollution and to ensure that a managing body is in place for the sustainable drainage system and there is funding and maintenance mechanism for the lifetime of the development.

15) a) Fumes, vapours and odours shall be extracted and discharged from the premises in accordance with a scheme to be submitted to and approved in writing by the City Council as local planning authority before the use to which these works hereby approved relate commences.

Mixed use schemes shall ensure provision for internal ducting in risers that terminate at roof level. Schemes that are outside the scope of such developments shall ensure that flues terminate at least 1m above the eave level and/or any openable windows/ventilation intakes of nearby properties.

b) Before the use to which these works hereby approved relate commences, confirmation shall be submitted for the approval of the City Council as local planning authority that the approved scheme has been fully implemented.

c) The equipment shall then be operated and maintained only in accordance with the above approved documents for the duration of the use.

Reason - To ensure an acceptable development in the interests of residential amenity, pursuant to policies DM1 and SP1 of the Core Strategy and saved policy DC26 of the Unitary Development Plan.

16) a) Prior to any above ground works, a scheme shall be submitted to acoustically insulate and limit the breakout of noise from the uses hereby approved in accordance a noise study of the premises and a scheme of acoustic treatment, including measurements that has been submitted to and approved in writing by the City Council as local planning authority. The scheme shall then be implemented in full before the use hereby approved commences.

b) Upon completion of the development and before the use hereby approved becomes operational, a verification report shall be undertaken to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic consultant's report. The report shall also undertake post completion testing to confirm that above criteria is met. Any instances of non-conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the noise criteria. This report shall be submitted to and approved in writing by the City Council as local planning authority before the use becomes operational along with a timescale that confirms when any necessary changes will be implemented.

Reason - To ensure an acceptable development in the interests of residential amenity, pursuant to policies DM1 and SP1 of the Core Strategy and saved policy DC26 of the Unitary Development Plan.

17) a) Prior to the installation of any externally mounted ancillary equipment, this equipment shall be acoustically insulated in accordance with a scheme submitted to and approved in writing by the City Council as local planning authority in order to secure a reduction in the level of noise emanating from the equipment.

Externally mounted ancillary plant, equipment and servicing shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve a rating

level of 5dB (LAeq) below the typical background (LA90) level at the nearest noise sensitive location.

b) Upon completion of the development and within 6 months of the equipment becoming operational, a verification report will be required to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic consultant's report. The report shall also undertake post completion testing to confirm that above criteria is met. Any instances of non-conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the noise criteria. This report shall be submitted to and approved in writing by the City Council as local planning authority within 6 months of the equipment becoming operational along with a timescale that confirms when any necessary changes will be implemented.

Reason - To safeguard the amenities of the occupiers of nearby residential accommodation, pursuant to saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

18) a) Notwithstanding the information detailed within Condition 2 of this application, full and final details of the works to each of the existing windows and window openings, shall be submitted to and formally approved in writing by the City Council as Local Planning Authority prior to the commencement of any works to the works to the elevations of the retained buildings.

b) Full details of works to retained windows shall include a condition survey and method statement clarifying how the retained windows will be repaired.

c) Full details of replacement windows/reinstated windows shall include detailed scaled drawings, including cross sections showing the replacement windows in comparison to existing original examples of windows at the property or researched examples where such window does not exist.

d) Any removal of historic windows shall require assessment, justification and evidence of disrepair preventing reuse, which shall include a condition survey and method statement for the removal and clarification of how the windows affected will be reused.

e) The works to windows shall be carried out only as agreed in accordance with the approved details and shall be completed prior to first occupation of the development hereby approved.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

19) Prior to the use of the ground floor retail unit a scheme for the storage (including segregated waste recycling) and disposal of refuse from this use has been submitted to and approved in writing by the City Council as local planning authority. The details of the approved scheme shall be implemented as part of the development and shall remain in situ whilst the use or development is in operation (This shall apply to any subsequent change of use within Class E).

New developments shall have refuse storage space for segregated waste collection and recycling. Internal storage areas are required.

Reason - In order to ensure that adequate provision is made within the development for the storage and recycling of waste in accordance with policies DM1 and EN19 of the Core Strategy for the City of Manchester.

20) The Class E uses hereby approved shall only be used in accordance with a schedule of days and hours of operation submitted to and approved in writing by the City Council as local planning authority before the commencement of the use of the development hereby approved. The uses hereby approved shall not operate outside of the approved opening hours.

Reason - In interests of residential amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

21) The 10th floor external terraces hereby approved shall only be used in accordance with a schedule of days and hours of operation, occupier capacity and use, which shall be submitted to and approved in writing by the City Council as local planning authority before the first use of the development hereby approved. The terraces hereby approved shall not operate outside of the approved opening hours and there shall be no use of speakers, amplified sounds, or any music in these external areas at any time.

Reason - In interests of residential amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

22) No part of the development shall be occupied unless and until details of a servicing strategy (deliveries, collections, and ad-hoc maintenance) to detail final arrangements in relation to both refuse collection and deliveries has been submitted to and approved in writing by the City Council as local planning authority. This should cover the frequency and dimensions of vehicles requiring access to the site, along with final details of the location for loading/unloading. Any approved Strategy shall be implemented in full at all times when the development hereby approved is in use.

Reason - In interests of residential amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

23) Prior to the occupation of the development, a scheme of highway works, in order to provide an adequate pedestrian and vehicular environment in the vicinity of the

application site, shall be submitted for approval in writing by the City Council, as Local Planning Authority.

For the avoidance of doubt this shall include the following:

- a) The tree planting along King Street West.
- b) The provision of a loading bay on the east side of St. Mary's Parsonage.
- c) All works to the adopted highway, including footway improvements.

The approved scheme shall be implemented and be in place prior to the first occupation of the development hereby approved and thereafter retained and maintained in situ.

Reason -To ensure safe access to the development site in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012)

24) The development shall be carried out in accordance with the framework travel plan stamped as received by the City Council, as Local Planning Authority, on the 23 September 2022.

In this condition a Travel Plan means a document which includes:

- i. the measures proposed to be taken to reduce dependency on the private car by those working at the development.
- ii. a commitment to surveying the travel patterns of staff during the first three months of the first use of the building and thereafter from time to time
- iii. mechanisms for the implementation of the measures to reduce dependency on the private car
- iv. measures for the delivery of specified travel plan services
- v. measures to monitor and review the effectiveness of the Travel Plan in achieving the objective of reducing dependency on the private car

Within six months of the first occupation of the building, a Travel Plan which takes into account the information about travel patterns gathered pursuant to item (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority. Any Travel Plan which has been approved by the City Council as Local Planning Authority shall be implemented in full at all times when the development hereby approved is in use.

Reason - To assist promoting the use of sustainable forms of travel for occupants, pursuant to policies T1, T2 and DM1 of the Manchester Core Strategy (2012).

25) No part of the development shall be occupied until the space and facilities for the parking of a minimum of 92 bicycles have been provided within the basement of the building. The approved spaces and facilities shall then be retained and permanently reserved for bicycle parking.

Reason - To ensure that adequate provision is made for bicycle parking so that persons occupying or visiting the development have a range of options in relation to transport mode, pursuant to policy T1 of the City of Manchester Core Strategy.

26) Prior to the first use of the single commercial unit as indicated on drawing B1_02_2200 Rev B stamped as received by the City Council, as Local Planning Authority, on the 17 April 2023, details of any roller shutters to the ground floor of the premises shall be submitted for approval in writing by the City Council, as Local Planning Authority. For the avoidance of doubt the shutters shall only be fitted internally to the premises. The approved details shall be implemented prior to the first occupation of the commercial unit and thereafter retained and maintained in situ.

Reason - To ensure that the roller shutters are appropriate in visual amenity terms pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

27) Prior to implementation of any proposed lighting scheme (including on the 13th floor roof terrace), details of the scheme to include a report to demonstrate that the proposed lighting levels would not have any adverse impact on the amenity of occupiers within this and adjacent developments shall be submitted to and agreed in writing by the City Council as local planning authority.

The approved details shall be implemented as part of the development.

Reason - In the interests of visual and residential amenity pursuant to Core Strategy policies SP1, CC9, EN3 and DM1 of the Core Strategy.

28) Notwithstanding the information detailed within Condition 2 of this application and prior to the commencement of any landscaping works full and final details of the landscaping scheme shall be submitted for approval in writing by the City Council, as Local Planning Authority. The following information shall be submitted:

- a) Samples and specifications of all materials to be used in the landscaping and jointing and fixing details between all component materials.
- b) Details of planting species.
- c) Details of the inclusion of age friendly measures.
- d) Submission of a Construction Environmental Management Plan (CEMP) (Materials) to include details of the strategy for securing more efficient use of non-renewable material resources and to reducing the lifecycle impact of materials used in the landscaping and how this would be achieved through the selection of materials with low environmental impact throughout their lifecycle.
- e) A strategy for quality control management and maintenance.

The approved scheme shall be implemented not later than 12 months from the date the buildings are first occupied. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted, or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory measures to enhance biodiversity and provide age friendly measures are incorporated within the development in accordance with policies R1.1, I3.1, T3.1, S1.1, E2.5, E3.7 and RC4 of the Unitary Development Plan for the City of Manchester and policies SP1, DM1, EN1, EN9 EN14 and EN15 of the Core Strategy.

29) Notwithstanding the information detailed within Condition 2 of this application, full details of all new external doors and glazing, including blocked openings shall be submitted to and approved in writing by the City Council as local planning authority prior to any works to implement these elements commencing on site.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

30) Notwithstanding the information detailed within Condition 2 of this application, full details of all new and/or replacement rainwater goods, including materials and finishes and details of the rainwater goods to be removed shall be submitted to and approved in writing by the City Council as local planning authority prior to the removal of any rainwater goods.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

31) Notwithstanding the information detailed within Condition 2 of this application, full details of all works to external historic features and finishes, including doors, stonework, cobblestones and brickwork etc. shall be submitted to and approved in writing by the City Council as local planning authority prior to any works commencing to those elements.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

32) a) Prior to first occupation of the completed development hereby approved, final details including samples and specifications of the materials, including natural stone or other high-quality materials to be used in the repair or improvement of the affected footpaths and for the areas between the pavement and the line of the proposed building on all site boundaries; shall be submitted and approved in writing by the City Council as Local Planning Authority.

b) The approved scheme shall be implemented not later than 12 months from the date the proposed building is first occupied.

Reason - To ensure safe access to the development site in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

33) The 18no. proposed street trees shall be implemented not later than 3 months from the date the proposed building is first occupied. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted, or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies R1.1, I3.1, T3.1, S1.1, E2.5, E3.7 and RC4 of the Unitary Development Plan for the City of Manchester and policies SP1, DM1, EN1, EN9 EN14 and EN15 of the Core Strategy.

34) a). Three months prior to the first occupation of the development, a Local Labour Proposal Framework that outlines the approach to local recruitment for the end use(s), shall be submitted to and approved in writing by the City Council, as Local Planning Authority. In this condition a Local Labour Proposal means a document which includes:

- i) the measures proposed to recruit local people including apprenticeships
- ii) mechanisms for the implementation and delivery of the Local Labour Proposal
- iii) measures to monitor and review the effectiveness of the Local labour Proposal in achieving the objective of recruiting and supporting local labour objectives

The approved document shall be implemented as part of the occupation of the development.

(b). Within 6 months of the first occupation of the development, a Local Labour Proposal which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted to and approved in writing by the City Council, as Local Planning Authority. Any Local Labour Proposal approved by the City Council, as Local Planning Authority, shall be implemented in full at all times whilst the use is in operation.

Reason - To ensure that the application honours their commitment to recruit local labour and to understand the impact of this pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy (2012)

35) The development hereby approved shall achieve a post-construction Building Research Establishment Environmental Assessment Method (BREEAM) rating of at least 'very good'. Post construction review certificate(s) shall be submitted to and approved in writing by the City Council as local planning authority before the development hereby approved is first occupied.

Reason - In order to minimise the environmental impact of the development, pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy, policy DP3 of Regional Spatial Strategy for the Northwest (RSS), and the principles contained within The Guide to Development in Manchester SPD (2007), and the National Planning Policy Framework.

36) The development shall be carried out in accordance with the Crime Impact Statement referenced 2022/0017/CIS/02 Version A dated 27 July 2022. The development shall only be carried out in accordance with these approved details. The development hereby approved shall not be occupied or used until the Council as local planning authority has acknowledged in writing that it has received written confirmation of a secured by design accreditation confirming achievement of Bronze, Silver or Gold accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Core Strategy and to reflect the guidance contained in the National Planning Policy Framework

37) Before first occupation of the development hereby approved a signage strategy shall be submitted to and agreed in writing by the City Council as local planning authority. All signage displayed at the property shall accord with the approved details.

Reason - In the interests of visual amenity to enable careful attention to signage details and the level of visual clutter is required to protect the character and appearance of this building in accordance with policies SP1 and DM1 of the Core Strategy.

38) Following commencement of construction of the hereby approved development, any interference complaint received by the Local Planning Authority shall be investigated to identify whether the reported television interference is caused by the Development hereby permitted. The Local Planning Authority will inform the

developer of the television interference complaint received. Once notified, the developer shall instruct a suitably qualified person to investigate the interference complaint within 6 weeks and notify the Local Planning Authority of the results and the proposed mitigation solution. If the interference is deemed to have been caused by the Development, hereby permitted mitigation will be installed as soon as reasonably practicable, but no later than 3 months from submission of the initial investigation to the Local Planning Authority. No action shall be required in relation to television interference complaints after the date 12 months from the completion of development.

Reason - To ensure terrestrial television services are maintained In the interest of residential amenity, as specified in Core Strategy Polices DM1 and SP1

39) If any lighting at the development hereby approved, when illuminated, causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, within 14 days of a written request, a scheme for the elimination of such glare or light spillage shall be submitted to the Council as local planning authority and once approved shall thereafter be retained in accordance with details which have received prior written approval of the City Council as Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Core Strategy.

40) When the development is operational, no loading or unloading shall be carried out on the site outside the hours of:

07:30 to 20:00, Monday to Saturday,
10:00 to 18:00, Sunday/Bank Holiday.

Reason - In order to protect the amenity of local residents and in accordance with policies SP1 and DM1 of the Core Strategy.

41) The scheme for the storage (including segregated waste recycling) and disposal of refuse for the office use (Class E) as detailed within Section 16.1 and 16.3 of the Design and Access Statement prepared by Jon Matthews Architects dated September 2022 and within the completed Waste Management Proforma dated 14/09/2022 shall be implemented as part of the development and shall remain in situ whilst the use or development is in operation.

Reason - In order to ensure that adequate provision is made within the development for the storage and recycling of waste in accordance with policies DM1 and EN19 of the Core Strategy for the City of Manchester.

42) The development hereby approved shall include for full disabled access to be provided for the occupiers of the building to all areas of the development via all building entrances and to the floors via the proposed lifts.

Reason - To ensure that satisfactory disabled access is provided by reference to the provisions Core Strategy policy DM1.

43) The windows at ground level, fronting onto King Street West, St Mary's Parsonage, Garden Lane, and Smithy Lane shall be retained as a clear glazed window opening at all times and views into the premises shall not be screened or obscured in any way.

Reason - The clear glazed window(s) is an integral and important element in design of the ground level elevations and are important in maintaining a visually interesting street scene consistent with the use of such areas by members of the public, and so as to be consistent with saved policy DC14 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

44) No externally mounted telecommunications equipment shall be mounted on any part of the building hereby approved, including the rooves other than with express written consent of the Local Planning Authority.

Reason - In the interest of visual amenity pursuant to Core Strategy Policies DM1 and SP1

45) The ground floor commercial unit as shown on drawing 10133-SHP- Z1- 00- PL- A- B5D8- G200- 0001 03 received by the City Council, as Local Planning Authority, on the 6 October 2022, shall remain as a separate unit and shall not be sub divided or amalgamated without the benefit of planning permission being secured.

Reason- In the interests of residential amenity and to ensure the future viability and vitality of the commercial units pursuant to saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies DM1, C5 and SP1 of the Manchester Core Strategy.

46) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification) the building hereby approved shall only be used for offices (Use Class Egi) and the flexible commercial unit at Ground floor Level shall only be used for Use Classes E a, b, c, e, f, (excluding convenience retail).

Reason - In the interest of retaining the provision of office space within the development pursuant to policies EC1, EC4 of the Manchester Core Strategy (2012)

Application referenced 135029/LO/2022 - Condition(s) to be attached to decision:

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents:

The site location plan referenced 107_B1_00_0000 received by the City Council as Local Planning authority on 23 September 2022

The drawings referenced:

107 B1_05_2200
107 B1_05_2201
2951-PLA-GF-XX-DR-L-0001
2951-PLA-GF-XX-DR-L-2002

received by the City Council as local planning authority on 23 September 2022

2951-PLA-GF-XX-SK-L-2004 Rev P01

received by the City Council as local planning authority on 30 November 2022

107_B1_02_0099 Rev A
107 B1_02_0100 Rev B
107 B1_02_0101 Rev A
107 B1_02_0102 Rev A
107 B1_02_0103 Rev A
107 B1_02_0104 Rev A
107 B1_04_2200 Rev A
107 B1_04_2201 Rev A
107 B1_04_2202 Rev B
107 B1_04_2204 Rev B
107 B1_04_2205 Rev B
107 B1_04_2206 Rev B
107 B1_04_2210 Rev A
107 B1_04_2211 Rev B
107 B1_10_4201 Rev A
107 B1_10_4202 Rev A
107 B1_10_4203 Rev A

received by the City Council as local planning authority on 17 April 2023

2951-PLA-GF-XX-DR-L-0001 Rev P04

received by the City Council as local planning authority on 24 May 2023

107 B1_02_2201 Rev C
 107 B1_02_2202 Rev B
 107 B1_02_2203 Rev B

received by the City Council as local planning authority on 13 June 2023

107 B1_02_2204 Rev A
 107 B1_02_2210 Rev A
 107 B1_02_2211 Rev A
 107 B1_02_2213 Rev A
 107 B1_02_2214 Rev A
 107 B1_04_2203 Rev C

received by the City Council as local planning authority on 20 June 2023

107 B1_04_2207 Rev C
 107 B1_02_2199 Rev C
 107 B1_02_2200 Rev F

received by the City Council as local planning authority on 22 June 2023

The Condition Report and Repair Schedule prepared by stephenson, hamilton, risely STUDIO received by the City Council as local planning authority on 13 June 2023
 The Design and Access Statement prepared by John Matthews Architects received by the City Council as Local Planning authority on 23 September 2022
 The Archaeological Desk-Based Assessment prepared by the University of Salford received by the City Council as Local Planning authority on 23 September 2022
 The Planning Statement prepared by Euan Kellie Property Solutions received by the City Council as Local Planning authority on 23 September 2022
 The Heritage Asset Statement prepared by Jon Matthews Architects received by the City Council as Local Planning authority on 17 October 2022

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to Core Strategy SP1, CC9, EN1, EN3 and DM1 saved Unitary Development Plan polices DC10.1, DC18.1 DC19.1 and DC20.

3) The demolition of 31-33 King Street West shall not be undertaken before a contract for the carrying out of the building works in accordance with application referenced 135028/FO/2022, and planning permission has been granted for the redevelopment for which the contract provides, and evidence of that contract has been submitted to and approved in writing by the City Council as Local Planning Authority.

Reason - In the interests of visual amenity and for the avoidance of doubt, and to ensure that redevelopment of the site takes place following external alterations to the existing building pursuant to policy EN3 of the Manchester Core Strategy and saved DC19 of the Unitary Development Plan for the City of Manchester, and the National Planning Policy Framework

4) No development or demolition shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by the local planning authority. The WSI shall cover the following:

- a) Informed by the updated Northwest Regional Research Framework, a phased programme and methodology of investigation and recording to include:
 - i. historic building recording to Historic England Level 3 for Reedham House (31-33 King Street West)
 - ii. historic building recording to Historic England Level 1 to 2 (No.3 Smithy Lane and the Carriage Works at junction of Garden Lane/St Mary's Parsonage)
 - iii. informed by (i) and (ii), strip-out and/or demolition watching brief(s) to allow recording of previously concealed historic fabric etc
 - iv. a scheme of targeted post-demolition archaeological evaluation trenching
 - v. informed by (iv), more detailed targeted excavation (subject of a new WSI)
- b) A programme for post investigation assessment to include:
 - i. analysis of the site investigations records and finds
 - ii. production of a final report(s) on the investigation results.
- c) Deposition of the final report(s) with the Greater Manchester Historic Environment Record.
- d) Dissemination of the results commensurate with their significance.
- e) Provision for archive deposition of the report and records of the site investigation.
- f) Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason: In accordance with NPPF Section 16, paragraph 205: To record and advance understanding of the significance of any heritage assets to be lost in part and to investigate the archaeological interest of the site and record and preserve any remains of archaeological interest and to make this evidence (and any archive generated) publicly accessible, pursuant to policy DM1 of the Core Strategy for the City of Manchester and saved policy DC20.1 of the Unitary Development Plan for the City of Manchester.

5) Notwithstanding the details submitted with the application, prior to the commencement of development the following shall be submitted for approval in writing by the City Council, as Local Planning Authority:

(a) Samples and specifications of all material to be used on all external elevations and rooves of the development including the alterations to the heritage asset, drawings to illustrate details of full-sized sample panels and mock-ups that will be produced (The panel to be produced shall include jointing and fixing details between all component materials and any component panels), details of external ventilation requirements, details of the drips to be used to prevent staining and details of the glazing and frames, a programme for the production of the full sized sample panels and a strategy for quality control management.

(b) Submission of a Construction Environmental Management Plan (CEMP) (Materials) to include details of the strategy for securing more efficient use of non-renewable material resources and to reducing the lifecycle impact of materials used in construction and how this would be achieved through the selection of materials with low environmental impact throughout their lifecycle.

(c) The sample panels and quality control management strategy shall then be submitted to and approved in writing by the City Council as local planning authority.

The approved materials shall be used in the construction of the development.

Reason - In the interests of visual amenity and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest and careful attention to building work is required to protect the character and appearance of this building in accordance with policies EN3 and CC9 of the Core Strategy and saved policy DC19.1; of the Unitary Development Plan

6) The works hereby approved shall not commence until a full method statement for all internal and external works has been submitted to and approved in writing by the City Council as local planning authority. The works shall then be carried out only in accordance with the approved method statement and shall be completed as part of the approved works.

Reason - In the interests of visual amenity and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest and careful attention to building work is required to protect the character and appearance of this building in accordance with policies EN3 and CC9 of the Core Strategy and saved policy DC19.1; of the Unitary Development Plan

7) Full details of the method of support and protection of all historic fabric adjacent to the location of construction work shall be submitted to and approved in writing by Manchester City Council as Local Planning Authority prior to the commencement of any development within the site. The approved method of support and protection shall be fully implemented prior to development commencing on site.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

8) Prior to the commencement of any above ground works, a methodology and specification for any associated scaffolding and support structure for the listed building shall be submitted to and approved in writing by the City Council as local planning authority. These documents shall include the locations of the scaffolding, the means of affixing to the building fabric, the location of any associated fixings to the building, details of how the building fabric would be protected from potential damage as a result of the erection of the scaffolding and details of making good to the building fabric following the removal of the scaffold. All making good works shall be completed within three months of the removal of the scaffolding.

These works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

9) Notwithstanding the information detailed within Condition 2 of this application, full details of the junction of the new build to the listed building shall be submitted to and approved in writing by the City Council as local planning authority prior to the commencement of any above ground works.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

10) Notwithstanding the information detailed within Condition 2 of this application, full details of internal finishes, including floor finishes (including floorboards, fireplace hearthstone and other 19th century finishes) ceiling finishes and wall finishes shall be submitted to and agreed in writing by the City Council as local planning authority prior to the commencement of any internal works.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

11) Notwithstanding the information detailed within Condition 2 of this application full details of the treatment, finishes and enclosure of the basement staircase (item X20 within the Condition Report and Repair Schedule prepared by stephenson, hamillton, risely STUDIO) shall be submitted to and approved in writing by the City Council as local planning authority prior to any works commencing to this item. The details shall include the treatment and finishes of the historic staircase and associated historic fabric and the material of and level of visibility through the enclosure.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

12) a) Notwithstanding the information detailed within Condition 2 of this application, a full assessment of the need for tanking and/or alternative damp proofing measures shall carried out and be submitted to and approved in writing by the City Council as local planning authority six months prior to the completion of the development.

b) Should damp proofing measures and/or tanking have been proven to be necessary full details of these works shall then be submitted to and approved in writing by the City Council as local planning authority fully implemented prior to the completion of the development.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

13) a) Notwithstanding the information detailed within Condition 2 of this application, full and final details of the works to each of the existing windows and window openings, shall be submitted to and approved in writing by the City Council as Local Planning Authority prior to the commencement of any works to the works to the elevations of the building.

b) Full details of works to retained windows shall include a condition survey and method statement clarifying how the retained windows will be repaired.

c) Full details of replacement windows/reinstated windows shall include detailed scaled drawings, including cross sections showing the replacement windows in comparison to existing original examples of windows at the property or researched examples where such window does not exist, specification of any new glazing, redecoration and repair works, including a physical sample of all new window types.

d) Any removal of historic windows shall require assessment, justification and evidence of disrepair preventing reuse, which shall include a condition survey and method statement for the removal and clarification of how the windows affected will be reused.

e) Full details of the location, siting, design, fixings and appearance of any secondary glazing.

f) The works to windows shall be carried out only as agreed in accordance with the approved details and shall be completed prior to first occupation of the development hereby approved.

Reason - In the interests of visual amenity, and because the proposed works affect a building that is within the curtilage of a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

14) Notwithstanding the information detailed within Condition 2 of this application, full details of all repair works to internal and external masonry, stonework and woodwork and removal of detritus from the elevations shall be submitted to and approved in writing by the City Council as local planning authority. These works shall be carried out fully and only in accordance with the approved information and shall be completed within three months of the completion of the development.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

15) Notwithstanding the information detailed within Condition 2 of this application, full details of all mechanical and electrical plant and equipment, including all light fittings and any service openings and/or risers etc. associated with the implementation of mechanical and electrical plant and equipment and details of the finishes of all equipment shall be submitted to and approved in writing by the City Council as local planning authority prior to works commencing to implement any mechanical and electrical plant and equipment.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

16) Notwithstanding the information detailed within Condition 2 of this application, full details of all new and/or replacement rainwater goods, including materials and finishes and details of the rainwater goods to be removed shall be submitted to and approved in writing by the City Council as local planning authority prior to the removal of any rainwater goods.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

17) Notwithstanding the information detailed within Condition 2 of this application, full details of the partitions to be used within the plant room and the shower room to protect the brickwork, including the materials, measurements, locations and method of ventilation shall be submitted to and approved in writing by the City Council as local planning authority prior to the commencement of any works to the internal fabric of the building. The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

18) Notwithstanding the information detailed within Condition 2 of this application, full details of all acoustic and fire separation and insulation shall be submitted to and approved in writing by the City Council as local planning authority prior to any internal works commencing.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

19) Notwithstanding the information detailed within Condition 2 of this application, prior to any internal and external painting or repainting the following information shall be submitted to and approved in writing by the City Council as local planning authority:

- a) a paintwork analysis of historic features and finishes (windows, ironwork and stairs) carried out by a specialist

- b) the report on this analysis
- c) the proposed painting scheme that has been developed based on the report (within b)

The painting and repainting works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

20) Notwithstanding the information detailed within Condition 2 of this application, full details of all works to internal & external historic features and finishes, including doors, staircases, stonework, cobblestones, brickwork and cast-iron columns shall be submitted to and approved in writing by the City Council as local planning authority prior to any works commencing to those elements.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

21) Building lighting proposals, including security lighting shall be submitted to and approved in writing by the City Council as local planning authority prior to any external lighting being installed on site. All building lighting shall be designed and installed so as to control glare and overspill onto nearby residential properties. Full details of this external lighting, including the following shall be submitted to and agreed in writing before any external lighting has been installed on site:

- a) Details of where external lighting would be installed.
- b) Details of the design and appearance of lighting units and the treatment and routing of associated cabling and equipment,
- c) Details of the fixings.

All building lighting shall be installed and maintained in accordance with agreed specifications and locations set out in the above information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

22) Notwithstanding the information detailed within Condition 2 of this application, full details of all new partitions, doors and glazing, including blocked openings shall be submitted to and approved in writing by the City Council as local planning authority prior to any works to implement these elements commencing on site.

The works shall be carried out fully and only in accordance with the approved information.

Reason - In the interests of visual amenity, and because the proposed works affect a building which is included in the Statutory List of Buildings of Special Architectural or Historic Interest so careful attention to building work is required to protect the character and appearance of this building in accordance with policies SP1, CC9, EN1, EN 3 and DM1 of the Adopted Core Strategy for the City of Manchester, the Design and Principle of the Guide to Development in Manchester SPD and saved policies DC18.1 and DC19.1 of the Unitary Development Plan for the City of Manchester.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 135028/FO/2022 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

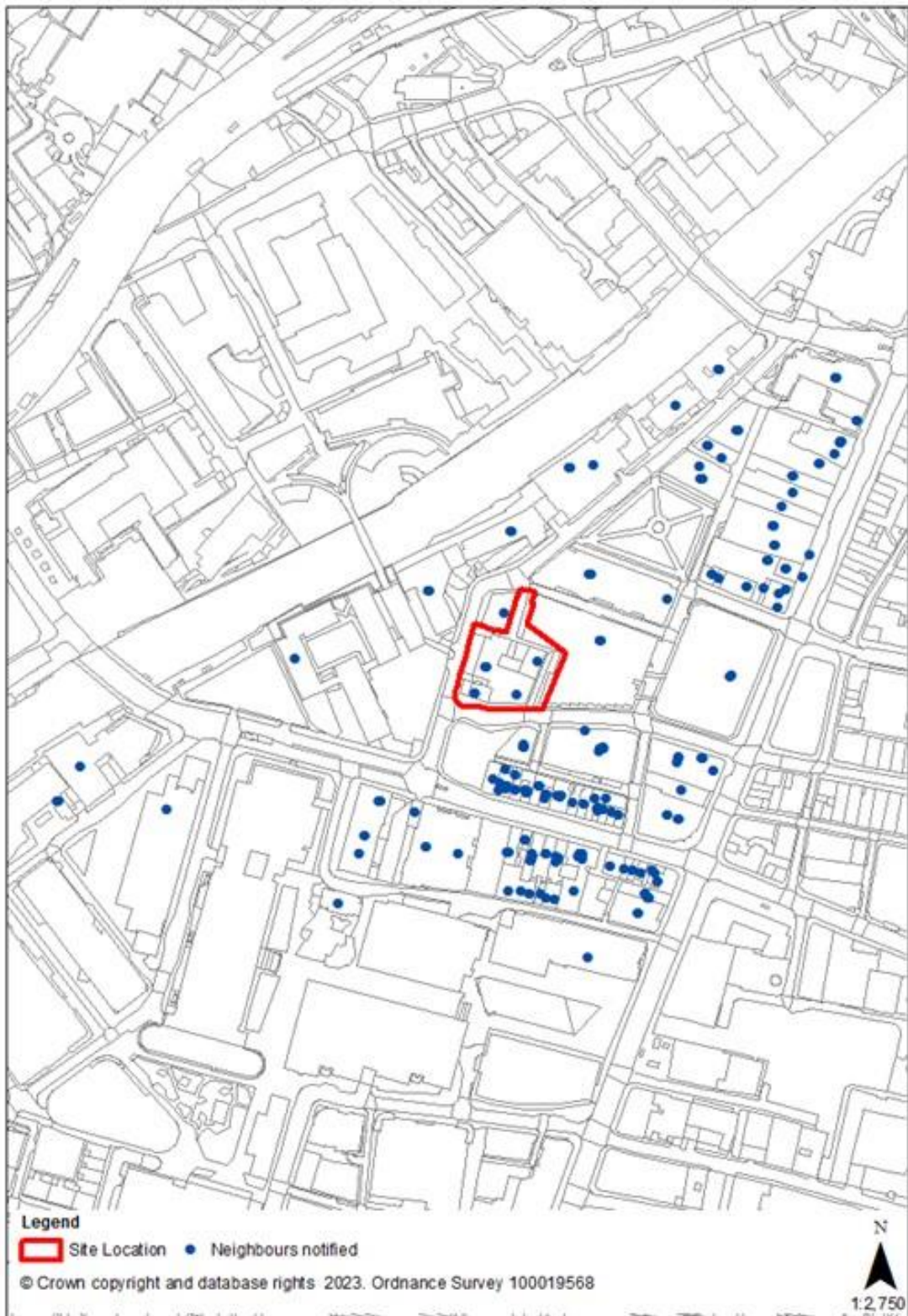
**Highway Services
 Environmental Health
 MCC Flood Risk Management
 Greater Manchester Police
 Work & Skills Team
 Greater Manchester Police
 Historic England (North West)
 Environment Agency
 Transport For Greater Manchester
 Greater Manchester Archaeological Advisory Service
 United Utilities Water PLC
 Greater Manchester Ecology Unit**

**Health & Safety Executive (Fire Safety)
Manchester Airport Safeguarding Officer
National Amenity Societies
Salford City Council
Urban Design & Conservation**

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Relevant Contact Officer : Emily Booth
Telephone number : 0161 234 4193
Email : emily.booth@manchester.gov.uk



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Application Number	Date of Appln	Committee Date	Ward
135544/FO/2022 and 135545/LO/2022	24 Nov 2022	6 July 2023	Moss Side Ward

Proposal Erection of part nine, part eight, part seven storeys building for use as purpose built student accommodation and conversion of 466-468 to purpose built student accommodation use with link, associated landscaping, access and cycle parking following demolition of 470 - 472 Moss Lane East

Listed Building Consent application for internal and external alterations and extension, in the form of a link building to 466 to 468 Moss Lane East to facilitate its conversion to purpose-built student accommodation

Location 466 - 472 Moss Lane East, Manchester, M14 4PJ

Applicant CS Moss Lane East Ltd

Agent Steven Healey, Turley

Executive Summary

Full planning permission is sought for the demolition of 470-472 Moss Lane East and conversion of 466 – 468 Moss Lane East, in order to facilitate the erection of a 7-9 storey building to form purpose-built student accommodation (sui generis) and provide in total 261 student bedrooms with a mix of studios and en-suite rooms set within clusters bedrooms.

Listed Building Consent is also sought for internal and external alterations and extension, in the form of a link building to the Grade II listed 466 to 468 Moss Lane East to facilitate its conversion to purpose-built student accommodation.

17 individual representations have been received, 12 of which object and 5 of which support the proposed development. Objections have also been received from Platt Claremont Residents Association, Moss Side Tenants Union, Great Southern to Western Community Action Network, The Victorian Society and from Councillor Emily Rowles on behalf of the Moss Side Councillors.

The key issues arising from the proposal relate to the impact of the proposal on the amenity of neighbouring occupiers, the need for purpose-built student accommodation (PBSA), design and the impact on the designated heritage asset to be retained, the non-designated heritage asset that is to be demolished and the regeneration benefits of the scheme. These matters are dealt with in detail within the main report.

Description

The application site is situated to the southern side of Moss Lane East opposite Whitworth Park and measures approximately 0.19 hectares in area. The site can be accessed via Heald Place off Moss Lane East.

The site, which is not allocated for any specific use in the core strategy, comprises a pair of semi detached villas at 470-472 Moss Lane East (defined as a non-designated heritage asset in the submitted heritage statement) that most recently had consent for conversion into 12 flats in 2012 and the adjacent Grade II listed pair of semi detached villas at 466 – 468 Moss Lane East that most recently had consent for conversion into 16 flats in 2014. 28 units in total. Both properties have been occupied as Houses in Multiple Occupation. Land to the rear of both pairs of dwellings is a combination of lawn and hardstanding used for car parking (24 spaces).



View of existing Grade II listed building 466 – 468 Moss Lane East as viewed from Moss Lane East



View of existing none designated heritage asset at 470-472 Moss Lane East as viewed from Moss Lane East



Site location plan showing the site within its immediate context

The site is bounded to the north by Moss Lane East with Whitworth Park beyond. Oxford Road lies 150m to the east beyond Heald Place, a surface level hand car wash, the 4- 6 storey Ropemaker Court (student accommodation block) and then

district centre uses associated with Rusholme District Centre. To the west beyond Parkfield Street lies predominantly residential property. To the south lies a number of low-rise commercial uses adjoining the southern and southeast boundaries which feature blank elevations where they face on to the site. The buildings include a garage, convenience store and community hall. Beyond this lies a housing estate.

The Proposal

Planning permission is sought for the erection of a building ranging from 7 to 9 storeys in height to form purpose-built student accommodation (sui generis), following demolition of the existing semi detached properties at 470 – 472 Moss Lane East and the associated conversion of the Grade II listed properties at 466 – 468 Moss Lane East, which would be connected to the new build via a lightweight glazed link at ground floor.



Proposed layout plan indicating link to existing Grade II listed building to Moss Lane East frontage



Detailed elevation of link

The proposed accommodation comprises a total of 261 student bedrooms with a mix of studios and en-suite rooms set within clusters bedrooms, alongside ancillary facilities and shared amenity space.



Image of proposal from Moss Lane East elevation

The proposed development is intended to be car-free and as such no dedicated access or parking is included, with the exception of the provision of three on street accessible parking spaces. Servicing and refuse collection vehicles would take place on Heald Place from a dedicated drop off point. 2No. Pedestrian accesses would be

provided from Moss Lane East, a secondary access would be available from Heald Place for those using bicycles. Externally there would be an amenity space provided in the internal courtyard created in the gap between the retained 466 – 468 Moss Lane East and the building proposed.

The proposed development would see the exterior of the listed building refurbished and internal alterations to the listed building with a ground floor glazed link to the new build element, which would be of contrasting modern appearance whilst drawing influence from the locality in terms of materiality.

A proportion of the units/beds are intended to be the subject of a nomination agreement with the University of Manchester. A letter of support from the University for the proposed development has been submitted. The applicant intends to include affordable provision as part of the proposal.

Publicity

The proposal due to the scale of development has been classified as a major development. As such it has been advertised in the local press (Manchester Evening News) as a major development. Site notices were displayed at the application site in relation to the planning application and listed building application. In addition, statutory consultees have been consulted and notification letters were sent to an extensive area.

Consultation responses

Following the neighbour notification and advertisement of the proposals, 17 responses were received from residents and businesses 12 of these were objecting to the proposals, 5 responses made supportive comments.

Comments made objecting to the scheme were as follows:

- Loss of a historical building at 470-472 Moss Lane East which relates to Whitworth Park. The building should be reutilised as it is in good condition with regards to sustainable policies about carbon neutrality. The loss of the building would also remove local housing for local people.

- Development not in keeping with the area due to the domestic scale of building in close proximity. The development will dominate surrounding 2 storey housing on Parkfield Street, Gateshead Close and on Heald Place. Breaking up the bulk with decoration on the façade will not change the fact that this building will be significantly taller than any surrounding property. The application appears to consider only the front of the building and not the rear. Noted that justification for this height comes from a comparison with other buildings on the "Oxford Road Corridor", which considers Ropemaker Court, Rusholme Place and Wilmslow Park. These latter two developments are on Oxford Road itself and Wilmslow Park (the tallest one) is not immediately adjacent to residential properties.

- The church immediately behind the proposed development is described as a community hall in the submitted documents separating the site from the terraced

properties on Heather Way. This church is a single storey building and will not shield the properties from a 9 storey residence.

- Overlooking and loss of privacy to people's property.
- The noise, of the students constant comings and goings / partying is not appropriate for the small street that this will sit alongside. People have jobs to go to, we cannot be kept awake until all hours because of student behaviours. In addition, litter in this area is absolutely awful, and having more students coming back later after drinking getting takeaways will add to the mess.
- There is no need for this facility, the area has too many students. There is a need for family housing.
- Not enough parking on street at the moment without adding 261 students. Staff will have cars despite good transport links. In spite of there being a plan in place to restrict students from owning cars this is not workable as this discriminates against disabled students. There are also due to be new residents parking restrictions.
- When did the applicant carry out door to door visits as part of their pre-application communication?
- Query about the locations selected within the noise consultants report submitted and whether or not the applicants have understood that the community centre is a church and the noise impacts associated with its use for the future occupants of the development. Objector took measurements of their own from their property and query how robust the outcomes of the report are. They believe that "Green" and "Amber" zones to the south elevation should be altered to "Red" and that this will necessitate a substantial redesign of the façade elements.
- Disruption during Construction to the highway network due to the constrained nature of the site, query over where contractors will park, how deliveries will be made to the development site, where a crane will be located and the noise that will occur during construction and why the noise report did not address this.
- Where are the plans to improve local facilities and infrastructure?
- Apart from 466-468 being a listed building and all that this entails, the facade lends an air of past grandeur to Moss Lane East and is pleasing to the eye. A tower block would not enhance the building; on the contrary it would obscure it and take away from its visual appeal.
- There is no green space and the size of the building is imposing and off balance for the plot of land.
- In an email sent to neighbours following the submission of the application the Moss Side Councillors state they "are now inclined not to lend any further support for the planning application". The Planning Statement cited the "support" of the Councillors, and included a letter of support from them.

Comments made supporting the scheme were as follows:

- Welcome this development as business relies heavily on student trade.
- Improves the street, making it much more pleasant which is only a benefit for our community. Perception that this area seems to be forgotten about when it comes to regeneration, so nice to see improvement.
- Excited to see proposed improvements along Heald Place and Parkfield Street, parking is currently a nightmare and getting rid of the flats and replacing with a car free student scheme should help reduce parking pressure. Had concerns about litter from new students but the developers assured them during their door knock that they would provide bins. Good to see positive change.

Platt Claremont Residents Association

Object on the following grounds:

Does not provide for family housing;

Loss of the flats currently on site (despite their occupancy);

Development will not “free up” existing House in Multiple Occupancy due to ambitious growth targets of universities;

This is not a student zone, there are some Houses in Multiple Occupancy but there are family homes;

No provision for car parking provision for disabled students;

Serious parking problem in vicinity of development site;

Cycle Parking inadequate due to location in basement, double decker storage, lack of access for disabled cyclists, entrance for cycles on Heald Place which is dangerous for pedestrians and cyclists;

Will there be no food recycling?; and

Visual replacement of a beautiful white villa frontage and another 2 attractive (though currently quite shabby) Victorian brick semis by this this very ugly tower block is very depressing. The white listed building will be completely swamped (as can be seen in the design mock-up). All the buildings fronting the main section of Whitworth Park were once very grand and it is a big shame that they might be replaced with such mediocre modern design. 9 storeys is higher than any comparable building nearby and feels completely wrong in this location.

Moss Side Tenants Union

Object on the following grounds:

1. The size is overly dominant and out of place for what is a residential area of small family houses. It is at odds with the height of existing buildings. The building is overly dominant and residents will lack privacy. The height of the building will allow noise to carry.

2. Lack of parking facilities may will impact availability of parking meant to support local businesses, fail to provide for those who need a car due to disability, place pressure on local parking and the drop off zone on Heald Place will bring late night disturbance.

3. It decreases the availability of housing for local permanent residents in an area of housing need.

4. This is primarily a family area and the council should not allow the increasing studentification that this development will bring.
5. The accommodation offers cramped living space in the kitchens/ living/ dining rooms of several clusters and some of the basement studios may have inadequate natural light.

Great Southern to Western Community Action Network

Object on the grounds of:

The scale and occupation density of the proposed building, it's impact and the lack of pre-application consultation.

The size of the units for students occupying the development.

Impact on view from Whitworth Park and Gallery.

Accuracy of documentation submitted to accompany the planning application.

Impact on the privacy of local residents.

Impact on parking in the area and the associated impact on proposed resident's parking scheme.

Loss of flats within the existing buildings.

Ward Members

Councillor Emily Rowles

The Moss Side Councillors raised several concerns prior to the application where conditional support for the application was given.

At the initial meeting:

- The height of the proposal was noted to be too high for the area
- The design was noted to not be in keeping with the area
- The public realm improvements as discussed were central and critical to any support
- The impact of the scheme in terms of parking and the forthcoming residents parking scheme was discussed
- Concerned that these have not been appropriately addressed in the final planning application.

Highway Services

CONTEXT - The site is bound by Moss Lane East, Heald Place and Parkfield Street, all of which are 20mph adopted highways with adoption extending to the back of footway. No Waiting At Any Time restrictions are currently in place within the vicinity of the Heald Place and Parkfield Street junctions and at various locations along Moss Lane East. No Waiting Mon-Sat 8am-6.30pm restrictions are also in place along the opposite side of the site along Moss Lane East. The highways surrounding the site are proposed to be included in the Rusholme and Moss Side Residents Parking Scheme which is programmed to commence mid-2023. At this stage, it is anticipated that in the immediate vicinity of the site, parking will be permitted for a maximum of 1hr (no return within 1hr Mon-Sun 7am-midnight) along Moss Lane East with no exemption for permit holders. The double yellow-line waiting restrictions running along the site's eastern frontage onto Heald Place will be retained as part of the parking scheme.

HIGHWAYS RESPONSE TRIP GENERATION AND JUNCTION CAPACITY - Given the car-free nature of the proposed development, the proposed use and accessible location, it is accepted that the proposed development is likely to generate only a negligible increase in vehicle trip generation. As such, any slight increase in trips can be absorbed on the existing network. No multi-modal trip generation has been provided within the submitted Transport Statement. Details of trips by all modes are required in order to determine whether any improvements to pedestrian, cycle and public transport infrastructure surrounding the site are necessary.

SITE ACCESSIBILITY & TRAVEL PLAN: The submitted Transport Statement demonstrates that the site is well accessed by sustainable modes of travel and is located within walking/cycling distance of various amenities and University of Manchester Campus buildings. It is noted that the majority of residents will be members of the UoM. A Framework Travel Plan has been submitted with the application. However, it is recommended that the development, submission, implementation and monitoring of a full Travel Plan within 6 months of occupation be attached as a condition of any planning consent.

ACCESS & OFF-SITE HIGHWAY WORKS: A rear car parking area associated within the existing site is currently accessed via Heald Place. Submitted plans show that the access via Heald Place will not be retained although the proposed dropped-kerbs will allow for the transfer of bins directly from the building onto Heald Place for servicing. For the avoidance of doubt, all external doors (with the exception of fire doors) should be inwardly opening, in order to prevent obstruction to passing footway users.

PARKING: It is understood from the application that this development will be a car free, therefore there will be no on site provision offered. Given the nature of this development, low levels of vehicle ownership are anticipated and given its accessible location, this is deemed to be acceptable to Highways. It is also understood that residents will not be permitted to bring cars to the site (or the surrounding areas) under the terms of their tenancy agreement. However, it is recommended that the operator monitors demand for potential leased parking with nearby operators as part of the Travel Plan Strategy to cater for any staff parking demand. As the site is located adjacent to a proposed Residents Parking Scheme, an amendment to the order will be required to exclude these residents from obtaining permits. No reference has been made to provision for disabled users and therefore further commentary is sought as to the existing and proposed disabled provision. No reference to Car Club provision has been referenced within the Transport Statement of Framework Travel Plan. The use of which should be fully promoted through the Travel Plan. The Transport Statement has given consideration to the existing pay-and-display car park located on-site and it is accepted that existing parking demand could be relocated within other local car parks.

PICK-UP/ DROP OFF: Although the Transport Statement has made some reference to residents moving in/out during key enrolment periods, given the proposed scale of the development and lack of on-site parking provision, this should be picked up fully as part of a resident pick-up/ drop_off management plan to be conditioned with any future consent.

CYCLE PARKING: 70no. cycle spaces are proposed, equating to a provision of 27% and is in accordance with BREEAM standards. Whilst this level of provision is acceptable, cycle parking demand should be monitored as part of the Travel Plan with additional cycle parking provision provided if necessary. The proposed cycle parking area will be located to the basement of the building with access provided via a lift and therefore provides sheltered and secure provision.

REFUSE AND SERVICING: The proposals include a dedicated bin store located within the basement of the building. The submitted Transport Statement states that refuse collections will be made from Heald Place and bins will be placed on Heald Place to await servicing. Whilst Highways have no objection to refuse servicing from Heald Place, the placement of numerous Eurobins onto Heald Place await servicing is likely to cause an obstruction to pedestrians and possibly motor vehicles should bins be placed on the footway or carriageway. It is therefore requested that bins are stored within an external area within the curtilage of the site when awaiting servicing.

CONSTRUCTION MANAGEMENT: A Construction (and Demolition) Management Plan should be provided by the applicant prior to any construction works beginning.

SUGGESTED CONDITIONS - S278 (Footway reinstatement, TRO extension & resident parking scheme TRO amendment) - Travel Plan - Resident Management Plan - CMP

TFGM COMMENTS: - The road accident data analysis should be updated to include the five-year period up to the whole of 2021 and possibly some of 2022 if this is available. The analysis area should include the junction with Oxford Road. - Suitable TROs should be in place to deter on-road parking, as site observations suggest that the surrounding streets are subject to significant levels of on-street parking. - Swept path plans should be provided to demonstrate that the proposed servicing arrangements can be undertaken safely and appropriately. - Any redundant access points should be reinstated accordingly. - Any access points or junctions surrounding the site should benefit from tactile paving and dropped kerbs. - A parking management plan should be employed to review how drop off and picks up will be managed on the network. This could also be covered within a Travel Plan. Review the potential for a loading bay. - Cycle parking should be provided on site and where possible a 1:1 provision be secured. The spaces should be secure and suitable for overnight accommodation, with sufficient lighting and CCTV. - Travel Plan conditioned.

UPDATED HIGHWAYS RESPONSE 15.05.2023 The applicant has submitted a Highways Technical Note (TN) in response to MCC Highways and TfGM comments. A multi-modal trip generation assessment has been provided demonstrating that the level of vehicular activity is anticipated to be low given the car free nature of the development proposals. Whilst multi-modal trip generation data has been provided, no further assessment/audit of pedestrian trips/routes has been provided. Based on the level of pedestrian trips (around 200 two-way trips per day), Highways request that the applicant undertakes a further assessment of such routes in order to identify where dropped-kerb/tactile paving provision is required to support pedestrian movement. Such improvements should form part of the conditioned S278 works. An Accident Analysis has been undertaken and shows that there are no existing trends

or deficiencies that would be affected by the development proposals. It has been confirmed that the existing access to the site from Heald Place will be amended and utilised as pedestrian footway with footway reinstatement and an extension of double-yellow line waiting restrictions. The TN states that it is understood that all external doors which open on Heald Place will open inwardly to prevent any potential obstruction to passing footway users, however this does not align with what is shown on submitted plans. Highways suggest that plans are amended to show inward opening doors (excluding fire doors). The applicant has confirmed that residents of the proposed development will be excluded from obtaining permits for the proposed residents Parking Scheme. An amendment to the TRO will be required via S278 agreement, to be funded at the applicant's expense. As the proposals are car-free, no disabled parking provision is proposed. It is recommended that the applicant provides further justification including an assessment of nearby disabled parking provision. The proposed (27%) level of cycle parking provision is considered acceptable by Highways given that any additional demand would be provided for as part of the conditioned Full Travel Plan. Whilst refuse servicing from Heald Place is considered acceptable by Highways, the operator must ensure that bins are not placed out onto the footway prior to/post servicing in order to maintain appropriate footway clearance for pedestrians. Bins should therefore be transported directly from the internal bin storage area for servicing. Highways still request that a resident pick-up/ drop-off management plan is conditioned with any consent. The applicant has confirmed that a CMP and Full Travel Plan will be submitted for approval prior to construction and within six-months of first occupation respectively.

Environmental Health

Recommend the imposition of conditions relating to Contaminated Land, Air Quality in accordance with the report submitted, Electric Vehicle Charging, Boiler information and Air Intake information, Construction Management, Refuse, Acoustic Insulation (residential and break out) and External Plant.

Neighbourhood Team Leader (Arboriculture)

The applicant has proposed to remove all existing trees on the site and replace with suitable replacements on a 1 for 1 basis. Arboriculture would not recommend any of the trees to be removed for TPO status. No objection subject to appropriate mitigation planting.

MCC Flood Risk Management

Recommend the imposition of conditions relating to Sustainable Urban Drainage and the maintenance thereof.

Parks & Events

Any comments received will be reported to committee.

Greater Manchester Police

Any comments received will be reported to committee.

Greater Manchester Archaeological Advisory Service

Archaeologically, the potential knowledge that could be gained from evaluation or excavation of former formal garden plots to the rear of the current buildings is negligible. Later re-use of these areas as car-parks is likely to have truncated any surviving features, and any artefacts, though potentially useful for informing on the discarded material culture associated with 19th century townhouses, would have limited research potential. Therefore GMAAS will not be seeking to impose archaeological requirements on the applicant.

The listed properties at 466-468 Moss Lane East have been extensively refurbished internally. This has likely led to the loss of many original features, fixtures and fittings of the property, along with impacts to its historic fabric that have eroded some of its previous significance. The listed building derives its principal significance from the high-quality and symmetrical architectural treatment to the front elevation (to be minimally affected by the proposals), unique within the area; whilst the limited significance internally is derived from surviving elements of the building's original configuration (to remain legible following development). With regard to 470-472 Moss Lane East, proposed demolition will amount to the full loss of buildings within the immediate setting of the listed structures. The Planning Statement states that 470-472 are regarded as non-designated heritage assets within the Heritage Statement, but GMAAS agree with the statement in paragraph 6.20 of the Planning Statement that 'although the Victorian-era villas are of good quality construction and likely to have been relatively prestigious of the time due to scale and detailing, this is only significant within a local context as there are many comparable buildings throughout Manchester'. GMAAS agree with this conclusion and as such will not be asking for mitigation due to the relatively small potential for knowledge gain.

Greater Manchester Ecology Unit

Recommend the imposition of conditions relating to Bats, Breeding Birds and Biodiversity Net Gain.

National Amenity Societies

Victorian Society object to the proposal, which they consider would cause substantial harm to the setting of the Grade II-listed building.

466 and 468 Moss Lane East are a handsome pair of white-painted stucco on brick townhouses with good classical details built in c.1840 to the south of Whitworth Park. Historically, this green space has been bordered by two- to three-storey townhouses and villas, politely set back from the road with good-sized front gardens. Buildings vary in style from classical to Gothic and largely date from the 19th century.

In contrast, the proposed massing of the nine-storey building would tower over Moss Lane East, actively eroding the attractive, domestically scaled streetscape bordering Whitworth Park.

Furthermore, the proximity of the proposed building to the Grade II-listed 466 and 468 Moss Lane East would cause substantial harm to the historic setting of this heritage asset. Handsomely set within its own grounds, in splendid isolation, the

heritage asset would be blocked in by the walls of the proposed building and reduced to a porch servicing the proposed building.

The demolition of the neighbouring unlisted, late 19th-century villa, 470 Moss Lane East, is wholly unnecessary. The present building provides an ideal setting for the neighbouring Grade II-listed building and blends in with the current streetscape. This building also appears to be in good condition, and alternative uses could easily be found.

Paragraph NPPF 200 states: "Any harm to, or loss of the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting) should require clear and convincing justification." The application does not provide adequate justification for the site-specific location of this scheme and the subsequent harm to a designated heritage asset.

The proposal cites Manchester's Core Strategy Policy on targets for the construction of purpose-built student accommodation and that it is "one of the last opportunities along Moss Lane East to develop a building suitable for purpose-built student accommodation". Given the nearby proximity of seven purpose-built student accommodation blocks, it could be argued that the immediate area surrounding Whitworth Park is already well-catered for. Moreover, Manchester's core strategy on building purpose built student accommodation is city-wide, with developments encouraged within the 'Oxford Road Corridor area', an area the proposed building would sit outside of. With some basic research, a much more appropriate site could be found. The justification for the harm to the setting of this designated heritage asset is unconvincing and therefore unwarranted.

Emphasis needs to be put on the importance of reusing and recycling buildings such as 470 Moss Lane East, as per Historic England guidance (<https://historicengland.org.uk/whats-new/news/recyclebuildings-tackleclimate-change/>). In addition to the Government's legally binding target to reach net zero, Manchester City Council's climate emergency declaration resulted in a commitment to halve carbon emissions by 2024. The demolition and replacement of 470 Moss Lane East would be wasteful and generate significantly more carbon emissions compared to its reuse. The proposals are therefore also contrary to the Council's commitment to tackle climate change and should be refused on this basis.

Health & Safety Executive (Land Use)

HSE raise the following:

- Means of escape including connection of single staircase with basement.
- Means of escape including connection of single staircase with ancillary accommodation.
- Means of escape including descend of lift shaft associated with single staircase to basement.
- Fire service access routes

Cadent

No objection subject to the imposition of an informative relating to proximity to their infrastructure.

United Utilities

Recommend the imposition of conditions relating to Sustainable Urban Drainage and the maintenance thereof.

Policy

Local Development Framework

The principal document within the framework is the Manchester Core Strategy which sets out the spatial vision for the City and includes strategic policies for development during the period 2012 – 2027.

'The Core Strategy Development Plan Document 2012 -2027 ("the Core Strategy") was adopted by the City Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) as the document that sets out the long term strategic planning policies for Manchester's future development. A number of UDP policies have also been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must therefore be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents.'

The following policies within the Core Strategy are considered relevant:

Policy SP1 (Spatial Principle) refers to the key spatial principles which will guide the strategic development of Manchester together with core development principles. It is stated that developments in all parts of the city should create well designed places which enhance or create character, make a positive contribution to the health, safety and well-being of residents, consider the needs of all members of the community and protect and enhance the built environment. Further, development should seek to minimise emissions, ensure the efficient use of natural resources, reuse previously developed land wherever possible, improve access to jobs, services and open space and provide good access to sustainable transport provision.

Policy DM1 (Development Management) states that new development should have regard to more specific issues for which more detailed guidance may be given within supplementary planning documents. Issues include: the appropriate siting and appearance of development, the impact upon the surrounding area, the effects on amenity, accessibility, community safety and crime prevention, health, the adequacy of internal accommodation and amenity space and refuse storage/collection.

Policy H1 (Overall Housing Provision) - New housing will be predominantly in the North, East, City Centre and Central Manchester, these areas falling within the Regional Centre and Inner Areas of Manchester. High density developments (over 75 units per hectare) are appropriate in both the City Centre and parts of the Regional Centre given the accessible location.

Policy H5 (Central Manchester) - over the lifetime of the Core Strategy, will accommodate around 14% of new residential development. Priority will be given to

family housing and other high value, high quality development where this can be sustained. High density housing will be permitted within or adjacent to the Regional Centre (Hulme and the Higher Education Precinct) as well as within Hulme, Longsight and Rusholme district centres as part of mixed-use schemes.

Policy H12 (Purpose Built Student Accommodation) states that the provision of new purpose built student accommodation will be supported where the development satisfied the criteria below:

- 1. Sites should be in close proximity to the University campuses or to a high frequency public transport route;*
- 2. High density developments should be sited in locations where this is compatible with existing developments and initiatives, and where retail facilities are within walking distance. Proposals should not lead to an increase in on-street parking in the surrounding area;*
- 3. Proposals that can demonstrate a positive regeneration impact in their own right will be given preference over other schemes;*
- 4. Proposals should be designed to be safe and secure for their users, and avoid causing an increase in crime in the surrounding area;*
- 5. Consideration should be given to the design and layout of the student accommodation and siting of individual uses within the overall development in relation to adjacent neighbouring uses. The aim is to ensure that there is no unacceptable effect on residential amenity in the surrounding area;*
- 6. Where appropriate proposals should contribute to the re-use of Listed Buildings and other buildings with a particular heritage value;*
- 7. Consideration should be given to provision and management of waste disposal facilities;*
- 8. Developers will be required to demonstrate that there is a need for additional student accommodation or that they have entered into a formal agreement with a University, or another provider of higher education, for the supply of all or some of the bedspaces; and,*
- 9. Applicants/developers must demonstrate to the Council that their proposals for purpose built student accommodation are deliverable.*

Policy EC1 (Employment and Economic Growth in Manchester) looks to ensure priorities for economic growth, the Council will support significant contributors to economic growth and productivity including health, education, retailing, cultural and tourism facilities, and other employment generating uses.

Policy EC8 (Central Manchester) seeks to create a positive sense of place at the interface of the health and education institutions along Oxford Road and the residential areas, using design and public realm.

Policy T1 (Sustainable Transport) - The Council will support proposals that: - Improve choice by developing alternatives to the car. Promote regeneration and economic vitality by relieving traffic congestion and improving access to jobs and services, particularly for those most in need and for those without a car. Improve access to transport services and facilities in order to enable disabled people and people with mobility impairments to participate fully in public life. Improve pedestrian routes and the pedestrian environment.

Policy T2 (Accessible Areas of Opportunity and Need) states that the Council will actively manage the pattern of development to ensure that new development: is located to ensure good access to the City's main economic drivers, including the Regional Centre, the Oxford Road Universities and Hospitals and the Airport and to ensure good national and international connections. Is easily accessible by walking, cycling and public transport; connecting residents to jobs, centres, health, leisure, open space and educational opportunities. Particular priority will be given to providing all residents access to strategic employment sites.

Policy EN1 (Design principles and strategic character areas) The proposal is considered to be a high quality scheme in terms of its design and appearance that would enhance the regeneration of the area.

Policy EN3 (Heritage) The impact on the historic environment would be acceptable and this is considered in further detail in the report

Policy EN4 (Reducing CO₂ Emissions by Enabling Low and Zero Carbon Development) concerns reducing CO₂ emissions and states that where possible, new development and retrofit projects must be located and designed in a manner that allows advantage to be taken of opportunities for low and zero carbon energy supplies. The use of building materials with low embodied carbon in new development and refurbishment schemes is also sought.

Policy EN6 (Target Framework for CO₂ reductions from low or zero carbon energy supplies) - Applications for residential development of 10 or more units and all other development over 1,000 sq m will be expected as a minimum to meet prescribed targets, unless this can be shown not to be viable. This should be demonstrated through an energy statement, submitted as part of the Design and Access Statement. Such a statement will be expected to set out the projected regulated energy demand and associated CO₂emissions for all phases of the development.

Policy EN8 (Adaptation to Climate Change) - All new development will be expected to be adaptable to climate change in terms of the design, layout, siting and function of both buildings and associated external spaces. In achieving developments which are adaptable to climate change developers should have regard to the following, although this is not an exhaustive list: Minimisation of flood risk by appropriate siting, drainage, and treatment of surface areas to ensure rain water permeability Reduction in urban heat island effect through the use of Green Infrastructure such as green roofs, green walls, increased tree cover and waterways. The need to control overheating of buildings through passive design. The opportunity to provide linked and diverse green space to enhance natural habitats which will assist species adaptation.

Developers will be permitted to use green infrastructure elements such as green roofs, green walls, street trees and waterways to contribute to compliance with CO₂ mitigation under Policy EN6, subject to sufficient evidence to quantify their contribution to compliance.

Policy EN9 (Green Infrastructure) - New development will be expected to maintain existing green infrastructure in terms of its quantity, quality and multiple function. Where the opportunity arises and in with current Green Infrastructure Strategies the

Council will encourage developers to enhance the quality and quantity of green infrastructure, improve the performance of its functions and create and improve linkages to and between areas of green infrastructure. Where the benefits of a proposed development are considered to outweigh the loss of an existing element of green infrastructure, the developer will be required to demonstrate how this loss will be mitigated in terms of quantity, quality, function and future management.

Policy EN14 (Flood Risk) – refers to flood risk and amongst other issues states that all new development should minimise surface water run-off, including through Sustainable Drainage Systems (SUDS) and the appropriate use of green infrastructure.

Policy EN16 (Air Quality) – states that the Council will seek to improve the air quality within Manchester, and particularly within Air Quality Management Areas, located along Manchester’s principal traffic routes. Developers will be expected to take measures to minimise and mitigate the local impact of emissions from traffic generated by the development, as well as emissions created by the use of the development itself.

Policy EN17 (Water Quality) states that developments should minimise surface water run-off and minimise ground contamination into the watercourse construction.

Policy EN18 (Contaminated Land and Ground Stability) - The Council will give priority for the remediation of contaminated land to strategic locations as identified within this document. Any proposal for development of contaminated land must be accompanied by a health risk assessment.

Policy EN19 (Waste) states that the Council will require all developers to demonstrate the proposals consistency with the principles of the waste hierarchy (prevention, reduction, re-use, recycling, energy recovery, and disposal). Developers will be required to submit a waste management plan to demonstrate how construction and demolition waste will be minimised and recycled.

In addition to the above, a number of UDP policies have also been saved until replaced by further development plan documents to accompany the Core Strategy. Unitary Development Plan for the City of Manchester, 1995 (Saved Policies)
The below saved policies of the Unitary Development Plan are also considered relevant:

Policy DC19 (Listed Buildings) – The impact on the listed building is considered in detail in the report.

Policy DC26 (Noise) states that the Council intends to use the development control process to reduce the impact of noise on people living and working in the City. In particular, consideration will be given to the effect of new development proposals which are likely to be generators of noise.

National Planning Policy Framework

The central theme to the revised NPPF is to achieve sustainable development. The

Government states that there are three dimensions to sustainable development: an economic role, a social role and an environmental role.

The Framework underlines a “presumption in favour of sustainable development”. This means approving development, without delay, where it accords with the development plan and where the development is absent or relevant policies are out of date, to grant planning permission unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

Sections 4, 5, 11, 12 and 16 are considered relevant to the consideration of this Application.

Paragraph 197 in Section 16 states that in determining planning applications, local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 199 states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset’s conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 200 states that any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification.

Paragraph 202 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

National Planning Policy Guidance

The Government produced a suite of documents to act as a live resource which sets out advice and best practice on a wide range of planning issues following a detailed review of planning policy guidance as a way of streamlining policy.

The relevant sections/extracts of the NPPG are set out below:

Housing Needs of Different Groups

Strategic policy-making authorities need to plan for sufficient student accommodation whether it consists of communal halls of residence or self-contained dwellings, and whether or not it is on campus. Encouraging more dedicated student accommodation may provide low-cost housing that takes pressure off the private rented sector and

increases the overall housing stock. Strategic policy-making authorities are encouraged to consider options which would support both the needs of the student population as well as local residents before imposing caps or restrictions on students living outside university-provided accommodation. Local Planning Authorities will also need to engage with universities and other higher educational establishments to ensure they understand their student accommodation requirements in their area.

Noise - Local planning authorities' should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.

Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:

- engineering: reducing the noise generated at source and/or containing the noise generated;
- layout: where possible, optimising the distance between the source and noise sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
- using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
- mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.

Design states that where appropriate the following should be considered:

- layout – the way in which buildings and spaces relate to each other;
- form – the shape of buildings;
- scale – the size of buildings;
- detailing – the important smaller elements of building and spaces;
- materials – what a building is made from.

Air Quality – Guidance states that when air quality is considered relevant to a planning application, which includes when proposals:

- Give rise to potentially significant impact (such as dust) during construction for nearby sensitive locations;
- Significantly affect traffic in the immediate vicinity of the proposed development site or further afield; or
- Expose people to existing sources of air pollutants. This could be by building new homes, workplaces or other development in places with poor quality.

The Guide to Development in Manchester (SPD) (2007)

The Guide to Development in Manchester is a supplementary planning document which contains core principles to guide developers. The document offers design advice and sets out the City Council's aspirations and vision for future development and contains core principles to guide developers to produce high quality and inclusive design. The principles that development should seek to achieve, include,

character and context, continuity, and enclosure, ease of movement, quality of the public realm, diversity, legibility and adaptability.

The Manchester Green and Blue Infrastructure Strategy (G&BIS)

The G&BIS sets out objectives for environmental improvements within the City in relation to key objectives for growth and development. Building on the investment to date in the city's green infrastructure and the understanding of its importance in helping to create a successful city, the vision for green and blue infrastructure in Manchester over the next 10 years is: By 2025 high quality, well maintained green and blue spaces will be an integral part of all neighbourhoods. The city's communities will be living healthy, fulfilled lives, enjoying access to parks and greenspaces and safe green routes for walking, cycling and exercise throughout the city. Businesses will be investing in areas with a high environmental quality and attractive surroundings, enjoying access to a healthy, talented workforce. New funding models will be in place, ensuring progress achieved by 2025 can be sustained and provide the platform for ongoing investment in the years to follow.

Four objectives have been established to enable the vision to be achieved:

1. Improve the quality and function of existing green and blue infrastructure, to maximise the benefits it delivers
2. Use appropriate green and blue infrastructure as a key component of new developments to help create successful neighbourhoods and support the city's growth
3. Improve connectivity and accessibility to green and blue infrastructure within the city and beyond
4. Improve and promote a wider understanding and awareness of the benefits that green and blue infrastructure provides to residents, the economy and the local environment.

Places for Everyone Greater Manchester Joint Development Plan (Draft August 2021)

The draft version of the Places for Everyone Joint Development Plan was published in August 2021 and has been produced by Greater Manchester Combined Authority to provide a long-term plan for jobs, new homes, and sustainable growth for nine of Greater Manchester's districts. Once the Places for Everyone Plan is adopted it will form part of Manchester's development plan. As this plan is at an advanced stage it would now be considered as a material consideration for planning applications.

Manchester City Council Report for Resolution - Executive: Purpose Built Student Accommodation Manchester (May 2023)

This report acknowledges the significant economic contribution students make to Manchester whilst they live and study in the city and that providing a residential offer for students to address the needs has been a long held Council objective as part of the housing strategy and planning policy framework (policy H12). It also recognises the development of assets within the Oxford Road Corridor area is vital to capture the commercial potential of research and innovation and help to realise the economic potential of the Corridor.

The report also acknowledges that there is an unmet demand in the city and a high-quality residential offer for students in appropriate locations, is critical for Manchester's Universities ability to attract and retain students in a global market and confirms that accommodation should be located in the areas immediately adjacent to the core university areas, principally the Oxford Road Corridor area.

The approach to the provision of PBSA was endorsed to help guide decision making and the committee are requested to take this into account as a material consideration.

Legislative Requirements

Section 16(2) and 66(1) of the Planning (Listed Building and Conservation Areas) Act state that decision makers must have "special regard to the desirability of preserving the building or setting or any features of special architectural or historic interest which it possesses."

Principle

The proposed development relates to a site in a highly sustainable location, close to the main Manchester University campuses, within a reasonable walking distance of the Oxford Road corridor and university buildings.

In principle a proposal of high-quality design of an appropriate scale and mass in this Location which positively contributes to the student accommodation supply in the city, which by meeting a recognised quantitative and qualitative need for such accommodation and would reduce reliance on houses in multiple occupation, particularly in south Manchester is acceptable.

Not only would the proposal help to provide new student accommodation, it would deliver significant economic, social and environmental benefits including job creation, spending in the local economy, provide a catalyst for further regeneration in the area, and upgrade the appearance of the site, whilst reutilising a Grade II Listed Building. On this basis, the proposal is considered to accord with relevant policy and guidance.

However, there are specific planning issues, which need to be considered, this includes the specific requirements of policy H12 (new PBSA), the impact to amenity and the operation of the highway and the impact on the designated heritage asset are considered further below.

Principle of Purpose-Built Student Accommodation (PBSA)

Policy H12 of the Core Strategy is the main overarching development plan policy for PBSA, which should also be read in the context of the policy position on PBSA endorsed by the Executive Committee in May 2023. The report recognises that H12 remains an affective policy position, whilst demonstrating that there are opportunities to meet the significant demand by identifying potential sites for new PBSA.

Subject to the criteria in the policy, such development should be supported. Priority is

to be given to schemes that are part of the universities' redevelopment plans or which are being progressed in partnership with the universities, and which clearly meet the Council's regeneration priorities.

In this regard Policy H12 lists criteria developers are required to demonstrate that there is a need for additional student accommodation or that they have entered into a formal agreement with a university, or another provider of higher education, for the supply of all or some of the bedspaces.

Taking each of the 10 criteria in turn, the following commentary and assessment is provided and which sets out why each policy requirement is met:

1) Sites should be in close proximity to the University campuses or to a high frequency public transport route which passes this area.

The site is close to the university campuses and within reasonable walking and cycling distances to the nearest university buildings. The site is also a short distance from the Oxford Road Corridor which links the site with the universities and which accommodates a number of high frequency bus services to the universities and beyond.

2) The Regional Centre, including the Oxford Road Corridor, is a strategic area for low and zero carbon decentralised energy infrastructure. Proposed schemes that fall within this area will be expected to take place in the context of the energy proposals plans as required by Policy EN5.

An Environmental Standards Statement submitted as part of the application outlines how the proposed development would exceed the minimum standards set in current Building Regulations and also targets a BREEAM 'Excellent' rating. The scheme would include the installation of rooftop photovoltaics.

3) High density developments should be sited in locations where this is compatible with existing developments and initiatives, and where retail facilities are within walking distance. Proposals should not lead to an increase in on-street parking in the surrounding area.

The proposal is commensurate in scale to neighbouring buildings, including existing student accommodation. The site is close to and within a close walking distance to a range of amenities to the east along Oxford Road within Rusholme District Centre.

The location of the site is highly sustainable and accessible, within a reasonable walking distance of the universities and proximate to a range of bus services. The development would be marketed as car free. Ample cycle parking provision is contained within the site.

It is not anticipated that the proposal would lead to an increase in on-street parking. Tenancy agreements would stipulate that no provision is made for vehicle parking and the local residents parking scheme would exclude prospective occupiers.

4) Proposals that can demonstrate a positive regeneration impact in their own right

will be given preference over other schemes. This can be demonstrated for example through impact assessments on district centres and the wider area. Proposals should contribute to providing a mix of uses and support district and local centres, in line with relevant Strategic Regeneration Frameworks, local plans and other masterplans as student accommodation should closely integrate with existing neighbourhoods to contribute in a positive way to their vibrancy without increasing pressure on existing neighbourhood services to the detriment of existing residents.

The proposed development would facilitate a number of regenerative benefits including, the redevelopment of a brownfield site, contributing to the shortfall in student accommodation in an appropriate and accessible location, the provision of affordable student accommodation, enhancing the appearance of the site, improving connectivity with the Oxford Road corridor and direct/indirect investment in the local economy. The construction of the development is likely to provide in the region of 115 gross jobs at full time equivalent.

5) Proposals should be designed to be safe and secure for their users and avoid causing an increase in crime in the surrounding area. Consideration needs to be given to how proposed developments could assist in improving the safety of the surrounding area in terms of increased informal surveillance or other measures to contribute to crime prevention.

As outlined within the submitted Crime Impact Statement, the proposed development has been designed to be secured by design principles. The facility will be managed by a company with experience in managing of student accommodation in Manchester. Management staff will be present throughout the day, including at a reception desk overlooking the principal entrance. Student representatives will be resident in the accommodation.

Increased pedestrian footfall and public realm improvements would improve natural surveillance to the immediate area and bring increased activity to local streets, to the benefit of public safety.

6) Consideration should be given to the design and layout of the student accommodation and siting of individual uses within the overall development in relation to adjacent neighbouring uses. The aim is to ensure that there is no unacceptable effect on residential amenity in the surrounding area through increased noise, disturbance or impact on the street-scene either from the proposed development itself or when combined with existing accommodation.

The site layout, design, scale and appearance are considered acceptable and would be sympathetic to its surroundings as set out further in the report. The daylight and sunlight report and addendum report set out that the development is acceptable following technical analysis. The impacts of the development on the outlook and privacy of neighbouring residents have been assessed and given the separation distances and existing characteristics of the area (including positions of windows) there would be no unacceptable impacts on residential amenity. In terms of noise disturbance the majority of activity would be via the Moss Lane East and Heald Place entrances which are situated away from residential properties. A Management Plan which would proactively manage student behaviour through 24/7 on site

management presence. Conditions would be put in place with regards to the breakout of noise from the operation of the building or any associated equipment.

7) Where appropriate proposals, should contribute to the re-use of Listed Buildings and other buildings with a particular heritage value.

The listed building on site would be reutilised and refurbished as part of the redevelopment of the site, a sensitive conversion that would secure the long term viability of a heritage asset. It is acknowledged that the adjacent semi detached properties would be demolished; this is considered in more detail in the report.

8) Consideration should be given to provision and management of waste disposal facilities, that will ensure that waste is disposed of in accordance with the waste hierarchy set out in Policy EN 19, within the development at an early stage.

A waste Management Strategy provides details of the number of receptacles and recycling arrangements, along with details of on-site management and collection. A dedicated refuse store is provided within the site. Refuse collection would take place from Heald Place. Further details are required by condition with regards to the collection of organic waste, there is a commitment from the applicant to provide this.

9) Developers will be required to demonstrate that there is a need for additional student accommodation or that they have entered into a formal agreement with a university, or another provider of higher education, for the supply of all or some of the bedspaces.

The Executive Report to Committee, May 2023 acknowledges a clear need for new PBSA, with support given to new accommodation in close proximity to university campuses, such as that proposed.

The report references an earlier Executive Report to Committee in December 2020 which refers to evidence that suggests Manchester needs a more diverse pipeline of new PBSA to help stabilise rental growth and to meet the needs of students at an affordable price.

The report acknowledges that there is a need to replace existing poor quality stock and that PBSA development will not only provide accommodation for increasing number of students, but will also replace existing poor quality stock.

Given the policy background and the report to Executive which form important material considerations in decision making for new student accommodation, there is recognition that there is a need for further student accommodation in the city.

The applicant has also formally engaged with the University of Manchester who express support for the proposal and have provided a letter which has been submitted as part of the current application. On this basis, it has been satisfactorily demonstrated that the policy requirement has been satisfied.

10) Applicants/developers must demonstrate to the Council that their proposals for purpose-built student accommodation are deliverable.

The applicant has a track record of delivering similar accommodation throughout the UK. Based on the information provided, there is confidence that the scheme is deliverable.

Given the above, it is considered that the principle of development for this proposal is acceptable and complies with the criteria of policy H12 of the Core Strategy.

Demolition of the Existing Buildings

The existing site contains a pair of semi detached villas 470-472 Moss Lane East that are to be demolished as part of this development. The site is not located in a conservation area and the existing buildings are not listed, however, the submitted heritage statement acknowledges that they are non-designated heritage assets. Whilst the building contains a degree of aesthetic value when viewed from Moss Lane East, the submitted report concludes that the building is not of any special architectural merit or significance. The building has been significantly altered internally and provides poor quality accommodation. The building would not lend itself to a realistic conversion opportunity for the use proposed and the reuse of the building would not aid longevity of use or maximise the development potential of the site.

On this basis, the loss of the existing building is considered acceptable. The re-use of the building for student accommodation does not represent a realistic option and would not represent an efficient use of land and would not deliver the regenerative and environmental benefits offered by the proposed development. The impact of the demolition on the setting of the listed neighbouring villas is dealt with elsewhere on this agenda.

Whilst the residential flats that are within 466 – 468 and 470-472 Moss Lane East would be lost as a consequence of the proposed development, these flats do not presently offer a good standard of accommodation, being utilised predominantly as student houses in multiple occupation, Air B and B's student houses and the remaining properties as flats, with one vacancy. It is considered that the loss of these flats would be significantly outweighed by the benefits offered by good quality and amount of units much needed purpose built student accommodation. The applicant confirmed that one longstanding resident has been provided with alternative accommodation and where tenancies have ended these are now on rolling contracts.

Redevelopment of the Site and Contribution to Regeneration

The proposal would redevelop a brownfield site to support a viable redevelopment, with any perceived harm outweighed by bringing the site back into effective use to the benefit of the area, including through job creation and benefits to the local economy via increased expenditure at local businesses.

On balance, the proposal would have a positive and beneficial effect on the area. The high standard of design would enhance the character and appearance of the area and contribute to the ongoing regeneration of the area, providing a catalyst for future development.

There is a strong link between economic growth, regeneration and the provision of a range of residential accommodation. A key objective of development plan policy and relevant material considerations aims to deliver the accommodation and infrastructure needed to attract students to Manchester which matches its reputation

as a world class place to study, in order to ensure Manchester remains competitive on a global higher education stage. This proposal would support such a key objective.

Amount of Development

The proposed floorspace into a mix of studios and en-suite rooms set within clusters bedrooms resulting in 261 students bedrooms.

The studios are typically sized at 17.4 sqm (accessible studios ay 20.9 sqm) and cluster rooms (en-suite) 13.2 sq m and a shared kitchen/dinner (one per cluster flat) at 33.7 sqm.

Site Layout

The proposed development comprises the retained Grade II listed 466 – 468 Moss Lane East building, which links via a ground floor glazed link extension to a building ranging from 7-9 storey in height. The building runs along the Moss Lane East frontage on the same building line as the listed building.

Principal access into the site is gained from Moss Lane East, with a secondary entrance off Heald Place, with a dedicated drop off point in this location for servicing drop off/pick up purposes.

The ground level makes provision for communal areas to the north of the site fronting Moss Lane East, to the south of the site the ground floor includes student beds. The cycle parking and refuse storage would be accommodated within the basement. All of the other 261 student bedrooms would be accommodated in the upper floors. The upper floors also include amenity provision for the occupants of the building.

The building would be softened by planting, landscaping and a communal courtyard. It is considered that in this location, the proposed layout provides a satisfactory balance between built form, and amenity space/soft landscaping.

The arrangement maximises the main street frontage, with the proposed development fulfilling the potential of the site, without compromising the character and appearance of the area. The impact on the setting of neighbouring buildings follows below.

Design, Scale and Appearance

The immediate context largely comprises buildings ranging from two to six storeys in scale, with neighbouring buildings informing the context of the proposed development.

The proposed part seven storey, part eight storey, part nine storey building has an articulated façade and stepped height and massing to break down the scale of the building and reflect the variation in height of buildings in the area. The building is larger to the east where the building is associated with existing purpose built student accommodation situated towards the Oxford Road Corridor. The scale reduces in

height to the west in relation to the height of Grade II Listed Building (itself a two storey building with basement and room within the roof space) separated by a lightweight glazed link and in relation to the residential property to the west and south, with upper floors set back to decrease the sense of scale at street level and to provide separation distances.

The ground floor will be taller than those above to replicate the more classical proportions of the Grade II Listed Building. The window proportions have been influenced by those of the existing Georgian House, with a strong horizontal and vertical banding.



Scheme as viewed from Whitworth Park

It is believed that the proposed built form appropriately responds to the scale of its context and involves variations in height to integrate sympathetically to neighbouring buildings. Similarly, the massing relates appropriately to the surrounding context, with the massing of the building to the east relating to buildings of scale associated with Oxford Road Corridor and reducing in scale and massing to the west in relation to the more residential context to prevent any over-dominance. Regard has been had to policy EN1 of the Core Strategy which encourages buildings of higher density on arterial routes into the City Centre.

**North Elevation****South Elevation****East Elevation****West Elevation**

In terms of appearance, the design is considered to offer quality of design. Specifically in relation to the depth of window reveal allowing for increased articulation, light and shadow.

The elevations include brickwork detailing, recessing and banding to provide texture and depth to the buildings.

In terms of materials, the proposed development will use a limited palette of high quality materials such as facing red bricks, with elements expressed in a hit and miss red/brown brick texture and recessed anodised aluminium panels with large areas of glazing. The exact specification of the materials would be agreed by way of an attached planning condition.

It is considered that the resultant development would achieve a cohesive, high quality design which would rationalise the existing space and improve the character of the street-scene and local area.

Accessibility

The Grade II listed building will be adapted internally with the inclusion of a lift, whilst the new building will be built with level access. The entrance leads to lifts and the internally circulation cores and units are proposed to Part M standards. The scheme would include 5 accessible / adaptable units which is above the requirement set out in Part M. The landscaping would be level, other than to the access to the existing listed building. Routes within the site have been designed to be well lit and hazard free for wayfinding. Three off site disabled car parking bay would be secured through a planning condition.

Amenity Space

Residents would have access to a range of amenity space, including an external courtyard between the buildings and the retained setting for the Grade II Listed Building to the Moss Lane East frontage (440sqm of landscaped external amenity areas), study booths, collaboration spaces, residents lounges, cinema space, gym, games area, entertainment space, laundry facilities and a well being room. Given the nature of the development and the constraints of the site, the level of amenity space is considered acceptable.

Impact on the designated and non-designated heritage assets

The Grade II listed building (No 466-468) would be retained and converted to student accommodation. The reconfiguration has been designed with the key principals of retaining as much separation between the two self-contained villas as is possible and a retention of historical divisions within the building where possible. The proposals will also require the insertion of an opening into the listed building in the east wing and a single interface between the listed building and the proposed new building at this point to allow access between the two.

It is agreed that the approach to the conversion of the listed building involves a sensitive intervention which seeks to retain, protect and reveal as many of the building's original features as possible.

The significance of the designated heritage asset relates solely to the façade facing Whitworth Park, the side and rear elevations were not constructed to the same ornate standard or including the same proportions. There are limited internal features which have been retained since the conversion of the property into apartments.

The division between the two original villas would be noticeable following the implementation of this scheme. A lift shaft and smoke riser would be installed in order to accord with Building Regulations and provide accessibility for all, this would remove historic fabric in terms of dividing floors/ ceilings, however this would be limited. The stairway within no. 468 would be reconfigured in a more sympathetic manner than present arrangements accessed via no. 466. There are original decorative features proposed to be retained such as cornices and skirting.

With regards to the setting of the listed building, the acknowledged element of significance of the heritage asset is the façade which is maintained, as is the setting for the façade behind a retained lawned areas with trees (to be retained / replaced) and hedging to be supplemented.

The building to the side and rear proposed is of significant scale and is acknowledged as detracting from the prominence of the listed building in that it does have an impact on the setting of the Listed Building. However, the harm caused is considered to be less than substantial when weighed against the tests set out in the NPPF and the substantial public benefits including the provision of purpose built student accommodation in a sustainable location.

The rear of the property is far more utilitarian in nature and the harm to the setting of the listed building when viewed from the rear is considered far less significant, albeit that the development proposed does lose a sense of space around the designated heritage asset when viewed from the rear.

The submitted Heritage Report concludes that the proposals will result in less than substantial harm to the significance of the listed building due to physical changes to its historic fabric and changes in the way the building will be experienced from Moss Lane East.

470-472 Moss Lane East is acknowledged in the report to be a non-designated heritage asset. The report concludes that although the Victorian-era villas are of good quality construction and likely to have been relatively prestigious of the time due to scale and detailing, this is only significant within a local context as there are many comparable buildings throughout Manchester. The dwellings have had significant alterations associated with the subdivision to flats. Although the development would result in the loss of this building, a balanced judgement is required as to the scale of the loss. Given the building is not listed and only of limited local significance the harm in heritage terms can only be deemed as limited.

The NPPF requires weighing the benefits of a scheme against the less than substantial harm arising from this proposal. It is considered the holistic benefits of the scheme outweigh the identified harm to the listed building. Especially having regards to the retention of the designated asset and the proposed treatment of that designated asset, which should be the subject of appropriate conditions.

It is therefore considered that the scheme would accord with the approaches set out within EN3 of the Core Strategy, saved policy DC19 of the Unitary Development Plan and Section 16 of the National Planning Policy Framework.

Impact to Archaeology

In respect of archaeology, a detailed Archaeological Desk-Based Assessment has been submitted in support of the application. The report confirms that there are no designated archaeological assets within the site, and there is no record of development on site prior to the construction of the existing detached buildings. Archaeological remains are considered to be limited to 19th century gardens which have been lost to hardstanding. GMAAS confirmed that they were satisfied and did not require the imposition of any conditions. The proposal is therefore acceptable with regards to policy EN3 and saved policy DC20.

Impact to the Highway

The development would be largely car free, with the exception of delivery and emergency vehicles and the three proposed on street accessible car parking spaces. It is accepted that the proposed development is likely to generate a negligible increase in vehicle trip generation, which can be absorbed by the network.

A 'move in / move out' strategy would be required in for students to book a timeslot for loading/unloading in order to minimise the impact on the local road network. A condition has been included which requires the submission and agreement of a more detailed moving in/moving out strategy.

Given the car free nature of the proposed development, coupled with the highly accessible location close to the universities and the characteristics of the local highway, it is not envisaged that there would be any significant, harmful impact to the operation of the highway as a consequence of vehicle movements, parking demand or to highway safety.

The proposal incorporates as a minimum, 74 secure cycle spaces in the basement cycle store. Delivery of additional spaces would be demand-led and identified through the ongoing operation of a Travel Plan.

Based on the car free travel options and the assessment of the highway undertaken, the impact upon the operation of the highway and to highway safety is considered satisfactory.

In terms of servicing, waste collection and deliveries/collection would take place on Heald Place. The servicing has been assessed by Highway Services as being satisfactory.

A condition requesting a further travel plan within 6 months of occupation is appended to monitor travel patterns.

The immediate area is subject to a resident's parking scheme. The applicant has confirmed that students will not be allowed to apply for a parking permit, and this would be reflected within tenancy agreements. If visitor parking is required there are off-site public car parks located nearby, as well as on-street time limited car parking which is available locally to meet any occasional demand or the requirements of any visitors to the site.

A condition is also recommended requiring the following highways requirements:
Footway reinstatement;
Extension of double yellow line restrictions;

Amenity

Given the location of the site, the relationship with neighbouring uses, together with the busy nature of the surroundings, the introduction of student accommodation in the area is not considered to have a significant, adverse impact upon the nearest residential occupiers in terms of existing background noise levels, levels of activity, overdominance or loss of daylight/sunlight.

No representation have been received from the neighbouring property to the south of the site within the community centre.

Objections from residents on Moss Lane East and within the residential estate to the south have raised concerns about the impact of noise generated by prospective student occupiers, as well as concerns relating to the building having an overbearing and oppressive impact providing overlooking. The impact to all neighbouring occupiers is set out below.

Sunlight/Daylight

The applicant has produced a Daylight and Sunlight Report to assess the impact on existing light levels upon neighbouring residential buildings. An addendum report was also submitted with further information.

The study uses the industry standard methodology as prescribed by the Building Research Establishment (BRE) and British Standard guidance.

The applicant undertook an exercise where they compared the impact of the proposed development to the relationships that exist in the immediate context, namely Rusholme Place and Ropemaker Court. The impacts were comparable to the development proposal due to the intervening separation distances.

Computer generated images of the impact on Vertical Sky Component (VSC) to windows to neighbouring properties are shown below. Those windows which are coloured yellow receive excellent daylight meeting BRE criteria. Those in orange/yellow follow marginally short of BRE guidance, but have good potential for daylight and sunlight. Those in red, purple and below will receive daylight below the target criteria.

Moss Lane East – Proposed VSC



The information shows that 5 of the 6 windows to the north elevation of 1-5 Heather Way would have a major adverse impact to Vertical Sky Component and No Sky Line. In line with BRE guidance we must consider which rooms are affected to understand the significance of impacts. The windows affected are north facing galley kitchen window and bedroom window to each property. Habitable windows face southwards away from the development site. The sensitivity of the windows and rooms assessed is therefore reduced.

The submitted information shows that daylight/sunlight and overshadowing impact to the neighbouring property is contextually similar to the developments that have been approved in the locality. This is an urban location where it is not always possible to meet targets. BRE altered their guidance in 2011 to recognise that different targets would be appropriate for different locations. Alternative targets would be generated from assessments of existing development in the area. The developer had developments approved at Rusholme Place and Ropemaker Court in this regard.

Assessment have also been undertaken for:

464 Moss Lane East;

Three windows do not meet the BRE target criteria for VSC daylight, two of which serve bedrooms that are considered by the BRE as having a lesser requirement for daylight. The remaining window serves a living room located on the ground floor which is provided with two alternative windows. The BRE states that where a room is served by multiple windows of similar size, the mean VSC for the room overall can be calculated. Both alternative windows meet the VSC daylight criteria and, when the mean VSC is considered, the room overall will be reduced by 20.9% VSC, which is marginally outside the target reduction of 20%. As such, the impacts on VSC daylight to this property are considered to be minor.

2 – 4 Parkfield Street;

With the proposed development in place, these properties will fully accord with the BRE target criteria for VSC and NSL daylight, and APSH sunlight.

14 Heald Place;

With the proposed development in place, these properties fully accord with the BRE criteria for NSL daylight and APSH sunlight.

One window does not meet the target criteria for VSC daylight. The window is reduced by 20.7%, against a target reduction of 20%, which is a minor impact.

14-20 Gateshead Close.

With the proposed development in place, this property will fully accord with the BRE target criteria for VSC daylight. No living rooms face within 90 degrees of due south so no APSH sunlight assessment is required.

Seven rooms will not meet the BRE target criteria for NSL daylight, five of which will experience a minor reduction of between 20-30%, which is considered acceptable given the context of the site. The remaining two rooms will be reduced by 33.3% and 33.2%, but both rooms will continue to receive adequate daylight in excess of 62% of the room area, which is again considered acceptable given the location of the site and the emerging height and density in the area.

On balance, it is believed that the proposed development demonstrates broad compliance with the BRE guidance and that the impact upon neighbouring sunlight/daylight levels can be adequately sustained.

Noise Impact

Due to the residential nature of the proposed development and the relationship with neighbouring buildings, it is not considered that there would any significant harmful effect on the living conditions of neighbouring occupiers with regard to general disturbance and noise.

An acoustic assessment forms part of the application which looks at the proposals compatibility with surrounding uses. The assessment concludes that the proposed development is unlikely to cause undue harm from associated noise and where there is the potential for some impact (i.e. plant) this can be managed and mitigated i.e. mechanical ventilation.

The assessment also indicates that any external noise impact on future occupants can be appropriately mitigated. Namely from the operation of the nearby hand car wash facility and the operation of the highway. The development would also be in close proximity to a Community Centre to the south utilised for prayer.

Environmental Health did not raise concerns with the findings of the submitted report and subject to the use of appropriate conditions, the impact of noise is considered to be satisfactory.

No private external areas are proposed such as balconies and it is not intended for there to be any terraces, a condition is appended to secure this commitment. The use of the external area to the site frontage and the land to the rear is currently uncontrolled. The benefit of the use of these landscaped shared amenity areas and surveillance provided by the use of these areas would outweigh any harm associated with noise from the use of these areas. The amenity areas fronting Moss Lane East would be subject to the noise from the use of the road as identified in the submitted report.

Disturbance

The proposed occupier operates student accommodation throughout the country and adheres to a Management Plan that establishes protocols to be adhered to. The focus would be on preventing incidents of excessive noise and anti-social behaviour. There would be 24/7 management. A 'helpdesk' which will be contactable at all hours for the benefit of both future occupiers and the local community. CCTV would be installed. Tenancy agreements would be worded so that any serious incidents or repeat complaints could lead to eviction.

Overlooking and privacy levels

3.12 Key Site Considerations

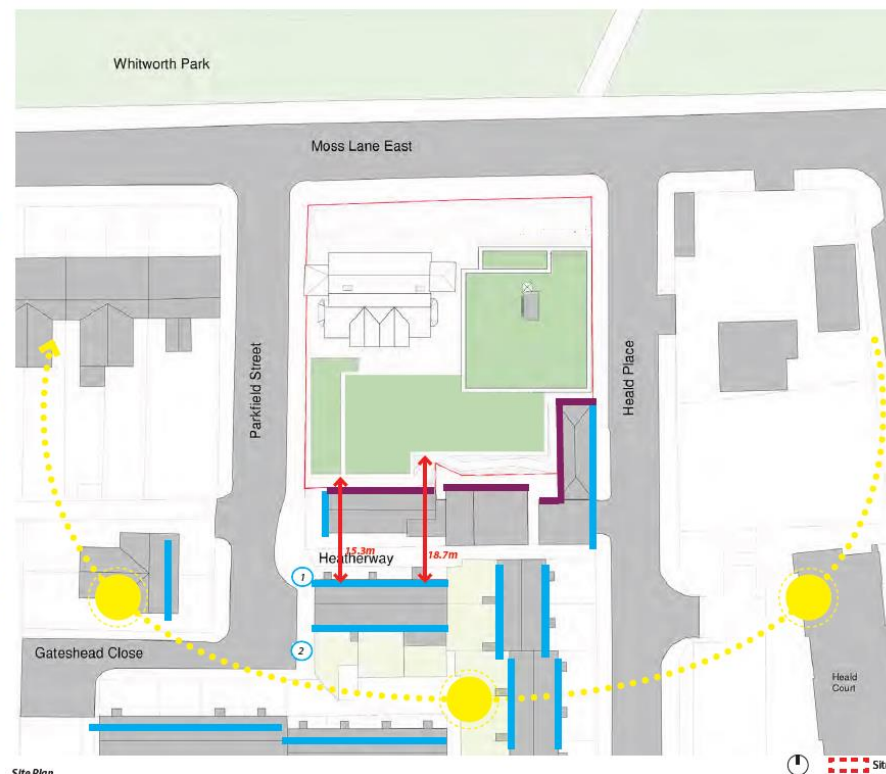


1
Gateshead Close Front Elevation



2
Gateshead Close Rear Elevation

- Neighbouring elevations with windows
- ⬅️ Sun path
- ↔️ Proximity between windows
- Blank elevations (no windows)



The proposal provides sufficient spacing between neighbouring buildings to maintain a reasonable interface distance between the proposed elements and that of the existing buildings. The nearest windows being the 6 windows mentioned in the above daylight/sunlight assessment to the north elevation of 1-5 Heatheyway, a galley kitchen window and a bedroom window to each property, being located some 15.3m – 18.7m away.

It is not therefore considered that the distance between habitable windows would give rise to harmful overlooking or any subsequent loss of privacy.

The design of the building, window positions and general façade design is considered to minimise the impact to neighbouring occupiers. No direct undue overlooking would result. Any impact is lessened by the presence of the intervening community hall which separates the site from the nearest residential property to the south. The community hall has no windows to the north elevation, the elevation facing Parkfield Street has 3 windows which would be in close proximity to the southernmost part of the development proposed, however, this element which would only c.1m forward of the building, giving limited opportunity for overlooking at an oblique angle.

The distances provided to the gable of 464 Moss Lane East (with 4 stained glass windows) to the west on the other side of Parkfield Street would be 13m across the highway. There are two polygonal windows to a bay to the rear elevation and a single storey rear element that is further set away from the side boundary, with two side facing windows. There would be some views available over the private garden area across Parkfield Street to the West but as stated above, this would be across a highway with a separation distance of 13m and the overlooking would be in keeping with the character of the wider area.

Further along Parkfield Street there would be a separation distance of c. 20m and above to windows at No. 2.

The development would sit opposite the existing hand car wash to Moss Lane East across the north part of Heald Place at a distance of c. 12.5m across the highway. The development proposed would sit behind existing commercial property on Heald Place to the south of the site, increasing separation distances to the other side of Heald Place where there is a setback student accommodation block known as Heald Court. The proposal would also sit to the north of residential property on Heald Place, however, No. 14 Heald Place currently has a gable with alleyway next to a high level wall associated with the commercial premises. Windows to the rear of these properties look to the west, views to the north interrupted by the community hall.

On this basis, the impact upon the living conditions and the amenity of neighbouring occupiers can be satisfactorily sustained.

Reception

A TV and Radio Impact Assessment was submitted to support the planning application. The report concludes that the development may cause very minor interference to digital terrestrial television reception in a limited localised area however a range of measures can successfully mitigate this if identified to be necessary following completion of the development. There are no identifiable impacts with respect to digital satellite television or radio signals. A condition is attached to protect the level of TV and Radio signal.

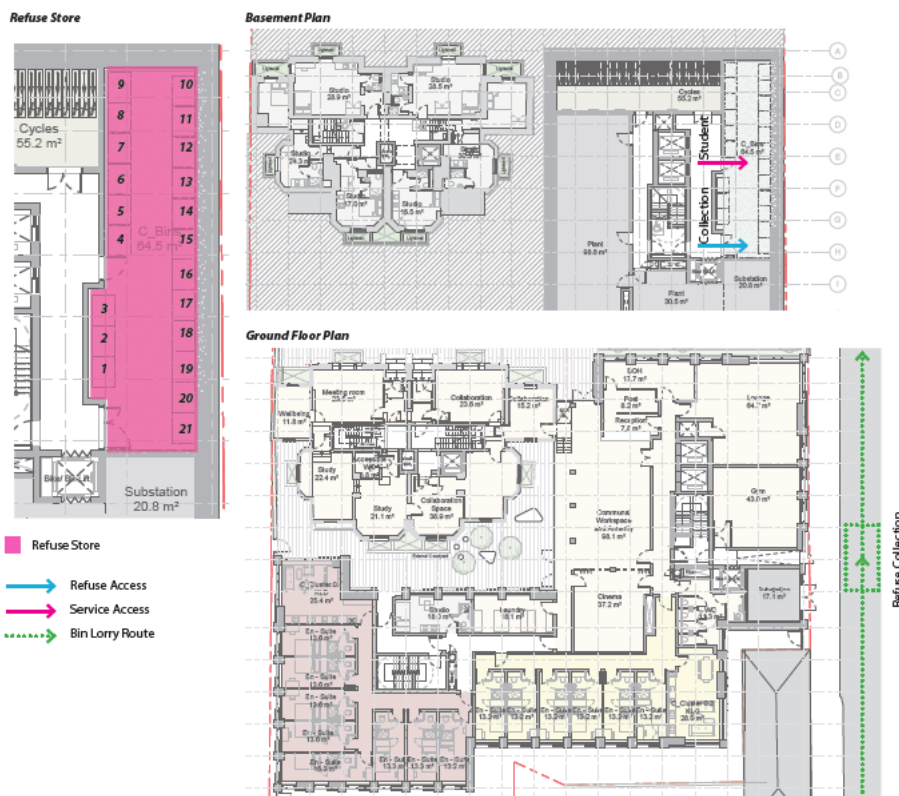
Waste Management

A detailed waste Refuse Strategy is submitted as part of the Design and Access Statement submitted with the application.

The on-site waste management strategy provides details an appropriate number and capacity of receptacles, recycling arrangements and details of on-site management and collection. Sufficient space is proposed to provide 5 general waste 1,100 litre Eurobins, along with 5 x 1,100 litre Eurobins for both pulpable and comingled recyclable waste.

The refuse store would be located in an enclosed area within the basement, with waste collected on a twice weekly basis from the collection point on Heald Place. On collection days, on-site management move waste containers from the refuse to the collection point on Heald Place to be collected by a private contractor. The applicant has acknowledged that more collections may be required at the beginning and end of terms, when students generate more waste.

Highway Services have confirmed that the proposed arrangements are acceptable. A condition is included to request further information with regards to the facilities available for the collection and storage of organic refuse, which the applicant has committed to providing.



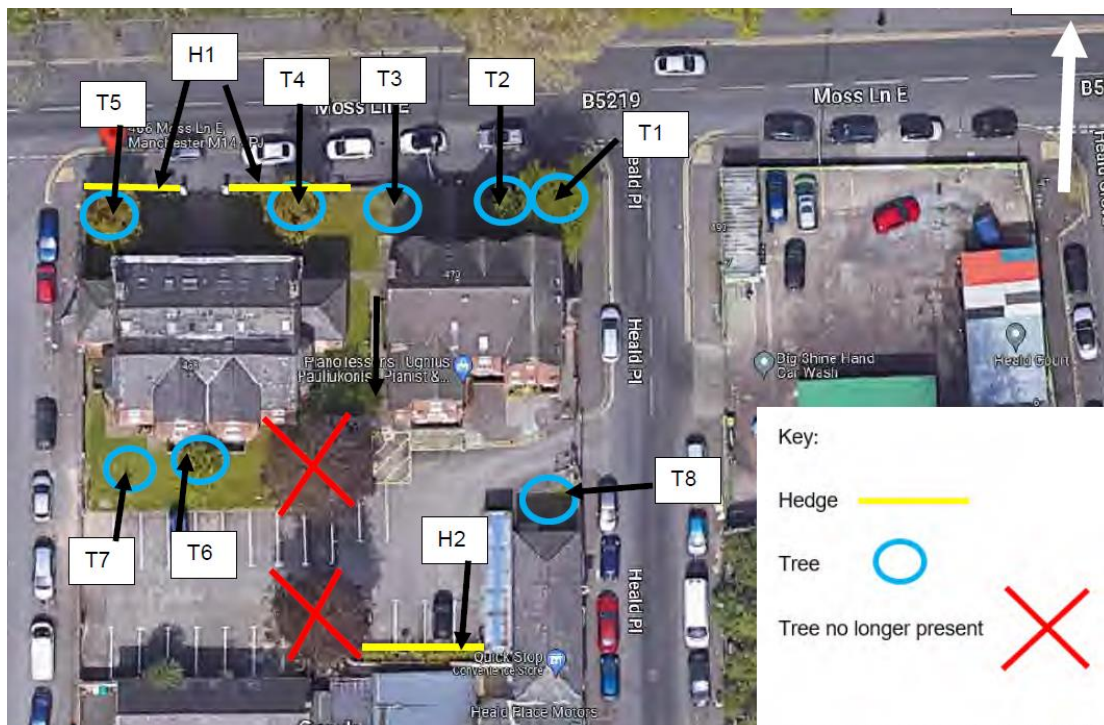
Crime and Security

A Crime Impact Statement has been reviewed by Greater Manchester Policy (Design for Security and provided the physical security measures detailed are implemented, the proposal can be supported from a crime and security perspective. A condition has been recommended.

Landscaping, Trees and Boundary Treatment

An Arboricultural Impact Assessment accompanies the application and confirms that out of the 8 trees on site 5 of these reside on the northern grassed area fronting Moss Lane East, 2 are located to the rear of 466-468 Moss Lane East and 1 is located on the boundary with Heald Place. There are hedges to the north boundary either side of the entrance to 466 – 468 Moss Lane East and one hedge to the southern boundary to the shared boundary with a property facing Heald Place.

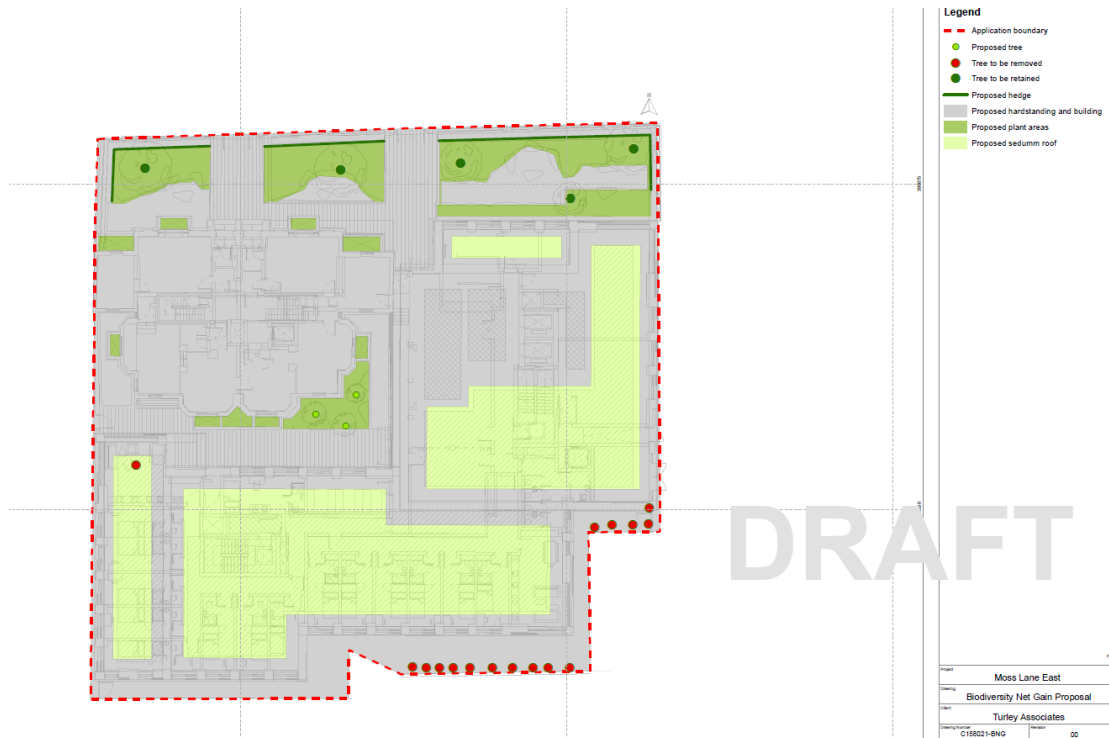
Three trees to the site frontage (T3,4,5) would be retained as part of the development proposed and tree T1 replaced with a higher quality Cherry tree. The trees to be lost are small scale, poor quality trees, being a category C plum, a category U Rowan, a category U Cherry, a category U Field Maple and a young category B Larch. The hedge to the southern boundary would also be removed.



An associated Arboricultural Method Statement advises of appropriate methodology which should be undertaken during the construction phase to avoid any harm to the root protection area of retained trees.

The City Council's Arboriculturalist has undertaken a review of the submitted assessments and concurs that with a good tree mitigation proposal the trees on this site could be replaced with minimal amenity impact to the local area. On this basis, the impact to existing tree provision is considered in acceptable.

In terms of landscaping, the submitted Biodiversity Net Gain Proposal proposes a plan that would respond positively to the setting of existing and proposed buildings and street-scene.



The plan shows large areas of sedum roofs, plant areas, a long hedge along the northern boundary of the site and planting of three trees in containers within the internal courtyard. There would be seating areas to the site frontage and within the internal courtyard area. There would be 8 trees, as there are now (3 of those being retained trees) as indicated in the plan above.

Hard landscaping would be in the form of permeable paving and gravel.

The two listed lamp posts placed in front of the listed building are proposed to be retained.

The existing boundaries are a mix of low walls with railings and brick walls. The northern boundary along Moss Lane East is comprised of low walls with railings. There are two gates along this boundary. These railings and gates would be retained as part of the proposals. The existing boundary along Parkfield Street is comprised of a brick wall of which approximately half would be retained. The existing boundary along Heald Place is a continuation of the railing that runs along Moss Lane East. This will be retained.

The overall approach would provide a cohesive, attractive and improved environment, appropriate for the type and location of the proposed development.

Ecology and Bio-diversity

The site largely comprises buildings and hardstanding. The site does not have any nature conservation designations, nor are the proposals likely to impact upon any such site. No invasive plant species have been identified.

A preliminary ecological and bat roost appraisal forms part of the application. One of the buildings on site is being used as a Common Pipistrelle day roost by small numbers of this species. Details of the mitigation measures that are required have been provided and these are acceptable.

Greater Manchester Ecological Unit are satisfied subject to the imposition of conditions relating to bats and nesting birds.

The development proposal also provides the opportunity to enhance bio-diversity, the applicant submitted reports to state that a biodiversity net gain of 12% could be achieved largely through sedum roofs and tree planting of ornamental trees.

Greater Manchester Ecological Unit require the imposition of a condition to secure the Biodiversity Net Gains requiring the submission of a landscape and ecological management plan to secure these improvements.

Flood Risk/Surface Water Drainage

The site is located within Flood Zone 1, where a low risk of flooding is anticipated (indicating an annual probability of fluvial flooding of less than 1 in 1,000). A Drainage Strategy Report has been considered by the City Council's Flood Risk Management Team who advise that further information is required. A condition is included which requires the submission of further design details and details of a clear adoption policy to ensure effective management and maintenance of the scheme thereafter. If these measures are successfully implemented, the strategy is in principle considered acceptable.

Ground Conditions

The application includes the submission of a Phase I Desktop study. This is accepted by Environmental Health. A Phase II study would be required before the commencement of development, excluding demolition to identify any necessary remediation. Appropriate conditions have been included which require the submission and approval of a site investigation report and any subsequent remediation strategy prior to the commencement of development. A further condition requiring a verification report to demonstrate the work is completed in accordance with agreed methodology is also included.

Environmental Standards

City Council policy requires that developers focus on achieving low carbon and energy efficient developments and therefore development should be expected to demonstrate its contribution to these objectives.

The application has been accompanied by an Environmental Standards Statement which sets out how proposed scheme complies with the energy efficiency requirements and carbon dioxide emission reduction targets prescribed within the Development Plan.

A commitment is made to achieving at least 4% building emission rate reduction

relative to the current Part L (2021) compliance standard within Building Regulations. This equates to a 30-50 improvement over Part L 2013. The report also sets out that the proposal's enhanced 'fabric led' material specification, alongside its high - quality design and construction standards are to improve the energy efficiency of the building. In addition, pursuant to Policy EN4 of the Core Strategy, the proposed development will be designed and specified in accordance with the principles of the energy hierarchy in line with policy.

The proposed development would also target BREEAM 'Excellent' with a focus on reducing carbon emissions. A suitable condition is included to ensure compliance with this rating.

The retention of the listed building would save embodied carbon and reduce the scheme as a whole would reduce carbon emissions during the operational lifetime of the development given improved energy efficiency.

Climate Change and Air Quality

The site is situated within a highly sustainable location with excellent access to a range of amenities and public transport and within walking distance of the nearby universities which the proposal is intended to serve.

As the proposal is car free and would generate only a small amount of additional traffic on the local road network and would also result in the removal of Car parking associated with the existing residential units, the impact upon climate change is considered negligible.

The site is located close to a busy, main arterial route and transport corridor, and is therefore unlikely that there would be any significant or harmful contribution to air quality or climate change as a consequence of vehicular movements or ongoing activity.

The proposal includes cycle storage and would require a detailed Travel Plan. Such measures will serve to limit the impact the upon climate change. The development will also incorporate energy efficient measures as part of the development, as outlined in the section relating to environmental standards.

Any impact during construction would be limited and temporary in duration. Mitigation measures would be implemented to reduce dust emissions and through the use of good practice control measures via the implementation of a Demolition/Construction Management Plan. Air quality factors are not considered a constraint to development of the site. A condition has been included to ensure the mitigation measures contained with a Demolition/Construction Management Plan are complied with.

Construction/Demolition Management

To ensure construction and demolition is effectively controlled and to prevent any disruption to existing occupiers in the area, or along key routes throughout this part the city, a condition is included which requires the submission and approval of a detailed construction management/demolition plan which includes amongst other

matters; working practices, working hours, dust suppression, the parking of construction vehicles and the removal of waste.

Affordability

The Executive reports recognise affordability is a key matter that will need to be addressed when the local plan is refreshed. In advance of this applicants are being requested to consider how affordability can be delivered. The applicant proposes to include an element of affordable provision and has agreed to enter into a legal agreement to this effect requiring affordable rent obligations with up to 10% of all bed spaces being advertised as being below market rent level in each academic year and is considered to represent a significant regenerative benefit.

Fire Safety

It is a mandatory planning requirement to consider fire safety for high rise buildings in relation to land use planning issues. A fire statement must be provided, and the Health and Safety Executive (HSE) consulted. Government advice is very clear that the review of fire safety at Gateway One through the planning process should not duplicate matters that should be considered through Building Control. A number of queries raised by the HSE have been addressed. There are outstanding matters but these are issues that should be addressed through Building Control and are not land use planning issues that can be dealt with through the planning process. On this basis it is considered that there are no outstanding concerns which relate to the remit of planning as set out in the Fire safety and high-rise residential buildings guidance August 2021. It is recommended that an informative of the planning approval highlights the need for further dialogue with relevant experts as part of the approval of Building Regulations in order to ensure that all matters relating to fire safety meet the relevant Regulations.

Public benefits

For proposals that are likely to cause less than substantial harm to the significance of a designated heritage asset and the loss of a non-designated heritage asset, the NPPF states that the harm should be weighed against the public benefits. It is acknowledged that there are public benefits to be derived from the provision of purpose built student accommodation in close proximity to the Oxford Road Corridor that would ensure the retention and long term future of the listed building.

Conclusion

It is recognised that the proposal has generated concerns within the local area, although some support has also been received. These concerns have been carefully considered and addressed as they must be. Whilst it is unlikely the concerns will have been allayed, the necessary assessment conclude that the proposal is acceptable.

The proposed development would make effective use of brownfield site and represents an appropriate and satisfactory form of development that would make a meaningful contribution to the supply of student accommodation.

The site is situated within a highly sustainable location close to the main Manchester University campuses and would deliver regenerative benefits involving economic, social and environmental improvements including job creation and spending in the local economy and would providing a catalyst for further regeneration in the area, whilst providing needed purpose-built student accommodation.

It is felt that given the surrounding environmental context, coupled with the design, scale and appearance of the building, the proposed development will enable the development to make the most efficient use of the land and will assimilate effectively into its surroundings.

It is further believed that the proposed building has been designed to high standard and reflects the nature and scale of buildings within this highly urbanised environment responds positively to the character, scale, massing and varying height of adjoining buildings. The proposal would ensure the retention of the listed building on site and has been designed to have regard to the character and designation of that building.

There would be no significant harmful impact upon either residential amenity, the function of the neighbouring community centre or the operation of the highway and is it considered that the proposal would integrate effectively into the host streetscape. As such, the proposed development accords with all relevant local policy and planning guidance.

Other Legislative Requirements

Equality Act 2010

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved polices of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation for application 135544/FO/2022

Minded to Approve subject to a legal agreement containing affordable rent obligations for up to 10% of all bed spaces being advertised as being below market rent level in each academic year.

Recommendation for application 135545/LO/2022

Minded to Approve

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application and listed building application.

Condition(s) to be attached to the decision for application 135544/FO/2022

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) Prior to the commencement of above ground development (excluding demolition), all materials to be used on all external elevations of the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include the submission of samples (including a panel) and specifications of all materials to be used on all external elevations of the development along with jointing and fixing details, vents, details of the drips to be used to prevent staining in, ventilation and a strategy for quality control management. The approved materials shall then be implemented as part of the development.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Manchester Core Strategy.

3) The development hereby approved shall be carried out in accordance with the following drawings and documents received by the City Council as Local Planning Authority on 17 November 2022.

Application Form Turley
 Covering Letter Turley
 Design and Access Statement Carson Sall
 Air Quality Assessment Hydrock
 Archaeological Desk-Based Study Nexus
 BREEAM Pre-Assessment Report Ridge
 Broadband Connectivity Assessment GTech Surveys Limited

Contaminated Land Report ROC Consulting
 Crime Impact Statement Greater Manchester Police
 Daylight & Sunlight Impact on Neighbouring Properties GIA
 Ecology Report Middlemarch
 Energy/ Ventilation Strategy Futureserv
 Environmental Standards Statement Ridge
 Flood Risk Assessment and Drainage Strategy ROC Consulting
 Fire Statement OFR Consultants
 Noise Impact Assessment Report Hann Tucker Associates
 Television and Radio Reception Impact Assessment GTech Surveys Limited
 Transport Statement RSK
 Travel Plan RSK
 Location Plan Existing CS-1094_MLE-001-SI Carson Sall
 Existing Site Plan CS-1094_MLE-003-EX Carson Sall
 Contextual North & South Existing
 GA Elevation
 CS-1094_MLE-006-EL Carson Sall
 Demolition Site Plan CS-1094_MLE-004-EX Carson Sall
 Proposed Site Plan CS-1094_MLE-005 Carson Sall
 GA-Basement Floor Plan CS-1094_MLE-099-GA Carson Sall
 GA-1st Floor Plan CS-1094_MLE-101 Carson Sall
 GA-2nd Floor Plan CS-1094_MLE-102 Carson Sall
 GA-3rd -6th Floor Plan CS-1094_MLE-103 Carson Sall
 GA-7th Floor Plan CS-1094_MLE-107 Carson Sall
 GA-8th Floor Plan CS-1094_MLE-108 Carson Sall
 GA-Roof Plan CS-1094_MLE-109 Carson Sall
 Proposed Section AA CS-1094_MLE-200 Carson Sall
 Proposed Section BB CS-1094_MLE-201 Carson Sall
 Proposed Section CC CS-1094_MLE-202 Carson Sall
 Proposed Partial Section AA CS-1094_MLE-203 Carson Sall
 Proposed North Elevation CS-1094_MLE-300 Carson Sall
 Proposed North Elevation (Courtyard) CS-1094_MLE-301 Carson Sall
 Proposed East Elevation CS-1094_MLE-302 Carson Sall
 Proposed South Elevation CS-1094_MLE-303 Carson Sall
 Proposed West Elevation CS-1094_MLE-304 Carson Sall
 Contextual Elevation East and West CS-1094_MLE-305 Carson Sall
 Contextual Elevation North and South CS-1094_MLE-306 Carson Sall
 Biodiversity Metric Calculator

23 November 2023

Planning Statement Turley

Received 24 November 2023

Heritage Statement Turley

Received 27 March 2023

Daylight and Sunlight Addendum GIA

Redacted Tenancy Schedule

Received 02 May 2023

Technical Note SCP

Received 04 May 2023

GA-Ground Floor Plan CS-1094_MLE-100 Rev 01 Carson Sall

Received 07 June 2023

Reason - To ensure that the development is carried out in accordance with the approved plans, pursuant to policies SP1 and DM1 of the Manchester Core Strategy.

4) a) Prior to the commencement of demolition, a Demolition Management Plan outlining working practices during demolition development shall be submitted to and approved in writing by the City Council as Local Planning Authority.

For the avoidance of doubt, the Demolition Management Plan should include:

- Measures to control noise and vibrations;
- Dust suppression measures;
- Compound locations where relevant;
- Location, removal and recycling of waste;
- Details of an emergency contact telephone number;
- Parking of construction vehicles; and
- Sheeting over of demolition vehicles.

The development shall only be carried out in accordance with the approved Demolition Management Plan.

b) Prior to the commencement of development (excluding demolition), a Construction Management Plan outlining working practices during development shall be submitted to and approved in writing to the City Council as Local Planning Authority.

For the avoidance of doubt, the Construction Management Plan should include:

- Measures to control noise and vibrations;
- Dust suppression measures;
- Compound locations where relevant;
- Location, removal and recycling of waste;
- Detail of an emergency contact telephone number;
- Parking of construction vehicles; and
- Sheeting over of construction vehicles.

The development shall only be carried out in accordance with the approved Construction Management Plan.

Manchester City Council encourages all contractors to be 'considerate contractors' when working in the city by being aware of the needs of neighbours and the environment. Membership of the Considerate Constructors Scheme is highly recommended.

5) The development hereby approved shall be carried out in accordance with details contained with the Environmental Standards Statement produced by Ridge

received by the City Council as Local Planning Authority on 17 November 2022. A post construction review certificate/statement shall be submitted for approval within 3 months post final construction of the development hereby approved.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Manchester Core Strategy and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

6) The development hereby approved shall achieve a post-construction Building Research Establishment Environmental Assessment Method (BREEAM) rating of at least a 'Excellent' rating. Post construction review certificate(s) shall be submitted to and approved in writing by the City Council as Local Planning Authority within 12 months of occupation of the development.

Reason - In order to minimise the environmental impact of the development, pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Manchester Core Strategy, and the principles contained within The Guide to Development in Manchester SPD (2007), and the National Planning Policy Framework.

7) a) Before the development commences a scheme for the storage and disposal of refuse shall be submitted to and approved in writing by the City Council as local planning authority.

New developments shall have refuse storage space for segregated waste collection and recycling. Internal and external storage areas are required.

b) The details of the approved scheme shall be implemented as part of the development and shall remain in situ whilst the use or development is in operation.

Reason - In the interests of residential amenity and public health, pursuant to policies DM1 and SP1 of the Manchester Core Strategy.

8) The development hereby approved shall be implemented in accordance with the submitted Phase I Desk Study (Ref:4453-ROC-ZZ-XX-RP-ES-P1DTS) prepared by ROC Consulting, dated 04 November 2022, received by the City Council as Local Planning Authority on 17 November 2022. The Preliminary Risk Assessment shall conform to City Council's current guidance document (Planning Guidance in Relation to Ground Contamination).

In the event of the Preliminary Risk Assessment identifying risks which in the written opinion of the Local Planning Authority require further investigation, the development (excluding demolition) shall not commence until a scheme for the investigation of the site and the identification of remediation measures (the Site Investigation Proposal) has been submitted to and approved in writing by the City Council as Local Planning Authority.

The measures for investigating the site identified in the Site Investigation Proposal shall be carried out, before the development commences (excluding demolition) and a report prepared outlining what measures, if any, are required to remediate the land (the Site Investigation Report and/or Remediation Strategy) which shall be submitted to and approved in writing by the City Council as local planning authority.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Manchester Core Strategy.

9) When the development commences (excluding demolition), the development shall be carried out in accordance with a previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the City Council as local planning authority. No occupation of the development shall take place until the completion/verification report is submitted to and approved by the City Council as local planning authority.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and SP1 of the Manchester Core Strategy.

10) The development hereby approved shall be implemented in general accordance with the physical security specifications outlined within the submitted Crime Impact Statement (Ref:2012/0607/CIS/01) dated 13 August 2022, received by the City Council as Local Planning Authority on 17 November 2022.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Manchester Core Strategy and to reflect the guidance contained in the National Planning Policy Framework.

11) No development shall take place until surface water drainage works have been implemented in accordance with Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacements national standards and details that have been submitted to and approved in writing by the Local Planning Authority.

In order to avoid/dischage the above drainage condition the following additional information has to be provided:

- o A finalised drainage layout showing all components, outfalls, levels and connectivity;
- o Maximised integration of green SuDS components (utilising infiltration or attenuation) if practicable;
- o Details of surface water attenuation that offers a reduction in surface water runoff

- rate in line with the Manchester Trafford and Salford Strategic Flood Risk Assessment, i.e. at least a 50% reduction in runoff rate compared to the existing rates with the aim of reducing to the Greenfield runoff rates, as the site is located within Conurbation Core Critical Drainage Area;
- o An existing and proposed impermeable areas drawing to accompany all discharge rate calculations.
 - o A blockage risk assessment is undertaken as part of the drainage strategy to support proposed flows less than 5 l/s. If the risk cannot be adequately managed, a higher minimum discharge rate should be considered / agreed with relevant parties.
 - o Runoff volume in the 1 in 100 year, 6 hours rainfall shall be constrained to a value as close as is reasonable practicable to the greenfield runoff volume for the same event, but never to exceed the runoff volume from the development site prior to redevelopment;
 - o Evidence that the drainage system has been designed (unless an area is designated to hold and/or convey water as part of the design) so that flooding does not occur during a 1 in 100 year rainfall event with allowance for 45% climate change;
 - o Assessment of overland flow routes for extreme events. Overland flow routes need to be designed to convey the flood water in a safe manner in the event of a blockage or exceedance of the proposed drainage system capacity including inlet structures. A layout with overland flow routes needs to be presented with appreciation of these overland flow routes with regards to the properties on site and adjacent properties off site.
 - o Progression through the drainage hierarchy shall be evidence based and supported by site investigation. Results of ground investigation carried out under Building Research Establishment Digest 365. Site investigations should be undertaken in locations and at proposed depths of the proposed infiltration devices. Proposal of the attenuation that is achieving half emptying time within 24 hours. If no ground investigations are possible or infiltration is not feasible on site, evidence of alternative surface water disposal routes (as follows) is required.
 - o Where surface water is connected to the public sewer, agreement in principle from United Utilities is required that there is adequate spare capacity in the existing system taking future development requirements into account. An email of acceptance of proposed flows and/or new connection will suffice.
 - o Hydraulic calculation of the proposed drainage system;
 - o Construction details of flow control and SuDS elements.

Reason: To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution. This condition is imposed in light of national policies within the NPPF and NPPG and local policies EN08 and EN14.

12) No development hereby permitted shall be occupied until details of the implementation, maintenance and management of the sustainable drainage scheme have been submitted to and approved by the local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details. Those details shall include:

- Verification report providing photographic evidence of construction as per design drawings;
- As built construction drawings if different from design construction drawings;

- Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

Reason: To manage flooding and pollution and to ensure that a managing body is in place for the sustainable drainage system and there is funding and maintenance mechanism for the lifetime of the development.

13) Notwithstanding details submitted, the development hereby approved shall not be occupied until there has been submitted to and approved in writing by the City Council as Local Planning authority a plan indicating the positions, design, materials and type of any new boundary treatment to be erected. The boundary treatment shall be completed before the development hereby approved is occupied and shall thereafter be retained.

Reason - To ensure that the appearance of the development is acceptable to the City Council as Local Planning Authority in the interests of the visual amenity of the area within which the site is located in order to comply with policies SP1, EN3 and DM1 of the Manchester Core Strategy.

14) a) Before the development commences a scheme for acoustically insulating the proposed residential accommodation against noise from Moss Lane East, Parkfield Street and Heald Place shall be submitted to and approved in writing by the City Council as local planning authority. There may be other actual or potential sources of noise which require consideration on or near the site, including any local commercial/industrial premises such as the nearby car wash.

The potential for overheating shall also be assessed and the noise insulation scheme shall take this into account. The approved noise insulation and ventilation scheme shall be completed before any of the dwelling units are occupied.

Noise survey data shall include measurements taken during a rush-hour period and night time to determine the appropriate sound insulation measures necessary. The following noise criteria shall be required to be achieved:

Bedrooms (night time - 23.00 - 07.00) 30 dB LAeq (individual noise events shall not exceed 45 dB L_{Amax,F} by more than 15 times)

Living Rooms (daytime - 07.00 - 23.00) 35 dB LAeq

Gardens and terraces (daytime) 55 dB LAeq

Reason: To secure a reduction in noise from traffic or other sources in order to protect future residents from noise disturbance and to reduce the potential for overheating.

15) a) Any externally mounted ancillary plant, equipment and servicing to be installed shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve a rating level of 5dB (LAeq) below the typical background (LA90) level at the nearest noise sensitive location. Prior to its installation, the scheme, including any necessary mitigation, shall be submitted to and approved in writing by the City Council as Local Planning Authority in order to secure a reduction in the level of noise emanating from the site.

b) Prior to any externally mounted ancillary plant, equipment and servicing to be

installed becoming operational, an approved verification report shall be submitted to and approved in writing by the City Council as Local Planning authority to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic report. The report shall also undertake post completion testing to confirm that the noise criteria have been met. Any instances of non - conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the agreed noise criteria.

Reason - To minimise the impact of the development and to prevent a general increase in pre-existing background noise levels around the site, pursuant to saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies DM1 and SP1 of the Manchester Core Strategy.

16) a) The premises shall be acoustically insulated and treated to limit the break-in/break-out of noise in accordance with a noise study of the premises and a scheme of acoustic treatment that has been submitted to and approved in writing by the City Council as Local Planning Authority. The scheme shall be implemented in full before the use commences.

b) Prior to occupation of the building hereby approved a verification report will be required to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic consultant's report. The report shall also undertake post completion testing to confirm that acceptable criteria have been met. Any instances of non-conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the agreed noise criteria.

Reason - To safeguard the amenity of nearby residential properties, pursuant to saved policy DC26 of the Unitary Development Plan for the City of Manchester and Policies DM1 and SP1 of the Manchester Core Strategy.

17) The development hereby approved shall only be implemented in accordance with the methodology and mitigation measures detailed within the submitted Arboricultural Impact Assessment and Arboricultural Method Statement produced by LAC, received by the City Council as Local Planning Authority on 17 November 2022.

Reason - In order avoid damage to trees adjacent to the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Manchester Core Strategy.

18) a) Prior to the commencement of the development, details of a Local Benefit Proposal, in order to demonstrate commitment to recruit local labour for the duration of the construction of the development, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved document shall be implemented as part of the construction of the development.

In this condition a Local Benefit Proposal means a document which includes:

- i) the measures proposed to recruit local people including apprenticeships;
- ii) mechanisms for the implementation and delivery of the Local Benefit Proposal;
- iii) measures to monitor and review the effectiveness of the Local Benefit Proposal in achieving the objective of recruiting and supporting local labour objectives.

(b) Within one month prior to construction work being completed, a detailed report which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority.

Reason - The applicant has demonstrated a commitment to recruiting local labour pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy.

19) Prior to the first occupation of the development hereby approved, a 'moving in and out' management strategy shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall then be implemented as part of the development and thereafter retained and maintained.

Reason - In the interest of managing the impact of the development on the car parking area and local highway network during moving in and out periods of activity pursuant to policies T2 and DM1 of the Manchester Core Strategy.

20) Prior to the first operation of the development hereby approved, an external signage strategy for the entire building shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved strategy shall then be implemented and used to inform any future advertisement applications for the building.

Reason - In the interest of visual amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy.

21) The development hereby approved shall include a scheme for the illumination of external areas during the period between dusk and dawn. Full details of such a scheme shall be submitted to and agreed in writing by the City Council as Local Planning Authority. The approved scheme shall be implemented in full prior to the first occupation and shall remain in operation for so long as the development is occupied.

Reason - In the interests of amenity, crime reduction and the personal safety of those using and ensure that lighting is installed which is sensitive to the bat environment the proposed development in order to comply with the requirements of policies SP1 and DM1 of the Manchester Core Strategy.

22) If any lighting at the development hereby approved, when illuminated, causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, within 14 days of a written request, a scheme for the elimination of such glare or light spillage shall be submitted to the Council as local planning authority and once approved shall thereafter be retained in accordance with details which have received prior written approval of the City Council as Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Manchester Core Strategy.

23) No part of the development hereby approved shall be occupied until the space and facilities for bicycle parking have been provided in accordance with the details shown on drawings Proposed Basement Plan (099), received by the City Council as Local Planning Authority on 17 November 2022.

The approved space and facilities shall then be retained and permanently reserved for bicycle parking.

In the event that Travel Plan monitoring identifies the need for increased cycle storage provision, the area identified on the approved drawing for a future expansion zone shall be implemented.

Reason - To ensure that adequate provision is made for bicycle parking so that persons occupying or visiting the development have a range of options in relation to mode of transport in order to comply with policies SP1, T1 and DM1 of the Manchester Core Strategy.

24) Notwithstanding the details contained within the Framework Travel Plan submitted, a detailed Travel Plan shall be submitted to and agreed in writing by the City Council as Local Planning Authority prior to first occupancy of the units hereby approved. In this condition a Travel Plan means a document which includes:

(1) The measures proposed to be taken to reduce dependency on the private car by those attending or employed in the development;

(2) A commitment to surveying the travel patterns of staff and customers during the first three months of the development and thereafter from time to time;

(3) Mechanisms for the implementation of the measures to reduce dependency on the private car; and

(4) Measures for the delivery of specified travel plan services; and measures to monitor and review the effectiveness of the Travel Plan in achieving the objective of reducing dependency on the private car.

Within six months of the first use of the development, a revised Travel Plan which takes into account the information about travel patterns gathered pursuant to item (2) above shall be submitted to the City Council as local planning authority for approval. Any Travel Plan which has been approved by the City Council as local planning authority shall be implemented in full at all times when the development hereby approved is in use.

Reason - To reduce dependency on the car travel and to promote alternative means of transport, pursuant to policies DM1 and SP1 of the Manchester Core Strategy.

25) Within one month of the practical completion of the development hereby approved and at any other time during the construction of the development, if requested in writing by the City Council as Local Planning Authority, in response to identified television signal reception problems within the potential impact area, a study to identify such measures necessary to maintain at least the pre-existing level and quality of signal reception identified in the survey carried out above for that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority. The measures identified must be carried out within one month of the study being approved in writing to the City Council as Local Planning Authority.

Reason - To provide an indication of the area of television signal reception likely to be affected by the development to provide a basis on which to assess the extent to

which the development during construction and once built, will affect television reception and to ensure that the development at least maintains the existing level and quality of television signal reception - In the interest of residential amenity, pursuant to policy DM1 of Manchester Core Strategy.

26) The development hereby approved shall be used as purpose-built student accommodation only (*sui generis*) and for no other purpose (including serviced apartments/apart hotels or similar uses where sleeping accommodation (with or without other services) is provided by way of trade for money or money's worth and occupied by the same person for less than ninety consecutive nights)

Reason - To ensure that the accommodation is used solely for the intended purpose - student accommodation and to safeguard the amenities of the neighbourhood by ensuring that other uses which could cause a loss of amenity such as serviced apartments/apart hotels do not commence without prior approval; to safeguard the character of the area, and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

27) Prior to occupation of the development hereby approved, full details of all necessary off-site highway works to be implemented, shall be submitted to and approved in writing by the City Council as Local Planning Authority and be implemented in accordance with a timescale to be agreed by the City Council as Local Planning Authority. Such works should include:

Footway reinstatement;

Extension of double yellow line restrictions.

Reason - In the interests of highway safety, pursuant to Policies DM1 and SP1 of the Manchester Core Strategy.

28) A landscape and ecological management plan (LEMP) shall be submitted to, and be approved in writing by, the local planning authority prior to the commencement of the development. The content of the LEMP shall include the following.

- a) Description and evaluation of features to be managed.
- b) Ecological trends and constraints on site that might influence management.
- c) Aims and objectives of management.
- d) Appropriate management options for achieving aims and objectives.
- e) Prescriptions for management actions.
- f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period).
- g) Details of the body or organization responsible for implementation of the plan.
- h) Ongoing monitoring and remedial measures.

The LEMP shall also include details of the legal and funding mechanism{s} by which the long-term implementation of the plan will be secured by the developer with the management body(ies) responsible for its delivery. The plan shall also set out (where the results from monitoring show that conservation aims and objectives of the LEMP are not being met) how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme. The approved plan will be

implemented in accordance with the approved details.

Reason - Pursuant to biodiversity enhancement of the site, in accordance with policy EN15 of the Core Strategy and the National Planning Policy Framework.

29) The following works demolition of 470, Moss Lane East as identified in Dusk Emergence And Dawn Re-Entry Bat Surveys report by Middlemarch dated September 2022 (ref RT-MME-158448), shall not in any circumstances commence unless the local planning authority has been provided with either:

- a) a licence issued by Natural England pursuant to Regulation 55 of The Conservation of Habitats and Species (Amendment) (EU Exit) Regulations 2019 authorizing the specified activity/development to go ahead; or
- b) a statement in writing from the relevant licensing body to the effect that it does not consider that the specified activity/development will require a licence

Reason: In order to prevent any habitat disturbance to bats in accordance with policy EN15 of the Core Strategy and the National Planning Policy Framework.

30) No removal of or works to any hedgerows, trees or shrubs or works to or demolition of buildings or structures that may be used by breeding birds shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation and structures for active birds' nests immediately before the vegetation is cleared or works commenced and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority.

Reason – To ensure the protection of habitat of species that are protected under the Wildlife and Countryside Act 1981 or as subsequently amended and to comply with policy EN15 of the Core Strategy.

31. The flat roofs of the development hereby approved shall not be used for any other purpose other than as a means of escape in emergency or for maintenance of the building.

Reason - To safeguard the amenities of the adjoining premises for overlooking or perceived overlooking and in accordance with policies SP1 and DM1 of the Core Strategy.

32. Prior to the commencement of the development, a strategy for the provision of up to three accessible parking spaces on Moss Lane shall be submitted to and approved in writing by the City Council. The strategy shall set out a timescale for implementation of accessible spaces and a review mechanism for demand.

The development shall be built out in accordance with the agreed details.

Reason -To ensure safe access to the development site in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012)

Informative:

Cadent Gas Ltd own and operate the gas infrastructure within the area of your development. There may be a legal interest (easements and other rights) in the land that restrict activity in proximity to Cadent assets in private land. The applicant must ensure that the proposed works do not infringe on legal rights of access and or restrictive covenants that exist.

If buildings or structures are proposed directly above the apparatus the development may only take place following diversion of the apparatus. The applicant should apply online to have apparatus diverted in advance of any works, by visiting cadentgas.com/diversions.

Prior to carrying out works, including the construction of access points, please register on www.linesearchbeforeudig.co.uk to submit details of the planned works for review, ensuring requirements are adhered to.

Any materials approved for planning purposes should be discussed in full with Building Control. This is to ensure they meet the guidance contained in the Building Regulations for fire safety. Should it be necessary to change the external facade treatment due to conflicts with the Building Regulations you should discuss these with the Planning Service as soon as possible as this could materially effect your permission.

Condition(s) to be attached to the decision for application 135545/LO/2022

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2. The development hereby approved shall be carried out in accordance with the following drawings and documents:

Application Form Turley
 Covering Letter Turley
 Design and Access Statement Carson Sall
 Heritage Statement Turley
 Archaeological Desk-Based Study Nexus
 Location Plan Existing CS-1094_MLE-001-SI Carson Sall
 Existing Site Plan CS-1094_MLE-003-EX Carson Sall
 Basement Floor Plan Existing CS-1094_MLE-EX-150 Carson Sall
 Basement Floor Plan Proposed CS-1094_MLE-EX-099 Carson Sall
 Basement Floor Plan Demolition CS-1094_MLE-EX-160 Carson Sall
 Ground Floor Plan Existing CS-1094_MLE-EX-151 Carson Sall
 Ground Floor Plan Proposed CS-1094_MLE-EX-100 Carson Sall
 Ground Floor Plan Existing CS-1094_MLE-EX-151 Carson Sall
 First Floor Plan Existing CS-1094_MLE-EX-152 Carson Sall
 First Floor Plan Proposed CS-1094_MLE-EX-152 Carson Sall
 First Floor Plan Demolition CS-1094_MLE-EX-162 Carson Sall
 Second Floor Plan Existing CS-1094_MLE-EX-153 Carson Sall

Second Floor Plan Proposed CS-1094_MLE-EX-102 Carson Sall
 Second Floor Plan Demolition CS-1094_MLE-EX-163 Carson Sall
 Roof Plan Existing CS-1094_MLE-EX-154 Carson Sall
 Roof Plan Proposed CS-1094_MLE-EX-154 Carson Sall
 Section A-A Existing CS-1094_MLE-EX-250 Carson Sall
 Section A-A Proposed CS-1094_MLE-EX-200 Carson Sall
 Section A-A Demolition CS-1094_MLE-EX-260 Carson Sall
 Section B-B Existing CS-1094_MLE-EX-251 Carson Sall
 Section B-B Proposed CS-1094_MLE-EX-201 Carson Sall
 Section B-B Demolition CS-1094_MLE-EX-261 Carson Sall
 Section C-C Existing CS-1094_MLE-EX-252 Carson Sall
 Section C-C Proposed CS-1094_MLE-EX-202 Carson Sall
 Section C-C Demolition CS-1094_MLE-EX-262 Carson Sall
 Front Elevation Existing CS-1094_MLE-EX-350 Carson Sall
 Front Elevation Proposed CS-1094_MLE-EX-300 Carson Sall
 Rear Elevation Existing CS-1094_MLE-EX-352 Carson Sall
 Rear Elevation Proposed CS-1094_MLE-EX-302 Carson Sall
 Side Elevation A Existing CS-1094_MLE-EX-353 Carson Sall
 Side Elevation A Proposed CS-1094_MLE-EX-303 Carson Sall
 Side Elevation B Existing CS-1094_MLE-EX-351 Carson Sall
 Side Elevation B Proposed CS-1094_MLE-EX-301 Carson Sall
 Basement Reflective Ceiling Plan Existing CS-1094_MLE-EX-550 Carson Sall
 Basement Reflective Ceiling Plan Proposed CS-1094_MLE-EX-500 Carson Sall
 Ground Floor Level Reflective Ceiling Plan Existing CS-1094_MLE-EX-551 Carson Sall
 Ground Floor Level Reflective Ceiling Plan Proposed CS-1094_MLE-EX-501 Carson Sall
 First Floor Level Reflective Ceiling Plan Existing CS-1094_MLE-EX-552 Carson Sall
 First Floor Level Reflective Ceiling Plan Proposed CS-1094_MLE-EX-502 Carson Sall
 Second Floor Level Reflective Ceiling Plan Existing CS-1094_MLE-EX-553 Carson Sall
 Second Floor Level Reflective Ceiling Plan Proposed CS-1094_MLE-EX-503 Carson Sall
 Reinstated Windows CS-1094_MLE-EX-600 Carson Sall
 Proposed Reinstated Windows CS-1094_MLE-EX-601 Carson Sall
 Proposed Typical Wall Finishes CS-1094_MLE-EX-602 Carson Sall
 Proposed Smoke Riser Details CS-1094_MLE-EX-603 Carson Sall
 Proposed Lift Riser Details CS-1094_MLE-EX-604 Carson Sall

Biodiversity Metric Calculator

Received 23 November 2023

Planning Statement Turley

Received 24 November 2023

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

3. (a) Notwithstanding the details submitted with the application, prior to the commencement of development the following shall be submitted for approval in writing by the City Council, as Local Planning Authority:

A programme for the issue of samples and specifications of all materials to be used on all external elevations of the development. The programme shall include timings for the submission of samples and specifications of all materials to be used on all external elevations of the development to include jointing and fixing details, details of the drips to be used to prevent staining, details of the glazing and a strategy for quality control management.

(b) All samples and specifications shall then be submitted to and approved in writing by the City Council as local planning authority in accordance with the programme as agreed for part a) of this condition.

The development shall be carried out in accordance with the approved materials.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

4. The alterations to 44 – 468 Moss Lane East shall not be undertaken before a contract for the carrying out of the building works have been made and evidence of that contract has been supplied to the City Council as Local Planning Authority.

Reason - In the interests of visual amenity and for the avoidance of doubt, and to ensure that redevelopment of the site takes place following external alterations to the existing building pursuant to policy EN3 of the Manchester Core Strategy and saved DC19 of the Unitary Development Plan for the City of Manchester, and the National Planning Policy Framework.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 135544/FO/2022 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

Highway Services
 Environmental Health
 Neighbourhood Team Leader (Arboriculture)
 MCC Flood Risk Management
 Parks & Events
 Greater Manchester Police
 Greater Manchester Archaeological Advisory Service
 Greater Manchester Ecology Unit
 National Amenity Societies

Health & Safety Executive (Land Use)

A map showing the neighbours notified of the application is attached at the end of the report.

Relevant Contact Officer : Jennifer Connor
Telephone number : 0161 234 4545
Email : jennifer.connor@manchester.gov.uk



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Application Number	Date of Appln	Committee Date	Ward
135848/FO/2022	23 Dec 2022	6 July 2023	Rusholme Ward

Proposal Erection of a residential development (Use Class C3), comprising 72 units, along with associated hard and soft infrastructure, access and associated works following demolition of existing buildings

Location Site Of Former Victoria Park Probation Centre, Laidon Road, Manchester

Applicant Britannia Victoria Limited

Agent Lauren Neary, Avison Young

EXECUTIVE SUMMARY

This application relates to a site of a former day nurse and probation offices and community centre that have been vacant for several years. The application now under consideration is for development of 72 homes (41 apartments and 31 houses).

16 objections have been received.

Key Issues

The principle of the proposal and the scheme's contribution to regeneration, as set out in the report, is considered to accord not only with national and local planning policies, but would also deliver key outcomes for the city.

The proposed development would deliver a substantial number of new homes, with a mix of market housing, an offer of 6 x 1 bed apartments within the scheme as discounted market sale units, for disposal on a discounted market sale basis at 80% of market value, and affordable rent dwellings (through Registered Provider grant funding).

The developer is working with Arcon who manage the residential development to the south of the site, with a view to the registered provider taking 12 residential units on a Homes England grant funded basis to deliver additional affordable housing at the site.

The existing vacant buildings on the site have been vandalised, and in the case of one – fire damaged. The proposed development would secure a future for the site leading to the removal of the existing deteriorating buildings on the fringe of Victoria Park Conservation Area.

The potential impacts on local residents are fully set out in the report. These include wider impacts from remediation to construction, to operational impacts such as traffic and visually from the development itself.

A full report is attached below for Members' consideration.

Description

The site is located in Rusholme Ward, is broadly rectangular in shape, and covers 0.65 ha. The site currently comprises a vacant Probation office and community centre (last occupied in May 2021), and a single storey flat roofed vacant vandalised day nursery building, both accessed from Laindon Road. The area between the two buildings is hard surfaced and was last used for car parking. The site is currently bounded by a mix of boundary treatments, including stone walling with copings, brick walls, and high galvanised palisade fencing.



The site is bounded by a mix of two and three storey residential properties. To the west of the site is a three storey residential dwelling currently under construction.



Consent is sought for a residential development (Use Class C3), comprising 72 units, with associated hard and soft landscaping, access and associated works following demolition of existing buildings.

The proposal includes 72 residential units of accommodation, consisting of a mix of 41 apartments and 31 houses. The block of apartments would have 37 units, with 4 additional apartments, in the form of two apartments in the unit at each end of the terrace of dwellinghouses facing Laindon Road within the south of the site.



The mix of accommodation would be in the form of 25, 1 bed and 16 two bedroom apartments, with house types comprising, 15 three bedroom, 15 four bedroom dwellinghouses, and 1 six bed townhouse, therefore providing a range of 1,2,3,4, and 6 bedroom accommodation.

The proposed residential development would comprise a mix of market housing, an offer of 6 x 1 bed apartments within the scheme as discounted market sale units with disposal on a discounted market sale basis at 80% of market value, and affordable rent dwellings (through Registered Provider grant funding).

The proposed buildings ranging from two and a half storeys up to four storeys in height. The scheme predominantly includes the provision of two and a half storey houses across the site, with the proposed apartment building being four storeys in height and located to the north of the application site.

The visuals shown below relate to the external design of the proposed apartment block and one of the house types.

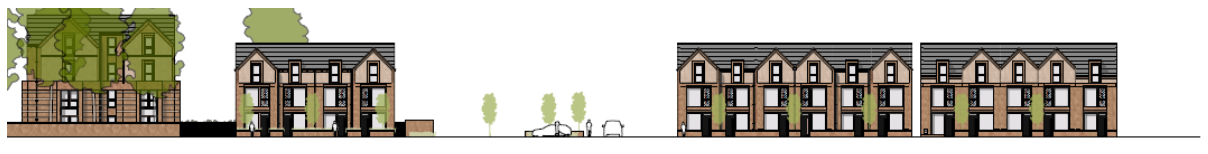


Houses- front and rear elevation



Apartment Block – front elevation

The layout plan for the site includes two vehicular access points from Laindon Road. One would serve the apartments and provide access to parking spaces to the rear of one of the terraces of houses fronting Laindon Road. The other further south on Laindon Road would serve the remainder of the proposed development.



Laindon Road Street view



Daisy Bank Road Street View

There are currently 30 parking spaces associated with the former uses on this site. The proposed development would lead to increased off street parking provision across the overall site, to enable 100% parking provision to each of the dwelling houses – 32 spaces in total, and 18 spaces to serve the proposed apartments (including 2 for disabled users).

Amenity space is being provided for dwelling houses through small front gardens and private rear garden areas, and with areas of shared amenity space within the curtilage of the proposed apartment block.

Planning History

There are no recent planning applications relating to the site. However, there has been an application on the site directly south of the application site for a change of use from supported living accommodation into residential accommodation for the over 55s (Class C3), including alterations to elevations, demolition of garage / carport, creation of new parking area and associated external works alterations (ref: 134076/FO/2022). This application was approved on 8th August 2022.

Publicity

The proposal, by virtue of the number of residential units, the proposed development has been classified as a large scale major development. As such, the proposal has been advertised in the local press (Manchester Evening News), on 24th January 2023 and site notices were displayed at locations around the application site on 1st

February 2023. In addition, notification letters have been sent to local residents and businesses (total of 308) in the local area on 16th January 2023.

Consultations & Notification Responses

Local resident and local businesses - 1 email of support from a resident wishing to support the removal of any trees near to their garden, due to the pollen and sap deposits from the tree onto their washing.

16 emails of objection (4 from 2no. addresses (8in total) and 2 from 1 address) raising the following concerns.

-The local area has become overpopulated that the infrastructure is struggling with the capacity of the number of people here. Local schools are struggling as new parents are having put their child's name down for nurseries and school before they are even born. The waiting times to GP and local hospitals have increased massively too.

-The local area no longer has the community feel it used to have and no longer feels like a neighbourhood.

-The crime rate has increased due the number of new people here and there are so many new residences that they now feel unsafe walking to the local shop. There are concerns that the proposed development would have a negative impact to the crime rates, and it is suggested that Kedleston Avenue be covered by CCTV cameras to improve the personal safety of existing residents.

-Concerns have been raised in relation to the type of residents who would live in the proposed development.

-There are existing problems with levels of traffic in the area and the proposed development would exacerbate this and lead to congestion.

-The junction of this road at Daisy Bank Road is already extremely busy and dangerous to cross.

-The site should be used as a park.

- Concern is expressed that mature trees would be removed from the site, and there would be impacts to wildlife.

- Daisy Bank Road is a part of the Victoria Park Conservation Area, and the environmental impact of the development has not been taken into consideration. The high-density scheme leaving no room for green space in the new community.

- Loss of privacy.

- Vehicular access to the development is opposite Kedleston Avenue. Children play on Kedleston Avenue and the increase in traffic would pose a risk to the children. Residents on Kedleston Avenue would prefer the access to be sited on Laindon Road between Kedleston and Curzon Avenues to deter vehicles from using Kedleston Avenue, rather than Laindon Road when access and egressing the site. Another resident seeks that this access be moved to between Kedleston Avenue and Argle Avenue.

-Concerns that the tall buildings would lead to loss of privacy in their garden area, and possibly block sunlight to the front and back of their home.

- The building would not be in keeping with rest of the local residential properties in this area.

- Too many housing units proposed for the site.

- Concerns about carbon emissions and environmental factors.
- Concerns about increases in noise levels, which will be continued for a while for such a huge demolition and planned structure.
- The scheme must have parking for all residents ,
- The size, massing and scale of the apartment block which faces on to Daisy Bank Road, is inappropriate, for this setting and out of context for the road. It creates a significant privacy issue, and needs to shrink in height, and to be moved further back from the road to reduce this impact.

- Problems of developers flouting or ignoring the planning process all together in Victoria Park. A very noisy Adult Residential Care facility has opened after a redevelopment of the two houses on Daisy Bank Road without planning permission.

- 3no dwellings on Daisy Bank Road in proximity to the site are supported living settings, for disabled Manchester residents. It has been advised that their learning disabilities, including Autism, create additional requirement for their housing settings. The behaviours they exhibit are based around heightened sensitivities. In this home a number of the residents have behaviours, which with the new apartments, would then create privacy issues for this cohort, as the design significantly overlooks their bedrooms. Privacy glass would need to be installed to their service users bedroom windows, which would be a substantial cost in order to protect them from the noise.

Individuals with a learning disability and autism are sensitive to noise. Concerns are expressed regarding construction hours of working, and include a request for a restriction to 10am to 3pm on Mondays to Fridays to reduce the impact to the residents. They are extremely concerned around the construction and the length of time this will take.

A request is also made for the use of conditions for the build to include for adaptation of the disabled persons home, to add sound proofing to their walls, windows, to mitigate the impact of this scheme, and to further remove the use of heavy plant to reduce noise pollution during the build.

Highways Services- Has advised that the TRICS methodology and trip rates are acceptable to Highways, and it is therefore accepted that the adjacent highway network can facilitate such additional demands with no highway safety or capacity concerns. Based on the highways' accident data within the area around the site , they have no concerns about highway safety issues within the vicinity of the site.

A Stage 1 Road Safety Audit (RSA) has been provided which considers the replacement of an existing vehicle access with two new access points as well as associated pedestrian and cycle access to the development site. The RSA has identified a significant level of vehicles currently parking on the footway, directly affecting the proposed access location and which would impact on driver visibility of oncoming vehicles on the main carriageway and pedestrians. The RSA recommends that double yellow line parking restrictions are installed at the access to protect the junction visibility. This is welcomed and acceptable to Highways.

It is understood that the site is to remain unadopted. If adoption is required, the highway design will need to be carried out under a S38 Agreement (Highways Act

1980) to ensure that all elements of new highway infrastructure are constructed to acceptable and adoptable standards.

As stated above two new access points will be installed off Laindon Road and the redundant dropped kerbs reinstated. Should the planning application be approved, the applicant will be required to enter into a Section 278/184 agreements for the alterations to the existing highways, reinstatement of the redundant dropped kerb/s and for construction of the proposed vehicular access and dropped kerbs on Laindon Road.

It is recommended that an off site highway works condition be attached to any approval.

It is expected that all modifications /improvements to the public highway are achieved with a maximum carbon footprint of 40%. Materials used during this process must also be a minimum of 40% recycled and fully recyclable.

The scheme has been designed to enable a 7.7m fire tender to be able to manoeuvre around the site.

The level of proposed parking provision and Electric Vehicle Charging points is acceptable to Highways.

Cycle parking spaces will be provided in a secure cycle store at a 1:1 ratio for the Apartments, with the proposed houses having storage for 2 cycles, which is acceptable to Highways.

Advice is provided in relation to boundary treatment and intervisibility between pedestrians and vehicles, and that all gated accesses should be inward opening.

An Interim Travel Plan (ITP) has been produced, which provides measures, management, aims and objectives, targets, action plan and marketing strategy. The development, submission, implementation and monitoring of a full Travel Plan should be included in the conditions of any planning consent.

A Construction Methodology and Management Strategy has been provided. This document details the construction proposals in respect of the proposed development. It is also requested the applicant provides a dilapidation survey as part of the Construction Management Plan document, and that a condition to require the submission of a Construction management Plan before commencement of development is attached to any approval .

Furter comments received 12.06.2023 –

Comments have been received in relation to the fact that Highways will not adopt the internal roads if below 5.5m in width or 2m wide footways and therefore the internal roads and footways, would remain private.

Environmental Health - recommend any approval has conditions attached relating to contaminated land, acoustic insulation, external plant , refuse storage , air quality , electric charging points , external lighting and a Construction Management Plan .

In regard to ground conditions , they advise they have examined the submitted Phase 1 and Phase 2, Geo-environmental investigation, risk assessment and remediation strategy , and additional information is required, which ,is the reasoning for the proposed contaminated land condition.

In regard to the submitted Noise Impact Assessment they accept the report regarding glazing requirements, however they require further assessment regarding overheating for windows that cannot be opened, with a document relating ventilation and overheating being needed. This can be dealt with through an appropriate condition.

In relation to the submitted Waste Management Strategy they have confirmed this is acceptable. They have also confirmed that the submitted Air Quality report is acceptable, but more information is needed regarding electric vehicle charging points.

Furthermore, the submitted Lighting Impact Assessment is acceptable , but a verification report will be needed.

In addition they advise that in regard to the submitted Construction Management Plan (CMP) that hours of working on Saturdays needs to be revised as work on Saturdays cannot commence before 8.30am, but the rest of the CMP report is acceptable.

Design for Security - have advised they support the application, but recommend a condition is attached to any approval to require the development where feasible, be designed and constructed in accordance with the recommendations in the Crime Impact Statement.

Greater Manchester Ecology Unit - The submitted Ecological assessment found the site to have some, but limited ecological value largely associated with the trees and scrub on the site. The main issue therefore would be associated with nesting birds and the clearance of tree and scrub during the bird breeding season. They therefore recommend a condition be attached to any permission, to ensure that no removal of or works to any hedgerows, trees or shrubs take place during the main bird breeding season

Furthermore, they advise that the biodiversity enhancement measures recommended by the ecology assessment have been incorporated into the Landscape Plan, to ensure that these measures are installed they recommend this aspect of the scheme is conditioned.

Flood Risk Management – Advise that there are new updates for peak rainfall climate change allowances in Flood risk assessments.

They therefore recommend that a condition is attached to any approval to require the submission of a surface water drainage system , together with a further condition relating to the submission of details of the implementation, maintenance, and management of the sustainable drainage scheme.

United Utilities - Have requested the submission of a detailed drainage plan, and if one is not provided prior to determination that a condition is attached to any approval to require the submission of a sustainable surface water drainage scheme and a foul water drainage scheme, including an investigation of the hierarchy of drainage options ;a restricted rate of discharge of surface water ;levels of the proposed drainage systems including proposed ground and finished floor levels in AOD; mitigation measures to manage the risk of sewer surcharge where applicable; and foul and surface water being drained on separate systems. A further condition relating to the submission of a sustainable drainage management and maintenance plan is also recommended.

Detailed advice regarding wastewater assets ,United Utilities property ,assets and infrastructure , water and waste water services has been provided and forwarded to the planning agent .

Greater Manchester Archaeological Advisory Service – recommend a condition is attached to require a programme of archaeological works to be implemented in accordance with a Written Scheme of Investigation (WSI) and this shall cover the following:

1. A phased programme and methodology of investigation and recording to include:
 - archaeological evaluation trenching;
 - pending the results of the above, a targeted open-area excavation.
2. A programme for post-investigation assessment to include:
 - production of a final report on the results of the investigations and their significance.
3. Deposition of the final report with the Greater Manchester Historic Environment Record.
4. Dissemination of the results of the archaeological investigations commensurate with their significance.
5. Provision for archive deposition of the report and records of the site investigation.
6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Arboricultural officers – have raised concerns over the impact on the retained trees on this site due to the inevitable damage to the rooting areas and the constraints of the building lines and landscape requirements.

They advise that careful monitoring and site supervision by the appointed arboricultural consultant will be required to ensure that the tree protection fencing is installed correctly and is regularly checked, and suggest this should be included as a condition if planning is permitted for this site.

Legislative and Policy Context

The National Planning Policy Framework

National Planning Policy Framework (NPPF) - This Framework came into effect on 27th March 2012 and was amended and updated in February 2019 and subsequently in July 2021.

It represents key up to date national policy and is an important material consideration in determining the current application. A number of key aspects of the NPPF that impact on the considerations that need to be given to the current application are identified below.

The document states that the 'purpose of the planning system is to contribute to the achievement of sustainable development. The document clarifies that the 'objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs' (paragraph 7).

The purpose of the planning system is to achieve sustainable development. Paragraph 8 of the NPPF states that to achieve this, there are three overarching objectives, which need to be pursued mutually. Firstly, the economic role should contribute to sustainable development by building a strong, responsive and competitive economy and ensuring the sufficient amount of and right type of development to support growth. Secondly, the social role is required to support communities by creating a high quality built environment with accessible local services to reflect the needs of the community. Lastly, the environmental role should protect and enhance the natural, built and historic environment.

The Framework re-iterates that planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The statutory status of the development plan remains as the starting point for decision making.

Section 5 'Delivering a sufficient supply of new homes' states that in order to support the Government's objective of significantly boosting the supply of homes, 'it is important that a sufficient amount and variety of land can come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay' (paragraph 60).

With regards to affordable housing, paragraph 65 states that where major developments are proposed involving the provision of housing, planning policies and decisions should expect at least 10% of homes to be available for affordable home ownership, unless this would exceed the level of affordable housing required in the area, or significantly prejudice the ability to meet the identified affordable housing needs of specific groups.

Section 8 'Promoting Healthy and Safe Communities' states that planning policies and decisions should aim to achieve healthy, inclusive and safe places (paragraph 92) which promote social interaction, be safe and accessible and enable and support healthy lifestyles.

Section 9 'Promoting Sustainable Transport' states that 'significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health' (paragraph 105). Developments should only be prevented or refused on highways

grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe (paragraph 111).

Within this context, applications for development should:

- a) give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use;
- b) address the needs of people with disabilities and reduced mobility in relation to all modes of transport;
- c) create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards;
- d) allow for the efficient delivery of goods, and access by service and emergency vehicles; and
- e) be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations (paragraph 112).

All developments that will generate significant amounts of movement should be required to provide a travel plan, and the application should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed (paragraph 113).

Section 11 'Making effective use of land' states that 'planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions' (paragraph 119).

Decisions should support development that makes efficient use of land, taking into account:

- a) the identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it;
- b) local market conditions and viability;
- c) the availability and capacity of infrastructure and services – both existing and proposed – as well as their potential for further improvement and the scope to promote sustainable travel modes that limit future car use;
- d) the desirability of maintaining an area's prevailing character and setting (including residential gardens), or of promoting regeneration and change; and
- e) the importance of securing well-designed, attractive and healthy places. (paragraph 124).

Where there is an existing or anticipated shortage of land for meeting identified housing needs, it is especially important that planning decisions avoid homes being built at low densities, and ensure that developments make optimal use of the potential of each site.

Paragraph 125 (c) states that Local Planning Authorities should refuse applications which they consider fail to make efficient use of land, taking into account the policies in the NPPF. In this context, when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight

and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards).

Section 12 'Achieving Well Designed Places' states that 'the creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this' (paragraph 130).

Planning decisions should ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping.

The NPPF is clear that planning permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions, taking into account any local design standards or style guides in plans or supplementary planning documents. Conversely, where the design of a development accords with clear expectations in plan policies, design should not be used by the decision-maker as a valid reason to object to development. Local planning authorities should also seek to ensure that the quality of approved development is not materially diminished between permission and completion, as a result of changes being made to the permitted scheme (for example through changes to approved details such as the materials used). (paragraph 134).

Section 14 'Meeting the challenge of climate change, flooding and coastal change' states that the planning system should support the transition to a low carbon future in a changing climate, taking full account of flood risk and coastal change. It should help to: shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure (paragraph 152).

Section 15 'Conserving and Enhancing the natural environment' states that planning decision should contribute and enhance the natural and local environment by protecting valued landscapes, sites of biodiversity, minimising impacts on and providing net gains for biodiversity, preventing new and existing development from contributing to unacceptable levels of soil, air, water or noise pollution or land instability and remediating contaminated land.

Section 16 'Conserving and enhancing the historic environment' states that in determining applications, Local Planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record

should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation (paragraph 194).

In considering the impacts of proposals, paragraph 195 states that local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise.

Paragraph 196 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

NPPF defines designated heritage assets as: ‘A World Heritage Site, Scheduled Monument, Listed Building, Protected Wreck Site, Registered Park and Garden, Registered Battlefield or Conservation Area designated under the relevant legislation’.

The proposal lies approximately 190 m from a Grade II listed building (Summerville House), and is separated from it residential development. Reference is made to the above as context to the proportionate consideration of the issues concerning consideration of a designated heritage asset.

NPPF adds that the effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset (paragraph 203).

Paragraphs 10, 11, 12, 13 and 14 of the NPPF outline a “presumption in favour of sustainable development”. This means approving development, without delay, where it accords with the development plan and where the development is absent or relevant policies are out-of-date, to grant planning permission unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

Planning Practice Guidance (PPG)

The relevant sections of the PPG are as follows:

Noise

This section states that Local Planning Authorities’ should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.

Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:

- engineering: reducing the noise generated at source and/or containing the noise generated;
- layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
- using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
- mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.

Design

This section states that where appropriate the following should be considered:

- layout – the way in which buildings and spaces relate to each other
- form – the shape of buildings
- scale – the size of buildings
- detailing – the important smaller elements of building and spaces
- materials – what a building is made from

Health and Well Being

This section states opportunities for healthy lifestyles have been considered (e.g. planning for an environment that supports people of all ages in making healthy choices, helps to promote active travel and physical activity, and promotes access to healthier food, high quality open spaces and opportunities for play, sport and recreation);

Travel Plans, Transport Assessments in Decision Taking

This section states that applications can positively contribute to:

- encouraging sustainable travel;
- lessening traffic generation and its detrimental impacts;
- reducing carbon emissions and climate impacts;
- creating accessible, connected, inclusive communities;
- improving health outcomes and quality of life;
- improving road safety; and
- reducing the need for new development to increase existing road capacity or provide new roads.

Relevant Local Policies

The Development Plan consists of:

- The Manchester Core Strategy (2012); and
- Saved policies of the Unitary Development Plan for the City of Manchester (1995)

The Core Strategy was adopted in July 2012 and is the key document in the Local Development Framework. It replaces significant elements of the Unitary Development Plan (UDP) and sets out the long term strategic planning policies for Manchester's future development.

A number of UDP policies have been saved and accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents as directed by the National Planning Policy Framework (NPPF).

Section 38(6) of the 2004 Act requires planning applications to be determined in accordance with the provisions of the Development Plan unless material considerations indicate otherwise. The NPPF also refers to this requirement. The relevant policies within the Core Strategy are as follows:

Policy SP1 'Spatial Principles' - one of the key spatial principles is the emphasis on the creation of neighbourhoods where people choose to live, providing high quality and diverse housing, in a distinct environment. New development should maximise the use of the City's transport infrastructure, in particular promoting walking, cycling and the use of public transport.

The proposal would contribute towards meeting housing growth in the City and creating a quality neighbourhood for residents to live in. Consideration has been given to minimising the impact on existing local residents along with promoting a sustainable development.

Policy T1 'Sustainable Transport' seeks to deliver a sustainable, high quality, integrated transport system to encourage modal shift away from car travel to public transport, cycling and walking. The proposal is located in an area where there is access to a range of public transport modes, including regular bus services. The scheme includes the provision of cycle storage and electric charging points. These aspects of the scheme would be conditioned. It is considered that the scheme would accord with the broadly aspirations set out in policy T1.

Policy T2 'Accessible areas of opportunity and needs'. It is considered that the proposed development is in a sustainable location and is close to forms of public transport includes bus services within walking distance on Plymouth Grove and Stockport Road, and would not have a significant impact on the local highway network and encourage the use of other forms of transport.

Policy H1 'Overall Housing Provision' states that the proportionate distribution of new housing, and the mix within each area, will depend on a number of factors, in particular, the need to diversify housing stock in mono tenure areas by increasing the availability of family housing. Policy H1 prioritises residential development on previously developed land, in particular through the re-use of vacant housing or other existing buildings. The redevelopment of this brownfield site would accord with policy H1.

The development will form a medium density residential scheme (110 dwellings per hectare) within an area of Central Manchester that is expected to accommodate housing growth. Consideration has been given to the access and the layout of the housing along with prioritising the re-use of the previously developed site. In addition, the proposal will also provide accommodation which will be attractive to a diverse range of housing needs through varying accommodation size. The proposed accommodation would comprise a mix of 3 and 4 bed houses, a 6 bed roomed house,

and 1, 2 bedroom apartments. These have been designed to comply with the guidance within the Manchester Residential Quality Guidance in regard to floorspace.

Policy H5 'Central Manchester', over the lifetime of the Core Strategy, will accommodate around 14% of new residential development. Priority given to family housing, and other high value, high quality development where this can be sustained. Higher density housing will be permitted within or adjacent to the Regional Centre which are adjacent to the City Centre.

The proposed development would accord with policy H5 by facilitating the delivery of a range of residential accommodation consisting of 1, 2, 3, 4 and 6 bedroom accommodation.

The residential-led development will provide high-quality homes to support the economic growth of the Region and redevelop an underutilised brownfield site in what is a sustainable location. There is a continued demand for Manchester to provide quality homes to support its economic growth and increased population. The proposed residential development will help to meet the recognised demand for high quality housing that meets the needs of the local community, including families. As such, the delivery of 72 new homes will contribute 7.76% to MCC's annual housing delivery target.

Policy H8 'Affordable Housing' states affordable housing contributions will be considered of 0.3 hectares and 15 units or more.

A viability assessment has been submitted which indicates that the development cannot support affordable housing. This has been independently assessed, and has identified issues in regard to the calculation of GDV values for the site. As a result the developer has offered the provision of 6 x 1 bedroom apartments which would be discounted market sale units for disposal, on a discounted market sale basis at 80% of market value. This would deliver 8.3% affordable housing at the site. Furthermore, it is understood that a further 12 of the proposed residential units would be affordable, but this is provided via a Registered Provider through grant funding.

The viability of the proposed development would be reviewed at a later date to determine whether this has changed. Further details will be provided in the main body of the report in this regard.

Policy EC2 'Existing Employment Space' seeks to retain and enhance the existing employment spaces and enhance existing employment space. Alternative uses will only be supported on sites allocated accordingly, or if it can be demonstrated that: The existing use is un-viable in terms of business operations, building age and format;

The existing use is incompatible with adjacent uses;

The existing use is unsuitable for employment having regard to the Manchester-Salford - Trafford SFRA; or

On balance, proposals are able to offer greater benefits in terms of the Core Strategy's vision and spatial objectives than the existing use.

The Council will seek to retain and enhance existing employment space and sites.

In this instance, however, it is considered that as this site is a strategic housing location, on balance the overall proposed development is considered to offer greater benefits in terms of the Core Strategy's spatial objectives.

Policy EC8 Central Manchester. Central Manchester is expected to provide approximately 14ha of employment land. The majority of this provision will be provided within:

1. The Corridor (Manchester) within the Central Manchester Regeneration Area is suitable for office, research and development, light industrial, general industrial, education and health. This area overlaps with the City Centre boundary and should be in line with CC1 and C1.

2. Existing employment and economic development areas:

Princess Parkway suitable for office development adjacent to the City Centre;
Along Stockport Road and Hyde Road suitable for employment as part of a mix of uses;

3. District centres of Hulme, Rusholme and Longsight all located on major radial routes through Central Manchester. These will be suitable for mixed use development providing local employment.

However, whilst the objectives of policies EC2 and EC8 seek to promote employment opportunities and safeguard existing development, in this instance it is considered that on balance the overall proposed development is considered to offer greater benefits in terms of the Core Strategy's spatial objectives.

Policy EN1 'Design principles and strategic character areas'. states that all development in Manchester will be expected to follow the seven principles of urban design (character, continuity and enclosure, public realm, ease of movement, legibility, adaptability and diversity) and have regard to the strategic character area in which the development is located. The proposed development has addressed the principles set out in policy EN1 .

Policy EN3 'Heritage' throughout the City, the Council will encourage development that complements and takes advantage of the distinct historic and heritage features of its districts and neighbourhoods, including those in the City Centre. New development must be designed so as to support the Council in preserving or, where possible, enhancing the historic environment, the character, setting and accessibility of areas and buildings of acknowledged importance, including scheduled monuments, listed buildings registered parks and gardens, conservation area and archaeological remains.

The proposed development site contains no heritage assets and is not located within a Conservation Area. It is, however, in close proximity to Victoria Park Conservation Area, which lies to the north west of the site. There are three listed buildings in the Conservation Area - Newbury House, Summerville House, and Addison Terrace which lie further to the west of the site. It is not, however, considered that the proposed development would adversely impact on the setting of these Grade II listed buildings on Daisy Bank Road, as the closest (Summerville House) is approximately 190.7m to the west of the site, and none of these buildings are in a direct line of sight with the proposed development.

Policy EN4 'Reducing CO2 emissions by enabling low and zero carbon development' the Council will seek to reduce fuel poverty and decouple growth in the economy, growth in CO 2 emissions and rising fossil fuel prices. All development must follow the principles of the energy hierarchy being designed to reduce the need for energy through design and the use of energy efficient features and through the use of low or zero carbon energy generating technologies. The Environmental Standards Statement suggests a fabric first approach, with the use of renewable technologies to enable further carbon reduction within the development to accord with the aspirations of policy EN4.

Policy EN6 'Target framework for CO 2 reductions from low or zero carbon energy supplies' states that developments over 1000 sqm will be expected to meet targets shown with the policy unless this can be shown not to be viable.

The application is accompanied by an Environmental Standards Statement which advises that to meet the challenging demands associated with sustainable development an approach utilising a number of measures including "fabric first" and renewable energy options is considered the most effective and efficient way of delivering an energy saving development that can meet housing needs and reduce CO2 emissions during the life cycle of each dwelling.

Due to the scale of the development, delivery of individual plots will be subject to the Future Homes Standard programme of changes to Part L of the Building Regulations which have exceeded Manchester City Council's Core Strategy Policy EN6 from June 2022. Part L 2021 will result in carbon reductions at least 31% over Part L 2013 along with significantly improved fabric performance.

Policy EN 8 'Adaptation to Climate Change' states that all new development will be expected to be adaptable to climate change in terms of the design, layout, siting and function of both buildings and associated external spaces.

Policy EN9 'Green Infrastructure' states that development should maintain green infrastructure in terms of its quantity, quality and function. Developers should enhance the quality and quantity of green infrastructure, improve the performance of its functions and create and improve linkages to and between areas of green infrastructure.

This is a brownfield site that has the potential to be subject to contamination due to past use. Due to the existing use of the site, the existing green infrastructure currently at the site is located to the north of the nursery building, with narrow grassed buffers alongside the former probation office premises. There are, however, opportunities to create improved areas of green infrastructure as part of the development proposals in the form of well sized amenity spaces for the proposed apartments and garden areas for the dwellinghouses.

Policy EN14 'Flood Risk' advises that proposals on sites greater than 0.5ha within critical drainage areas. In line with the risk-based sequential approach, policy EN14 seeks that development should be directed away from sites at the greatest risk of flooding, and towards sites with little or no risk of flooding; this should take account of

all sources of flooding identified in the Manchester-Salford-Trafford Strategic Flood Risk Assessment (SFRA).

All new development should minimise surface water run-off, including through Sustainable Drainage Systems (SUDS) and the appropriate use of Green Infrastructure. Developers should have regard to the surface water run-off rates in the SFRA User Guide.

Manchester contains many sections of rivers which are culverted or 'hidden'; where these are indicated in the SFRA beneath the proposed development site, further investigation will be required and the development proposal should take this into account; where feasible and appropriate development should seek to open up culverted/hidden rivers to reduce the associated flood risk and danger of collapse, taking advantage of opportunities to enhance biodiversity and Green Infrastructure. This site is in Flood Zone 1 and the issue is dealt with in greater detail elsewhere in this report.

Policy EN15 'Biodiversity and Geological Conservation' states the Council will seek to maintain or enhance sites of biodiversity and geological value throughout the City, with particular consideration to sites of biodiversity value including Sites of Biological Importance (SBIs) and Local Nature Reserves (LNRs). Policy EN15 requires developers to identify and implement reasonable opportunities to enhance, restore or create new biodiversity, either on site or adjacent to the site contributing to linkages between valuable or potentially valuable habitat areas where appropriate. Measures to respond to ecological enhancement and biodiversity are considered in detail elsewhere in this report.

Policy EN16 'Air Quality' states the Council will seek to improve the air quality within Manchester, and particularly within Air Quality Management Areas, located along Manchester's principal traffic routes and at Manchester Airport. Developers will be expected to take measures to minimise and mitigate the local impact of emissions from traffic generated by the development, as well as emissions created by the use of the development itself.

When assessing the appropriateness of locations for new development the Council will consider the impacts on air quality, alongside other plan objectives. This includes cumulative impacts, particularly in Air Quality Management Areas. The proposed works have the potential to lead to temporary impacts to air quality due to dust creation as part of the remediation and construction processes to be undertaken.

The proposal would not compromise air quality and would incorporate measures to minimise dust from the construction process and car usage during the operational phases in the form of Electric Vehicle Charging points and a Travel Plan. Measures to manage air quality impacts during construction process would be fully addressed as part of the construction management plan for the site.

Policy EN18 'Contaminated Land' states that the Council will prioritise remediation of contaminated land. The applicant has provided details relating to ground conditions and further investigative work would be needed to confirm the findings of the

provisional details and determine the levels of mitigation required. This matter is considered in detail elsewhere in this report.

EN19 'Waste' states proposals must be consistent with the principles of the waste hierarchy (prevention, reduction, re-use, recycling, energy recovery, and disposal). The applicant has a waste management strategy for the site which will ensure that residents adhered to recycling principles. Compliance with this strategy will form part of the conditions of the planning approval.

Policy DM1 'Development Management' all development should have regard the following specific issues:-

- Appropriate siting, layout, scale, form, massing, materials and detail;
- Impact on the surrounding areas in terms of the design, scale and appearance of the proposed development. Development should have regard to the character of the surrounding area;
- Effects on amenity, including privacy, light, noise, vibration, air quality, odours, litter, vermin, birds, road safety and traffic generation. This could also include proposals which would be sensitive to existing environmental conditions, such as proposals which would be sensitive to existing environmental conditions, such as noise;
- Community safety and crime prevention;
- Design for health;
- Adequacy of internal accommodation and external amenity space;
- Refuse storage and collection;
- Vehicular access and car parking;
- Effect on biodiversity, archaeological or built heritage;
- Green infrastructure;
- Flood risk and drainage.

These matters have all been considered and assessed as part of the detailed analysis of the application. In addition to the proposal has been assessed against the Council's space standards.

As set out within the issues section of this report below, the application is considered to accord with these policies.

Saved UDP Policies

Saved policy DC7 advises that the Council will negotiate with developers to ensure new housing is available at ground floor level to people with a disability. All new developments containing family homes will be expected to be designed so as to include safe areas within which children can play.

Saved UDP policy DC18.1 relates to development in Conservation Areas states that the Council will give particular careful consideration to development proposals within Conservation Areas, and advises that development proposals adjacent to Conservation Areas will only be granted where it can be shown that they will not harm the appearance or character of the area. This will include the protection of views into and out of Conservation Areas.

Saved UDP Policy DC26 advises that the Council intends to use the development management process to reduce the impact on people working and living in the city

and will consider the effect of new development proposals that are likely to be generators of noise. Conditions will be used to control the impacts of developments.

Other material policy considerations

Guide to Development in Manchester Supplementary Planning Document (2007) and Technical Guidance (2015)

The Guide to Development in Manchester - Supplementary Planning Document and Planning Guidance (SPD) was originally adopted on 11 April 2007. The document reflects and provides guidance on the importance of creating attractive, well-designed, well-maintained neighbourhoods. It includes a set of reasoned principles which will guide developers, designers and residents to the sort of development which is anticipated in Manchester.

In addition, technical guidance for developers was published in December 2015. It is composed of two documents, GD03 Environmental Protection and GD05 Planning and Noise.

These documents reflect and provide guidance on the importance of creating attractive, well-designed, well-maintained neighbourhoods. It includes a set of reasoned principles which will guide developers, designers and residents to the sort of development which is anticipated in Manchester.

Manchester Residential Quality Guidance (2016)

This is a material planning consideration in the determination of planning applications and weight should be given to this document in decision making. The purpose of the document is to outline the consideration, qualities and opportunities that will help to deliver high quality residential development as part of successful and sustainable neighbourhoods across Manchester. Above all the guidance seeks to ensure that Manchester can become a city of high-quality residential neighbourhood and a place for everyone to live.

The document outlines nine components that combine to deliver high quality residential development, and through safe, inviting neighbourhoods where people want to live. These nine components are as follows:

- Make it Manchester;
- Make it bring people together;
- Make it animate street and spaces;
- Make it easy to get around;
- Make it work with the landscape;
- Make it practical;
- Make it future proof;
- Make it a home; and
- Make it happen.

Providing for Housing Choice – Supplementary Planning Document and Planning Guidance (2008)

This document sets out in detail the Council's Affordable Housing Strategy and sets out the rationale for the target proportion of affordable housing and tenure mix.

Manchester's Great Outdoors – a Green and Blue Infrastructure Strategy for Manchester (2015)

Adopted in 2015, the vision for the strategy is that 'by 2025 high quality, well maintained green and blue spaces will be an integral part of all neighbourhoods. There are four objectives in order to achieve this vision which relate to quality and function of green and blue infrastructure; its use as a component of new development; improving access and connectivity and promoting wider understanding and awareness of its benefits.

Residential Growth Strategy (2016)

The Strategy aims to ensure that there is the right quality, mix and type of housing in the right locations to meet demand and changing demographics, develop neighbourhoods of choice and improve equality amongst the City's residents in terms of housing choice, quality and affordability in order to develop strong communities.

Strategic Housing Land Availability Assessment (2022)

Whilst the site is not included in the 2022 annual Strategic Housing Land Availability Assessment (SHLAA) (which outlines that MCC considers the land suitable for residential development and expects it to delivered housing over the next 15 years or more), it lies directly to the north of a SHLAA site.

Manchester Climate Change Framework: 2020-2025 and Manchester Zero Carbon Framework (2020):

In November 2018, Manchester City Council made a series of commitments, informed by the Manchester Climate Change Agency's (MCCA) work with the world-renowned Tyndall Centre for Climate Research based at University of Manchester. The key commitments are well documented and work towards the city becoming zero carbon by 2038, significantly accelerated from the original target of 2050.

A draft Manchester Zero Carbon Framework has been developed which sets out the City's overarching approach to meeting its science-based climate change targets over the period 2020-38 and draft action plans for the period 2020-22 are being developed by the Council and other strategic organisations and sectors.

On 10 July 2019, a motion asking Manchester City Council to formally declare a climate emergency was unanimously agreed. In March 2020 the Executive adopted the Manchester Climate Change Framework 2020-2025. The five-year framework sets out measures that MCC will take in order to reduce carbon emissions from its buildings, energy use and transport.

The Environmental Standards Statement suggests an approach utilising a number of measures including "fabric first" and renewable energy options as being the most effective and efficient way of delivering an energy saving development that can meet housing needs and reduce CO2 emissions during the life cycle of each dwelling.

**Other Legislative Requirements
Equality Act 2010**

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation

and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

For the reasons that are set out in this report the specific statutory duties concerning planning proposals affecting Listed Buildings or their settings are not engaged.

Section 17 Crime and Disorder Act 1998 provides that in the exercise of its planning functions the Council shall have regard to the need to do all that it reasonably can to prevent crime and disorder.

Principle of Development

The principle of the proposal and the schemes contribution to regeneration, as set out in the report, is considered to accord not only with national and local planning policies but would also deliver key outcomes for the city.

This is a site bounded by residential development and is in proximity to the Victoria Park Conservation Area. The site is currently occupied by a vacant former probation offices/ community centre and a former day nursery building. The application would enable the regeneration of the site for 72 dwellings which would boost the supply of much needed new homes, contributing to the regeneration of the local area.

Whilst the principle of the development is consistent with the planning policy framework, there are detailed matters that require particular attention. This report will therefore consider the following specific policies and material considerations and determine whether any undue harm will arise as a consequence of the development.

Climate Change

How climate change is addressed is a key factor in the consideration of planning applications. The proposal and key issues, including air quality, flooding and environment standards are considered in detail in the following sections.

Loss of Employment Land

Policy EC2 'Existing Employment Space' seeks to retain and enhance the existing employment spaces and enhance existing employment space, with alternative uses only being supported on sites allocated accordingly, or if it can be demonstrated that the criteria in policy EC2 would be met, on balance, proposals are able to offer greater benefits in terms of the Core Strategy's vision and spatial objectives than the existing use. The existing buildings on site have also been vacant for some time and are in a poor state of repair.

Proposed Residential Accommodation

There is an identified need for housing in Manchester to meet the growing population and workforce. In line with the requirements in the NPPF, the mix of housing has been designed to take into consideration existing and future housing needs in this area of City.

The homes have been designed to meet the Council's clear design aspirations and requirements as set out in the Residential Quality Guide. This includes the provision of front and rear garden spaces to support place making. The residential units have been designed with consideration to the space standards outlined in the Manchester Residential Quality Guide. In regard to the proposed apartments the sizes vary from 37.1sqm to 43.6sqm for a 1 person 1 bedroom apartment (space standard 37sqm), 61.6 sqm for a 3 person , 2 bedroom apartment , 71.5 sqm for a 3 person , 2 bedroom house , 106.1 sq.m for a 5 person , 4 bedroom house and 90.1 sq.m for a 4 person , 3 bedroom house .

The proposed residential development would comprise a mix of market housing, with an offer of 6 no. discounted market sale apartments from the applicant , and affordable rent (through a Registered Provider and grant funding consisting of a mix of consisting of a mix of 41 apartments and 31 houses . The mix of accommodation would be in form of 25, 1 bed and 16, two bedroom apartments, with four house types comprising , 15 three bedroom, 15 four bedroom dwellinghouses, and 1 six bed townhouse. Therefore providing a range , of 1,2,3,4, and 6 bedroom accommodation.

The design guidance is a material consideration in the determination of planning applications and sets out the space standards required to ensure quality in residential schemes coming forward. The design guidance is intended to provide a clear indication on what is required to deliver sustainable neighbourhoods of choice where people will want to live and also raise the quality of life across Manchester. It is considered that the proposed development has been designed in accordance with the design guidance outlined within this document. This is outlined in more detail in the paragraphs below.

The proposed ground floor gardens, and amenity spaces for the apartments would provide an opportunity for residents to benefit from outdoor living and to meet and socialise, helping to create a new community within the development. By including a mix of apartment and family housing types, it is considered that the development will be capable of accommodating a wide range of households.

The site is within easy walking distance of local amenities, as well as public transport links to larger district centres and the main retail facilities in Longsight and the city centre. It is considered that the proposed houses and apartments are an appropriate size and provide well-sized, well-designed living, and sleeping spaces.

Affordable Housing

Policy H8 sets out how developments should respond to the 20% contribution of affordable housing across the City. Using 20% as a starting point, developers should look to provide new houses that will be for social or affordable rent with a focus on affordable home ownership options. Any requirement or not for affordable housing will be based upon an assessment of a particular local need, a requirement to diversify the existing housing mix and the delivery of regeneration objectives.

An applicant may be able to seek an exemption from providing affordable housing, or a lower proportion of affordable housing, a variation in the mix of affordable housing, or a lower commuted sum, should a viability assessment demonstrate that a scheme

could only deliver a proportion of the 20% target; or where material considerations indicate that intermediate or social rented housing would be inappropriate. Examples of these circumstances are set out in part 4 of Policy H8.

The application has been accompanied by a Viability Assessment which indicates that the development cannot support affordable housing. This has been independently assessed, and has identified issues in regard to the calculation of GDV values for the site. As a result of further information an agreement has been reached that the scheme can deliver 6 x 1 bedroom apartments which would be discounted market sale units for disposal, on a discounted market sale basis at 80% of market value. This would deliver 8.3% affordable housing at the site. Furthermore, it is understood that a further 12 of the proposed residential units would be affordable, but this is provided via a Registered Provider through grant funding. The offer of affordable housing in the form of 6 no. 1 bed apartments is acknowledged, however finalised details would be subject to a legal agreement and may be subject to change in the type and tenure of the affordable units, and discussions are on-going.

The legal agreement with the Local Planning Authority would also include a provision for a reconciliation which would require a contribution to be paid if values change at an agreed point. Should there be an uplift in market conditions which would allow an the provision of an affordable housing contribution, there would be provisions incorporated to allow the viability to be re-tested to secure an additional contribution should this be viable.

Confirmation has been received from Arcon (registered Provider) who manage properties directly to the south of the application site advising that have been working in partnership with the applicant throughout the process, from inception, to ensure that the design of the development takes into account the needs of affordable housing provision to be managed by Arcon, should this be achievable on site, and they support the drawings and documentation which has been submitted with the application.. Arcon have advised that they are committed to working with the applicant to provide affordable homes through an application to the Government for a Housing Grant. Arcon's intention is for 12 affordable homes to be provided subject to that grant funding. All of these affordable dwellings are proposed to be affordable rent and let at the Manchester Living Rent. The mix of dwellings proposed would comprise 8 houses and 4 maisonettes. The housing and mix and the provision of entirely affordable rented dwellings meet Arcon's identified need and will assist in the contribution towards the 10,000 affordable homes required within the City. It is proposed the affordable units would comprise Blocks 4 and 8 on the Site Plan which are located in the southern section of the site, adjacent to the existing affordable housing development managed by Arcon on the neighbouring site.

Siting / Layout

The proposed development has been designed so that the apartment block would be located within the north of the site, parallel to Daisy Bank Road. The proposed 4 storey block has been sited in order to create a focal point within the street scene in proximity to the junction of Daisy Bank Road and Laindon Road. The proposed siting would also enable a good quality landscaped setting for the proposed apartment block and enable the majority of the mature trees on Daisy Bank Road and the

northern end of Laindon Road to be retained . The green spaces around the apartment block would provide amenity space for the future occupants.

Vehicle parking for the proposed future occupiers of the apartments has been located between the proposed apartment block and the proposed dwellinghouses, which would provide a further separation distance between the proposed 4 storey apartment block and the proposed three and two and half storey houses, to reduce any potential impacts from the massing of the proposed apartment block.

The family houses have a street frontage to Laindon Road and the internal access road with small front gardens, and tree planting to soften the appearance on the street scene. This would maintain active frontages and surveillance along the proposed routes within the development.



The proposed development comprises terraces of town houses, which are appropriate and in character with the wider area. Bin storage is shown to the rear of the dwellings with gated routes between properties to enable bins to be brought to collection points on the street.

In line with the Residential Quality Guidance and the Guide to Development in Manchester, the new buildings and their key architectural features along with the proposed tree planting and landscaping to front garden areas, would contribute positively to the street scene. Off street parking in the form of driveways are proposed to the dwellinghouses.

The potential impacts on the residential amenity of existing residents are discussed in more detail below; however, in relation to the siting and layout of these properties, the positioning and orientation of the new properties are considered to be acceptable.

The closest location to existing residential properties is along the east of the site adjacent to the boundary with the properties on Daisy Bank Road, Merwood Grove and Ayton Grove. The proposed properties either back onto the rear gardens of the existing properties with a rear garden creating (with the use of obscure glazing in

some instances where garden sizes are limited) adequate separation distance to the existing buildings, or are located side on, to the gables of existing houses . The potential impacts on residential amenity are discussed in more detail below, however in relation to the siting and layout of these properties, the positioning and orientation of the new properties are considered to be acceptable.

Therefore, overall, the siting and layout of the development maximises the relationship with the surrounding road network. The siting and layout provides a logical arrangement and is therefore considered to be in accordance with the development plan.

Scale / Massing

The Guide to Development in Manchester SPD advises that consideration should be given to the scale of new developments and ensure that they are informed by their context. Where buildings are of different scale to their surroundings they should be of the highest quality and be of landmark status. The scale of existing buildings in the immediate surroundings consists of a mixture of 2 and 3 storey housing.

Taking account of this context and the urban design objectives of providing appropriate scale frontages onto the surrounding streets, the proposed family houses would be 2.5 storeys in height to compliment the scale of the existing houses around the application site and the apartment building would be 4 storeys in height to forming part of the frontage to Daisy Bank Road. The scheme would introduce a four storey apartment block which would be sited between 5.6 m and 1.8m from the eastern boundary of the site, which adjoins a pair of two storey houses with driveways . Due to the curved nature of Daisy Bank Road to the east of the site ,the semi detached houses are also sited at an angle to the site , which would enable space to be retained between the buildings . The alignment of the proerpties would also reduce the impact from massing.



Daisy Bank Road street scene with existing housing to either side of the proposed apartment block

The properties to the west of the site on Daisy Bank Road are a traditional terrace of houses with gables to the front façade at roof level serving accommodation within the roof space, with three storey dwellings to the northern side of Daisy Bank Road opposite the site . Furthermore, Laindon Road has traditional properties which are two and three storeys, close to the back of pavement which forms the character of the area in the vicinity of the site.

The proposed buildings within the wider development have been sited throughout the site taking account of matters including privacy, overlooking and daylight and sunlight. The height of the scheme is considered appropriate given the prominent location along key roads within the area. The scheme would lead to a building set

behind mature trees on Daisy Bank Road , but which would contribute to the street scene , and respond to the mass of the building under construction at the end of the terrace on Daisy Bank Road at the junction of Laindon Road opposite this site.

The matter of the scale of the building and the potential impact on residential amenity will be considered in more detail later in this report.

Overall, it is considered that the scale of the development responds appropriately to the scale of the existing developments in the area and is considered to be acceptable in this location.

Appearance

Policy EN1 of the Core Strategy states that opportunities for good design, that enhance the overall image of the City, should be fully realised. This is reiterated within the Guide to Development in Manchester SPD along with the NPPF. It is considered that the design of the proposed development has been to adopt many of the key characteristics of the surrounding area.

As described in the Design and Access Statement, the properties have been designed to complement and enhance the existing environment.

The new homes would be contemporary in design with tall vertical windows , and feature brickwork elements . The development would involve the use of traditional materials (brickwork and slates). The images in the description section of the report are typical of the designs which would form the proposed development.

The materials used would be red brick types with grey roofing material, double height glazing, grey aluminium window frames and door , which reference themselves to the local vernacular without being a pastiche.



The internal street scenes and the frontages to Laindon Road and Daisy Bank Road would look to compliment the existing character while not repeating the form of existing residential stock that surround the boundaries of the site.



The design and orientation of the proposed buildings, integrated landscaping and public open space would improve the site visually and deliver a high-quality development on a site with brownfield ground conditions. It is, however, recommended that a materials condition be attached to any approval to ensure the type and colouration of the proposed materials.

Residential Amenity

The site is bounded to the south and east by residential properties. As the buildings on the site have been vacant for some time, it is inevitable that the development would have some impacts. This includes noise from site remediation, construction and once operational. It is not considered the levels of noise would be to such an extent to cause unacceptable harm, and that conditions set out in the report would enable those aspects likely to give rise to residential disamenity, to be effectively managed for the temporary period of for example, the construction works. It must also be noted that the existing uses on the site could lead to significant comings and goings from both the office use and the day nursery. Any noise and disturbance would not be unduly harmful in this context.

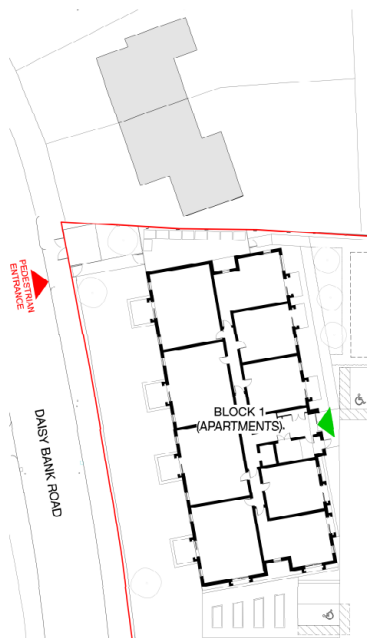
As advised earlier The scheme would introduce a four storey apartment block which would be sited between 5.6 m and 1.8m from the eastern boundary of the site, which adjoins a pair of two storey houses with driveways. Due to the curved nature of Daisy Bank Road to the east of the site, the semi detached houses are also sited at an angle to the site, furthermore the existing houses are set approximately 9m from the back pavement. The proposed siting of the apartment block would be 8.2m from the back of pavement, with the proposed alignment designed to reduce impacts from overshadowing.

The distance from the rear elevation of the proposed dwellings and side gable of the existing dwellinghouse on Ayton Grove to the east would be 11.35m. In order to safeguard both existing resident and future occupiers the proposed internal layout situates a bathroom at first floor level in the rear elevation which would be obscurely glazed. Furthermore, the second floor bedroom windows to the rear are rooflights to reduce any potential or overlooking. In addition the terrace of dwellings in the south east of the site would be in proximity to existing dwellings operated by Arcon Housing Association, with the terrace projecting beyond the rear elevation of the existing cottage flats by 5m at ground floor level, and 3m at first floor level. Whilst is acknowledged that the massing would have some impacts to the residential amenity

of the occupiers of adjacent cottage flats , no representations from residents have been received regarding this matter, and it is not considered be give rise to significant impacts to warrant the refusal of permission. Furthermore, the location of the proposed development to the north of the cottage flats would reduce any potential for loss of sunlight or overshadowing.

This block and other terrace of dwellings proposed within the south west of the site have been identified as being affordable housing to be potentially to be delivered by Arcon .Where gables of the proposed dwellinghouses within the site would be in proximity to the gables of existing dwellings to the east of the site, the separation distances would vary from a pinch point of 2.8m to 5.5m . This is due to the angled nature of the eastern boundary.

Concerns raised in relation to the overlooking of the supported housing scheme which lies to the east of the site (coloured grey in the visual below) and the vulnerability of residents has been noted . The eastern gable elevation of the proposed block of apartments incorporates a window at each floor in the recessed element of the gable to serve a shower room , which it is proposed to obscurely glaze to safeguard both the privacy of the adjacent property and the future occupiers of the apartments. It is recommended that this aspect of the scheme is conditioned. A secondary living room window would also be located in the eastern gable at ground level.. Given that 2m high close boarded fencing is proposed to this boundary , it is not considered that the proposed block of apartments would give rise to any unduly detrimental impacts to the occupiers of the supported housing due to overlooking and privacy .



The use of conditions in relation to boundary treatments would also enable privacy issues to be appropriately managed.

Ground Conditions

Policy EN18 of the Core Strategy requires that consideration should be given to potential sources of ground contamination and the effect on new developments. A Phase I and Phase II Geo-Environmental Site and a Remediation & Enabling Works Strategy is submitted in support of the planning application.

The scheme would lead to the redevelopment of a brownfield site. The former recent commercial uses on the site have the potential to have led to contamination of the ground, which would need to be remediated prior to any residential development being undertaken. Furthermore, prior to the recent uses and historic residential development, there were old clay pits and a pond in the surrounding area.

Site investigation works have been undertaken and confirmed the scale and nature of the contamination due to these past uses.

An appropriately worded condition has been recommended to ensure compliance with these reports and the submission of final verification assessments in order to comply with policy EN18 of the Core Strategy.

Lighting

A lighting impact assessment accompanies the application, which confirms that a low impact lighting strategy is proposed. This will assist in regard to the proposed bat boxes which are proposed as part of the development of the site.

The scheme including the incorporation of 3no. building mounted lighting to the apartment block 3.5m above ground levels on the southern and eastern façade of the apartments to illuminate the route from the apartment block to the proposed bin store and cycle store, 1m high bollard LED lighting to the gardens (2 in each garden area) of the proposed apartment block, and 5m column mounted downlighters to the access road and the apartment block car park area. The scheme is considered to be acceptable. It is however, proposed to attach a condition to require the implementation of the submitted scheme.

Highway Related Matters and Car Parking

Policy T1 and T2 of the Core Strategy seeks to encourage modal shifts away from the car and locate new development that is accessible by walking, cycling and public transport. Policy DM1 goes on to state that traffic generation and road safety must be considered as part of new developments.

A Transport Statement and Interim Travel Plan has been submitted in support of the application. The site is accessible by sustainable modes of transport. The surrounding area exhibits good levels public transport opportunities within acceptable walking distance on Plymouth Grove, Stockport Road, and Anson Road.

The development proposals include the provision of 31 car parking spaces for the dwellinghouses (100% provision (with an additional space provided for the proposed six bed property)), and 18 spaces for the proposed apartments (44% provision). The proposed spaces for the apartments would be located in proximity to

the south of block. 2 spaces for disabled users are proposed to serve the apartments (5% of total parking provision) . All of the proposed spaces would have electric vehicle charging points .The level of parking provision is considered to be appropriate to the scale, location and nature of the development.

The submitted details also indicate the provision 200% cycle storage for the proposed dwellings in the form of secure storage in the rear garden areas . The development also includes the provision of a secure communal cycle store within the grounds of the apartment block. This level of car parking, cycle parking and electric vehicle charging points is considered to be acceptable for the proposed development in this location. It is recommended that this aspect of the scheme is conditioned.

Furthermore, a number of measures to encourage travel to and from the site by sustainable modes are contained within the site Interim Travel Plan. It is anticipated that this would also help to reduce car parking demand.

Primary access to the proposed development is to be provided via two points on Laindon Road ,with the existing access to the site on Laindon Road being stopped up and reinstated to footway. The first access point (serving the apartment block (Block 1) and the houses within Block 2) will be located south of the Laindon Road / Daisy Bank Road junction, whereas the second access point (serving the remaining housing blocks) will be located to the south of the junction, staggered opposite the Laindon Road / Kedleston Avenue junction. These access points would lead into the new internal roads proposed within the site boundary.

In terms of pedestrian access, in addition to that which would be provided along the proposed road layout , access from Daisy Bank Road would be provided at the north-west corner of the site to serve the proposed apartments.

The internal access road has been designed to accommodate waste and delivery vehicles so that they can enter the site, turn around and exit in a forward gear.

The Transport Assessment indicates that the proposed development would generate 11 additional vehicle movements in the AM and PM peak hours , in comparison with the former uses on the site. It is not considered that the indicative increase in traffic movements at the site would result in significant impacts on the surrounding highway network.

Following a review of road collision statistics, it is noted that there have not been any accidents in the immediate vicinity of the proposed development site , although there had been 32 recorded accidents which have occurred within the study area which includes Anson Road and Dickenson Road and Clarence Road, with a significant proportion of the accidents on the busier routes around the proposed development during the 5-year study period . The review does not highlight any clusters of collisions with common causation factors that would warrant further investigation in relation to the proposed development.

However, there are a number of off-site highways works that would be required in result of the development. Some are necessary to achieve the new vehicular access

points. It therefore proposed to attach a condition relating to all highways works (including any off site works).

Due to the scale of the development and the proximity of the site to a road junction and existing properties, a Construction Management Plan (CMP) is requested to be submitted for this development. It has been agreed that this should be requested through the inclusion of an appropriately worded condition.

Overall, it is considered that the development would have an impact on the local highway network, however the level of impact is not considered to be severe that would warrant refusal of the planning permission. There would be adequate car and cycle provision to serve the needs of the development. Travel planning would help take advantage of the sustainable location of the application site in order to further reduce the reliance on the car to the site. Servicing and construction requirements can also adequately met at the site. The proposal therefore accords with policies SP1, T1, T2 and DM1 of the Core Strategy.

Servicing

It is proposed that the service vehicles will service the apartment block on-street by pulling up on Daisy Bank Road, adjacent to the pedestrian entrance. The apartment block bin store is located to the east of the building. Bins would be pulled out from the bin store by waste operatives, unloaded onto the refuse vehicles and then taken back to the bins store. The houses within Block 2 and 5 will be serviced from Landon Road. Residents would be responsible for presenting the bins to the designated collection point on collection day and returning them. No parking restrictions that prevent loading/unloading are in place around the site, although there is on-street parking along Daisy Bank Road and Landon Road. The Transport Statement proposes the introduction of a parking restriction (with no waiting between certain hours of the day on a section of Daisy Bank Road in proximity to the proposed bin store to serve the apartments and Landon Road in proximity to the proposed access points). Refuse vehicles would turn into the southern access point to service the remainder of houses within Block 3, 4, 6, 7 and 8.

Travel Plan

A Interim Travel Plan accompanies this application, which considers access by public transport , cycle and on foot and includes measures and initiatives to encourage active and sustainable travel.

It is recommended that a condition is attached to any approval to require the submission of a detailed Travel Plan following first occupation.

Heritage Impacts

The proposed development site contains no heritage assets and is not located within a Conservation Area. It is, however, in close proximity to Victoria Park Conservation Area , which lies to the north west of the site. There are three listed buildings in the Conservation Area - Newbury House , Summerville House , and Addison Terrace . It is not, however, considered that the proposed development would adversely impact on the setting of these Grade II listed buildings on Daisy Bank Road, as the closest (Summerville House) is approximately 190.7m to the west of the site , and is not in a direct line of site with the proposed development .

The proposed development would lead to an improvement of the site in visual terms, and is not considered to give rise to significant impacts to the appearance or character of the Victoria Park Conservation area to the north west of the site .

Landscaping

21 trees would be planted , predominantly in the front gardens of the proposed houses, with trees also planted to the rear of plots adjoining the internal access road, the landscaped setting of the proposed apartment block and a further 6 fruit trees within the landscaping to the south of the apartment building.

A sedum roof is proposed to the communal cycle store to serve the apartments.

The footways, road and driveways within the development are proposed to be surfaced with tegula concrete paving in different colourations, with the parking space to serve the apartments comprising gravel stabiliser grids on a geotextile base. However, the finalised colouration of the proposed hard landscaping materials have not been provided for consideration.

It is therefore recommended that the soft and hard landscaping treatment proposed for the development are subject to conditions, including the requirement for colouration the proposed hard landscaping materials, the implementation of the proposed planting scheme, a requirement for any soft planting which dies or is removed within 5 years of planting to be replaced , together with an on-going maintenance strategy with an identified and confirmed management company for the planting associated with the apartment element of the proposed development.

Trees

The application is accompanied by an Arboricultural Impact Assessment . None of the trees are covered by a Tree Preservation Order and the site is not located within a Conservation Area.

Trees on the site were surveyed as part of this assessment and it was found that 3 trees were of high value (Category A), 22 trees of moderate value (Category B), 12 trees of low value (Category C) and 3 trees of removal value (Category U).

8 no. mature trees to the frontage of the site with Daisy Bank Road and 4 no. trees on Laindon Road close to the junction of Daisy Bank Road would be retained, together with 2no. trees on the eastern boundary of the site However , 19 trees (10 category B trees 6 category C trees and 3 category U trees) and 7 groups of trees would need to be removed,. Five trees and 1 group of retained trees would also need to be pruned.

A scheme of new tree planting has been incorporated into the Landscaping Plan which seeks to mitigate any loss of trees on site. This encompasses a mixture of native and ornamental trees planted at heavy standard size. The species have been chosen to suit the location and allow them to develop into mature trees, smaller trees have been located close to the buildings, however, where space is available, such as close to the boundaries, larger trees have been used. These will provide structure and height to the development. Fruit trees have also been proposed, to one of the

landscaped areas adjacent to the proposed apartments, which would also contribute to the bio-diversity of the site by providing foraging opportunities.

Concerns expressed by Arboricultural officers in regard to the impact on the retained trees on this site due to potential damage to the rooting areas and the constraints of the building lines and landscape requirements, and recommendation that careful monitoring and site supervision by the appointed arboricultural consultant will be required have been noted.

It is recommended that this aspect of the scheme is conditioned, and that the condition makes specific reference to a tree consultant being present during excavations of the foundations to supervise and ensure the roots are protected. Furthermore, that the development shall be carried out in accordance with the agreed method statement, and that all tree work should be carried out by a competent contractor in accordance with British Standard BS 3998 "Recommendations for Tree Work".

Ecology/Biodiversity

The site comprises two vacant buildings with areas of hard standing used for parking and servicing, with trees predominantly along the northern and western perimeters of the site, with some trees also on the eastern and southern boundaries. There are areas of unkempt grass around the periphery of the buildings.

There was no evidence of badger activity identified on site, but the accompanying appraisal recommends 'best practice measures are adhered to, given the mobile nature of badgers.

No evidence of roosting bats was identified during the survey, however the several mature and early mature scattered trees on the site could be used by bats for foraging. Greater Manchester Ecology Unit have not made any requests for further surveys in regard to the presence of bats.

Due to the open nature of the site with trees on the boundaries and area of scrub there is the potential for nesting birds to be present. It is therefore recommended that a condition to require any removal of trees and vegetation outside of the bird nesting season, and if the removal of trees and vegetation needs to occur from March to August (inclusive), then any areas to be impacted should be checked for active bird nests before the works begin (by an ecologist).

The biodiversity enhancement measures recommended by the ecology assessment have been incorporated into the Landscape Plan, and relates to the incorporation of bat and bird boxes within the development. Four types of bat box are proposed (total of 4 no. boxes), together with a swift box, 2 no. sparrow terraces, a robin box and two nest boxes. Hedging has also been incorporated to divide the front garden dwellings, with fruit trees proposed to one of the garden areas for the proposed apartments. It is recommended that this aspect of the scheme is conditioned.

No non native invasive plant species were identified on the site.

Noise/ Overheating

The application is accompanied by a noise assessment, which was dominated by road traffic and infrequent aircraft passing over the site, with some local construction noise, assesses the effects noise across the site. The Noise Assessment demonstrates that appropriate target internal noise levels are achievable using conventional mitigation measures. Mitigation advice, including the use of suitably specified glazing and acoustically attenuated ventilation, have been recommended to reduce to a minimum the adverse impact on health and quality life arising from environmental noise.

It is acknowledged there are existing residents adjacent the application site and with the proposed development and occupation there would be some impacts. However, it is not considered that this would be unusual or significant. Furthermore, the noise arising from the proposed residential development would be likely to be less than the former uses of the site as a day nursery and probation offices/ community centre.

Observations from Environmental Health in regard to requirements for further assessment regarding overheating for windows where windows are closed, but ventilators or MVHR systems to meet Part F minimum ventilation requirements are operational, together with further data and information regarding how the LA90 was calculated, are noted. It is therefore recommended that a condition relating to acoustic insulation, and the potential for overheating to be assessed, and that the noise insulation scheme take this into account is attached to any approval, so that the finalised specifications of the mitigation measures can be fully considered.

Furthermore, that a verification report is submitted upon completion of the development and before first occupation of the residential units, to validate that the work undertaken conforms to the recommendations and requirements in the approved report, and that this aspect of the scheme is conditioned.

Daylight and Sunlight

A Daylight and Sunlight Assessment accompanies this application. The assessment concludes that 100% of the surrounding properties will fully accord with the BRE guidelines with regard to VSC Daylight, NSL Daylight and APSH Sunlight. Overshadowing analysis was also undertaken as part of this assessment to the rear gardens serving the surrounding residential properties. This analysis indicates that 7/7 (100%) of gardens assessed will continue to receive levels of sunlight with the proposed development in place in full accordance with the BRE Guidance.

Flood Risk / Surface Water Drainage

The proposed development is located in Flood Zone 1 (area at low risk of flooding), i.e. land defined as having an annual probability of flooding less than 0.1% (1 in 1000 years). The development would be classed as 'More Vulnerable' under the NPPF vulnerability classification due to the residential nature of the proposed development. The NPPF does not require that the Sequential Test be applied to development proposals in Flood Zone 1.

The Flood Risk Assessment and outline drainage strategy accompanying the application states that the development will not result in a reduction in flood plain

storage compared to the existing situation, and in considering the potential drainage options for the site at present it is assumed that surface water runoff arising from the development will discharge into the existing combined sewer to the west of the site. Victoria Park Probation Centre.

The proposed development would increase the proportion of the site covered by impermeable surfaces and would therefore generate more runoff. Attenuation measures would therefore be required as an allowance for climate change. Foul sewage arising from the development would be discharged into the public combined sewer in Laidon Road to the west of the site. The outline drainage strategy considers the use of attenuation-based SuDS to be feasible with a discharge to the combined sewer on the western boundary of the site, and recommends the incorporation of a detention facility at the western boundary. The report indicates the required detention facility to be 65.2 m³ for the 1 in 30-year event restricted flow. Similarly, the estimated volume to be retained on site for the 1 in 100-year storm event including an allowance for climate change is between 128.6 m³ and 160.8 m³.

Furthermore, the assessment recommends that the minimum finished floor levels for the development should be set at a nominal height above the proposed ground levels to allow for overland flow during exceedance flows from an extreme event or a drainage failure.

In view of the mitigation measures indicated as being required in the assessment and the comments from Flood Risk Management officers, and United Utilities it is recommended that conditions relating to the details of surface water drainage and that the system that is put in place is managed and maintained thereafter are attached to any approval.

Sustainability and Energy Efficiency

The application is accompanied by an Environmental Standards Statement which advises that to meet the challenging demands associated with sustainable development an approach utilising a number of measures including “fabric first” and renewable energy options is considered the most effective and efficient way of delivering an energy saving development that can meet housing needs and reduce CO₂ emissions during the life cycle of each dwelling.

Due to the scale of the development, delivery of individual plots would be subject to the Future Homes Standard programme of changes to Part L of the Building Regulations which have exceeded Manchester City Council’s Core Strategy Policy EN6 from June 2022.

In regard to the proposed apartment block, a fabric first approach, together with the use of electric panel heaters (with App Control Functionality) and a KERS indoor heat pump, which is a combined hot water, heat recovery and MEV system with storage cylinder, which converts waste heat energy from bathrooms and kitchens to low cost, renewable hot water are proposed.

In relation to the proposed dwellinghouses a fabric first approach, together with the use of electric panel heaters and a KERS indoor heat pump, with electricity demands off set with the use of solar photovoltaics.

All proposed solutions identified would exceed the minimum betterment of a 15% over Part L 2010, thereby arcoording with policy EN6 . The proposed design effectively utilises design and construction techniques to reduce the demand for energy, employs renewable technology and provides at least a 50% betterment over the minimum requirements for Part L 2021.

Designing out Crime

The application is accompanied by a Crime Impact Statement (CIS) by GMP Design for Security officers . The report suggests that the proposed scheme is acceptable in terms of layout. Furthermore , in order to further improve the scheme from a crime prevention perspective there is a request that consideration is given to gable ends of properties to be protected with shrubbery or a low boundary to prevent misuse, access control to apartment entrance lobby (including a video entry system) for the proposed apartment block and that the overall development achieves secure by design accreditation. These measures are referred to in the CIS .

In view of the comments received it is proposed to attach a condition to any approval to required that the proposed development reflects the physical security specification listed in sections 3 and 4 of the Crime Impact Statement, with a requirement of the development to achieve secure by design accreditation.

Refuse Storage

The waste strategy includes the provision of bins stored in the curtilage of each dwellinghouse and the cottage flats, in the rear garden areas , for general waste, pulpable recycling, mixed recycling and for green waste.

In regard to the proposed blocks of apartments a bin store to the east of the building includes the provision 9no. 1,100 litre eurobins,

A condition is proposed to be attached to any approval in regard to refuse storage arrangements within the scheme.

Air Quality

This site is not located in a City Council designated Air Quality Management area (AQMA).

The application is accompanied by an Air Quality Assessment which considers the potential impacts to air quality. This recommends measures for inclusion in a Dust Management Plan to minimise emissions during construction activities, and that with the implementation of these mitigation measures the impact of construction phase dust emissions was considered to be 'not significant' in accordance with relevant guidance.

Such measures would form part of an agreed construction management plan that would be subject to a condition of the planning approval. It is considered that the impacts on air quality during construction would be temporary and predictable and could be appropriately mitigated through the measures identified above.

The assessment states that the operational phase of the proposed development was assessed in regard to potential impacts which could occur due to development generated road traffic. The development was not predicted to result in any new

exceedances of the relevant air quality objectives and the impact of the development on local air quality was predicted to be 'negligible' in accordance with guidance.

Furthermore, the proposed development would include the provision of electric vehicle charging infrastructure and cycle parking at the proposed dwellings. Finalised details of the location and type of charger have not been provided for consideration. It is therefore recommended that this aspect of the scheme is conditioned to ensure there are no detrimental impacts to existing air quality conditions as a result of the development.

A condition is proposed in relation to the design of the cycle stores which are proposed and to ensure the implementation of the cycle storage arrangements before the occupation of the proposed dwellings.

On this basis the scheme is considered to accord with the aspiration of policy EN16.

Broadband

The application is accompanied by Broadband Connectivity Assessment which indicate that existing broadband infrastructure and good connectivity is already available with existing fibre installed along streets adjacent to the site.

It is therefore anticipated that the application properties would have access to good download speeds.

Television and Radio Reception

A baseline (pre-construction) signal survey and reception impact assessment has been undertaken to determine the potential effects on the local reception of television and radio broadcast services from the proposed development.

The assessment indicates that the proposed development is unlikely to cause any interference to the reception of any television or radio broadcast platform. As no interference is expected, no mitigation measures are required to restore the reception of any broadcast service.

Archaeology

This site has been the subject of an archaeological desk-based assessment.

The outcome of the assessment is there is a high potential for the site to contain buried archaeological remains, including pre-industrial field boundaries and relict soils, early to mid nineteenth-century roads and remains of Stafford House and associated walls and outbuildings. Any such remains that do exist within the site are likely to be of local to significance and, without a programme of mitigation, the scheme has the potential to damage these remains.

In view of the former presence on the site of the buildings and structures referred to above, it is recommended that any approval be conditioned to require a programme of archaeology works to be submitted in accordance with a Written Scheme of Investigation to ensure any remains which are found are recorded.

Permitted Development

The National Planning Policy Guidance states that only in exceptional circumstances should conditions be imposed which restrict permitted development rights otherwise such conditions are deemed to be unreasonable. In order to protect the nature of the

accommodation being proposed it is considered there is a case for ensuring the dwellings are not converted into multiple occupation without proper consideration. It is recommended that a condition should therefore clearly define the approved residential units under the C3(a) use and to remove the permitted development rights that would normally allow the change of use of a property to a House of Multiple Occupation (HMO) without the requirement for formal planning permission.

Furthermore, it is considered that there is a case for ensuring that apartments hereby approved shall be used only as private dwellings and not as serviced properties or similar uses where sleeping accommodation (with or without other services) is provided by way of trade for money or money's worth and occupied by the same person for less than ninety consecutive nights). An appropriate condition is therefore proposed.

Inclusive Access

The applicant has confirmed that all houses would have level access to the main entrance, however, the duplex apartments at each end of the terrace forming block 4 are shown on the submitted plans as having a stepped rear access. This annotation however, reflects that there is a stair as soon as you pass through the front door leading up to the proposed upper floor apartment. A level threshold to these properties would be provided at ground level. All apartments have a single floor level with no level changes within each apartment.

Lift access is provided within proposed apartment block, with a refuge area in proximity to the lift and stairs for disabled people on the upper floors. 2 parking spaces for disabled users with appropriate transfer space are provided within reasonable distance of the entrance to the building.

The proposed cycle stores would be of sufficient size (1900mm x 850mm x 1200mm) to accommodate a mobility scooter, or a disability cycle.

The proposed kitchen gradient adjacent to the apartment block would include the provision of raised beds to enable access for all users.

Boundary treatment

The existing high stone wall to Daisy Bank Road and brick wall on the Laindon Road frontage at the junction with Daisy Bank Road would be reduced in height to 750mm including copings with a railing above to an overall height of 1.8m. The eastern boundary would be comprised of 2m close boarded fencing where it would adjoin the gardens of the retained properties to the east, with 2m high railings installed where the end of Merwood Road adjoins the site. The boundary treatment to the Laindon Road frontage would be a low brick wall.

It is acknowledged that the number of driveways and associated vehicle cross overs within the site would preclude the incorporation of significant levels of front boundary treatment to those plots. However, plans have been received which show the inclusion of pedestrian gates and associated gateposts together with railing boundary treatment above a plinth, to the back of pavement to the properties within block nos. 5 to 8. A condition is recommended in relation to the implementation of these details, but with a further requirement regarding the finalised details of the boundary

treatment to rear gardens and the eastern and southern boundary of the site , including finalised location and elevational designs.

Community Engagement

A leaflet drop for the public took place on 8th September 2022. The leaflet provided local residents and businesses with details of the proposal and the upcoming online and in-person consultation opportunities for them to share their views on the scheme. The leaflet was delivered to 440 properties in proximity to the development site.

Prior to the launch of the consultation, Local Ward Councillors were briefed on the proposal by the applicant.

A pre-application public consultation was undertaken by the applicant on 15th September 2022 at St. Joseph's RC Primary School, when 11 people attended, and the consultation web page was online until 23rd September 2022 to enable further feedback to be made.

13 responses were received from the local community , with 11 being either supportive or partially supportive. Concerns, however, were expressed over the creation of a road or footpath to link the development to Merwood Grove; about congestion on local roads; impacts to pedestrian safety at the junction of Laindon Road and Daisy Bank Rad, and lack of parking . Other matters raised related to the density of the development , and a request that the applicant remove the trees in proximity to a dwelling on Merwood Grove, the loss of any mature trees , and the safety of the future apartment occupiers using the proposed bin and cycle stores. Queries were also made regarding the provision of electric vehicle charging points and sustainable energy generation .

Other matters raised by residents

A resident raised concerns that Adult Residential Care facility has opened after a redevelopment of the two houses on Daisy Bank Road without planning permission. Permission, however was granted on 22nd January 2021 for a change of use to a residential care home providing accommodation for up to 6 people (Class C2) at no.39 Daisy Bank Road , which was subject 6 planning conditions.

It is acknowledged that the site adjoins a house with vulnerable residents. Whilst it would not be feasible to restrict the construction hours to those suggested , any approval would be subject to a condition to require the submission of a construction management plan. Guidance in relation to construction hours is covered by separate legislation and such impacts are short lived and are not material planning impacts that could be used to refuse planning permission. Furthermore , whilst the the former probation office and community centre has been vacant for some time , the hall element of the building was able to operate between 8.30 a.m. to 11.00 p.m. on Mondays to Saturdays and 9.00 a.m. to 5.00 p.m. on Sundays, with the potential to generate noise from visitors on arrival and departure, if this use were to re-commence. In addition the other building was used as a day nursery with an external play space, which would have had the potential to lead to noise to the existing adjacent occupiers.

The request for the inclusion of conditions for the build to include for adaptation of the house with vulnerable residents, in relation to sound proofing to their walls, and replacement of windows, to mitigate the impact of this scheme, has been noted, but would not be possible to condition the additional suggested measures, particularly as the issues regarding noise would primarily relate to the build period. Any impacts associated with construction would be short lived and would not be material considerations which could be used to refuse planning permission. If there are any issues relating to noise or pollution associated with the construction phase then these would be controlled through separate legislation such as the Environmental Pollution Act.

Local Labour and jobs

The applicant has submitted a statement regarding local labour in which they agree with the Work and Skills Team indicative targets against key performance indicators issued as part of the Agreement, and will endeavour to deliver and monitor these targets and provide regular updates as agreed with the Work and Skills Team. A condition is recommended to cover this matter.

Legal Agreement

It is recommended the proposal be subject to a legal agreement under section 106 of the Planning Act to deliver 8.3% affordable housing on the site and with a mechanism to re-test the viability should there be a delay in the implementation of the planning permission and a further review prior to the occupation of the development as explained in the paragraph with the heading 'Affordable Housing'.

Conclusion

The proposed scheme offers an opportunity to bring forward a sustainable development of family housing, identified as meeting the needs of the city.

All issues have been set out in the report and addressed and this includes where necessary mitigation to ensure the schemes delivery.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation Minded to Approve subject to a legal agreement in respect of affordable housing to be delivered at the site and a reconciliation clause.

Article 35 Declaration

Officers have worked with the applicant / agent in a positive and proactive manner to guide the application through all stages of the planning process and resolve any issues that arose in dealing with the planning application. The original submission raised initial concerns about massing in regard to impacts to the existing housing to the east of the site , and affordable housing, these matters has been satisfactorily resolved and the appropriate conditions have been attached.

1.The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2. The development hereby approved shall be carried out in accordance with the following drawings and documents:

Location Plan ref: A662-P-001 A;
 Existing Elevations plan ref: A662-P-016;
 Maintenance block plan ref: A662-P-103;
 Street Elevations plan ref: A662-P-110 A;
 Street Elevations plan ref: A662-P-111 A;
 House type 6 A elevation ref: A662-P-168A;
 House type 6A section ref: A662-P-175;
 Block 1 roof plan ref: A662-P-202A;
 Block 1 Elevations plan ref: A662-P-210C;
 Block 1 Sections plan ref:A662-P-220B;
 Blocks 2 , 5 and 76 elevations plan ref: A662-P-310A;
 Block 3 elevation plan ref: A662-P-410;
 Block 4 elevation plan ref: A662-P-510;
 Block 4 elevation plan ref: A662-P-610;
 Block 8 elevation plan ref: A662-P-810;
 House type 4A Envelope Section Detail plan ref: A662-P- 901;
 Design and Access Statement dated October 2022;
 Air Quality Assessment dated October 2022 ref: VPPC-BWB-XX-ZZ-LA-RP-0001-AQA;
 Floor Areas Schedule ref: A662-P- 901 rev.E;
 Broadband Connectivity Assessment Issue 1.2 20.12.2022;
 Construction Methodology and Management Strategy dated October 2022;
 Crime Impact Statement Version ref.2022/0422/CIS/01, Version B dated 25.10.22;
 Environmental Standards Statement ref: 2482-CWC-XX-XX-RP-002 dated 22.10.2022;
 Fire Statement ref: MA22098 dated 13.10.2022;
 Copy of Public Consultation invitation;

Lighting Impact Assessment ref: PPC-BWB-00-XX-RP-E-0001_LIA dated October 2022;

Local Labour Agreement dated 31.10.22;

Demolition Method Statement /Risk Assessment ref: A2466 dated 31.08.2022;

Outline Fire Safety Strategy rev.02 dated 16.12.2022;

Television and Radio Reception Impact Assessment version 1.2;

Utility Planning Statement ref: 2482-CWC-XX-XX-RP-ME-0001 dated 30th September 2022;

Topographical Survey plan ref: A6622-P-003;

Preliminary Ecological Assessment and Preliminary Roost Assessment Issue 2 23.09.2022;

Archaeological Desk Based Assessment ref: SA/2022/102 Version 1 dated September 2022

Phase 1: Preliminary Risk Assessment ref: CL-602-LKC 21 1679-01 dated August 2022;

Phase 2 Geo-environmental Investigation , Risk Assessment and remediation strategy ref: CL-602-LKC 21 1679-02 dated September 2022;

Received 23rd December 2023

Agent email 6th December 2022 regarding viability ;

Agent email 6th January 2023 regarding viability ;

Flood Risk Assessment and Outline Drainage Strategy ref: FRA 22 1121 v3 dated January 2023 received 19th January 2023;

Viability Assessment Dated December 2022 received 2nd March 2023

Stage 1 Road Safety Audit Response Report Ref: 82328-CUR-XX-XX-T-TP-00004 Revision: P02 Issue Date: 27 February 2023 received 21st February 2023.

Daylight and Sunlight report ref: 2220 dated 09 March 2023; received 15th March 2023

Access; House Type 6A Plan (A662_P_156C); Block 1 Plans (A662_P_200F); Block 1 Plans (A662_P_201F); Block 2, 5 and 7 Plans (A662_P_300B); Block 3 Plans (A662_P_400B); Block 4 Plans (A662_P_500B); Block 6 Plans (A662_P_600B); Block 8 Plans (A662_P_800B);
Received 11th April 2023;

LA90 dated received 11th April 2023;

Access plan ref: A 662 -P- 102 rev.B; received 18th April 2023;

Letter from Arcon Housing Association regarding affordable housing dated 22nd May 2023.

Viability Assessment review received 23rd May 2023.

Victoria Park Environmental Standards Statement Ref: 2482-CWC-XX-XX-RP-0002 datd 31.05.2023;
 Noise Impact Assessment report ref: 30113/NIA1 rev.2 dated 1 June 2023;
 Block plan ref: A 662 -P- 002 rev.H;
 Servicing plan ref: A 662 -P- 101 rev.D;
 Arboricultural Impact Assessment and Method Statement revision D Ref: MG/6765/AIA&AMS/REVD;
 Tree protection plan (sheet 1 of 2) – Demolition Phase 6785 .04 rev.D;
 Tree protection plan (sheet 2 of 2) – Construction Phase 6785 .05 rev.D;
 Landscaping Plan ref: 6765.03 rev.D;
 Transport Statement ref : 82328-CUR-XX-XX-T-TP-00001 Revision: P07 Issue Date: 05 June 2023;
 Interim Travel Plan ref: 82328-CUR-XX-XX-T-TP-00002 Revision: P06 Issue Date: 05 June 2023;
 received 2nd June 2023
 External Works Details (wall and railing design and railing above plinth boundary treatment) plan ref: A662-P-920;
 External Works plan ref: A 662 -P- 105 rev.F;
 Site Plan ref: A 662 -P- 100 rev.V received 22nd June 2023

Reason - To ensure that the development is carried out in accordance with the approved plans, pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

3. Prior to the commencement of the development, and notwithstanding the Construction Methodology and Management Strategy dated October 2022;

a detailed construction management plan outlining working practices during development shall be submitted to and approved in writing by the local planning authority, which for the avoidance of doubt should include;

- o The routing of construction traffic;
- o Detail the vehicular activity associated with the construction including appropriate swept path assessment;
- o Details of the location and arrangements for contractor parking;
- o The identification of the vehicular access points into the site;
- o Identify measures to control dust and mud, including on the surrounding public highway including: details of how the wheels of contractor's vehicles are to be cleaned during the construction period;
- o Sheeting of construction vehicles;
- o The details of an emergency telephone contact number for the site contractor to be displayed in a publicly accessible location;
- o A highway dilapidation survey including photographs and commentary on the condition of carriageway/footways on construction vehicle routes surrounding the site.
- o Identify measures to reduce noise emissions;

Development shall be carried out in accordance with the approved construction management plan.

Reason - To safeguard the amenities of nearby residents and highway safety pursuant to policies SP1, EN9, EN19 and DM1 of the Manchester Core Strategy (2012).

4. Prior to above ground works, samples and specifications of all materials to be used on all external elevations of the development shall be submitted to and approved in writing by the City Council as local planning authority. The development shall be carried out in accordance with the agreed details.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

5. Prior to above ground works, full detailed designs (including specifications) of all on site and off site highway works (including any Traffic Regulation Orders,) and a delivery sequence, shall be submitted to and approved in writing by the City Council as local planning authority. The highway works shall be implemented in accordance with the approved details and approved delivery sequence, and all the agreed works shall be completed prior to the first dwelling unit being occupied.

Reason - In the interests of highway safety, pursuant to policy T1 of the Manchester Core Strategy (2012).

6. The car parking indicated on the approved plans for the dwellings (including apartments), shall be surfaced, demarcated and made available for use prior to the associated dwellings (including apartments), hereby approved being occupied. The car parking shall then be available at all times whilst the dwellings (including apartments), are occupied.

Reason - To ensure that there is adequate car parking for the development proposed when the building is occupied, pursuant to Policies T2, SP1 and DM1 of the Manchester Core Strategy (2012).

7. Prior to above ground works commencing for the development, finalised detailed elevational designs of the proposed cycle stores for the dwellinghouses and the apartments, and provision for the apartments in block 4 shall be submitted to and approved in writing by the City Council as local planning authority. The cycle stores shall be implemented in accordance with the approved details, and all the agreed works shall be completed prior to the proposed accommodation being brought into use, and be retained thereafter.

Reason - To ensure that adequate provision is made for bicycle parking so that persons occupying or visiting the development have a range of options in relation to mode of transport in order to comply with Policies SP1, T1, T2, EN6 and DM1 of the Manchester Core Strategy (2012) and the guidance provided within the National Planning Policy Framework and the Guide to Development in Manchester Supplementary Planning Document and Planning Guidance.

8. a) Before the development hereby approved is first occupied, a Full Travel Plan shall be submitted to and agreed in writing by the City Council as Local Planning Authority. In this condition a Travel Plan means a document which includes:

- i) the measures proposed to be taken to reduce dependency on the private car by those attending or employed in the development
- ii) a commitment to surveying the travel patterns of staff during the first three months of use of the development and thereafter from time to time
- iii) mechanisms for the implementation of the measures to reduce dependency on the private car
- iv) measures for the delivery of specified travel plan services
- v) measures to monitor and review the effectiveness of the Travel Plan in achieving the objective of reducing dependency on the private car

b) Within six months of the first occupation of the development (or an alternative timescale to be agreed in writing with the Local Planning Authority), a revised Travel Plan, which takes into account the information about travel patterns gathered pursuant to item (ii) above, shall be submitted to and approved in writing by the City Council as local planning authority for the development. Any Travel Plan which has been approved by the City Council as local planning authority shall be implemented in full for the development at all times when the development hereby approved is in use.

Reason - To assist promoting the use of sustainable forms of travel to the development, pursuant to policies SP1, T2 and DM1 of the Manchester Core Strategy (2012) and the Guide to Development in Manchester SPD (2007).

9. a) Prior to above ground works, a scheme for electric vehicle charging and any other considerations shall be submitted to and approved in writing by the City Council as local planning authority.

b) Prior to occupation of the development any agreed mitigation measures shall be implemented and verified as such in writing by the City Council as local planning authority and shall remain in situ whilst the use or development is in operation.

Reason - To secure a reduction in air pollution from traffic or other sources in order to protect existing and future residents from air pollution, pursuant to policies EN16, SP1 and DM1 of the Manchester Core Strategy (2012).

10 a) Notwithstanding the depth of fill to the garden areas of apartment block referenced in Phase 1: Preliminary Risk Assessment ref: CL-602-LKC 21 1679-01 dated August 2022; and Phase 2 Geo-environmental Investigation, Risk Assessment and remediation strategy ref: CL-602-LKC 21 1679-02 dated September 2022; finalised details of the depth and type of materials of fill to the garden areas of apartment block, appropriate to enable planting of hedging, trees and shrubs, shall be submitted to and approved in writing by the City Council as local planning authority prior to above ground works. The development shall be implemented in accordance with the approved scheme.

b) Prior to above ground works , details of gas mitigation measures and the plots where these measures would be incorporated , shall be submitted to and approved in writing by the City Council as local planning authority.

The development shall be implemented in accordance with the approved scheme.

c) When the development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the City Council as local planning authority.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Manchester Core Strategy (2012).

11. a) Before the development commences a scheme for acoustically insulating the proposed residential accommodation against noise from Daisy Bank Road and Laindon Road shall be submitted to and approved in writing by the City Council as local planning authority. There may be other actual or potential sources of noise which require consideration on or near the site, including any local commercial/industrial premises.

The potential for overheating shall also be assessed and the noise insulation scheme shall take this into account. The approved noise insulation and ventilation scheme shall be completed before any of the dwelling units are occupied.

Noise survey data shall include measurements taken during a rush-hour period and night time to determine the appropriate sound insulation measures necessary.

b) Prior to first occupation of the residential units, a verification report shall be required to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic consultant's report. The report shall also undertake post completion testing to confirm that the internal noise criteria have been met. Any instances of non-conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the internal noise criteria.

Reason - To secure a reduction in noise from traffic or other sources in order to protect future residents from noise disturbance and to reduce the potential for overheating, pursuant to policy DM1 of the Manchester Core Strategy (2012).

12. a) The development shall be implemented in accordance with the Waste Management Strategy received on 23rd December 2022 and Site Plan ref: A 662 - P- 100 rev.V; Servicing plan ref: A 662 -P- 101 rev.D; Servicing plan shall remain in situ whilst the dwellings and apartments are occupied.
- b) Finalised details of the design of the bin stores submitted to and approved by the local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details.

Reason - In the interests of amenity and public health, pursuant to policies EN19 and DM1 of the Manchester Core Strategy (2012).

13a) Externally mounted ancillary plant, equipment and servicing shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve a rating level of 5dB (LAeq) below the typical background (LA90) level at the nearest noise sensitive location. Prior to commencement of the use hereby approved the scheme shall be submitted to and approved in writing by the City Council as local planning authority in order to secure a reduction in the level of noise emanating from the site. The scheme shall be implemented in full before the use commences or as otherwise agreed in writing by the City Council as local planning authority.

b) Prior to the operation of the scheme a verification report shall be submitted to and approved in writing by the City Council as local planning authority to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic report. The report shall also undertake post completion testing to confirm that the noise criteria have been met. Any instances of non- conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the agreed noise criteria.

Reason – To minimise the impact of the development and to prevent a general increase in pre-existing background noise levels around the site, in order to protect future residents from noise disturbance, pursuant to policy DM1 of the Manchester Core Strategy (2012).

14. The development shall only be carried out in accordance with sections 3 and 4 in the Crime Impact Statement Version ref.2022/0422/CIS/01, Version B dated 25.10.22, and the agreed measures shall be retained and maintained thereafter. The development hereby approved shall not be occupied or used until the Council as local planning authority has acknowledged in writing that it has received written confirmation of a secured by design accreditation for that the development.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012) and to reflect the guidance contained in the National Planning Policy Framework.

15. No development shall take place until surface water drainage works have been implemented in accordance with Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacements national standards and details that have been submitted to and approved in writing by the Local Planning Authority.

Reason - To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution, pursuant to national policies within the NPPF and NPPG and policies EN08 and EN14 in the Manchester Core Strategy (2012).

16. No development hereby permitted shall be occupied until details of the implementation, maintenance and management of the sustainable drainage scheme have been submitted to and approved by the local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details. Those details shall include:

- Verification report providing photographic evidence of construction as per design drawings, which shall include flow control and attenuation components;
- As built construction drawings if different from design construction drawings;
- Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.
- Management and maintenance plan for the culverted ordinary watercourses which shall include confirmation of responsible party.
- Evidence of post-installation survey and post-piling survey to confirm culvert integrity.

Reason- To manage flooding and pollution and to ensure that a managing body is in place for the sustainable drainage system and a maintenance mechanism for the lifetime of the development, pursuant to policies EN8, EN14, EN17 and DM1 of the Manchester Core Strategy (2012).

17. No development works shall take place (other than works to trees) until a programme of archaeological works has been implemented in accordance with a Written Scheme of Investigation (WSI) has been submitted to and approved in writing by the local planning authority. The WSI shall cover the following:

1. A phased programme and methodology of investigation and recording to include:
 - archaeological evaluation trenching;
 - pending the results of the above, a targeted open-area excavation.
2. A programme for post-investigation assessment to include:
 - production of a final report on the results of the investigations and their significance.
3. Deposition of the final report with the Greater Manchester Historic Environment Record.
4. Dissemination of the results of the archaeological investigations commensurate with their significance.
5. Provision for archive deposition of the report and records of the site investigation.
6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason- In accordance with NPPF policy 16, paragraph 205: To record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) and to make this evidence (and any archive generated) publicly accessible, and pursuant to policies EN3 and DM1 of the Manchester Core Strategy (2012).

18. The development shall be implemented in accordance with the Lighting Impact Assessment ref: PPC-BWB-00-XX-RP-E-0001_LIA dated October 2022.

Reason - To ensure adequate lighting within the development and to ensure full accessibility within the public realm works, and would not have an adverse impact on protected species pursuant to policies SP1 and policy DM1 of the Manchester Core Strategy (2012).

19. If, when the lighting units are illuminated, they cause glare or light spillage which is in the opinion of the City Council as Local Planning Authority to the detriment of adjoining and nearby residential properties, such measures as the Council as Local Planning Authority confirm in writing that they consider necessary including baffles and/or cut-offs shall be installed on the lighting units and adjustments shall be made to the angle of the lighting units and the direction of illumination, which shall thereafter be retained in accordance with details which have received the prior written approval of the Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of existing and proposed nearby residential accommodation, pursuant to policies SP1 and policy DM1 of the Manchester Core Strategy (2012).

20. The shower room windows in the western and eastern elevations of the apartment block on plan refs: A662-P-103, Block 1 A662-P- 200F, and Block 1 Plans A662_P_201F, shall be obscurely glazed to a specification of no less than level 5 of the Pilkington Glass Scale or such other alternative equivalent, and maintained in perpetuity.

Reason - In the interest of privacy and to protect the amenity and living conditions of adjacent properties and in accordance with policies EN1, SP1 and DM1 of the Manchester Core Strategy (2012).

21. The windows at first floor level in the rear (eastern) elevation of Block 8 on plan refs: A662-P-800 B, and Block 8 A662-P- 810, shall be obscurely glazed to a specification of no less than level 5 of the Pilkington Glass Scale or such other alternative equivalent, and maintained in perpetuity.

Reason - In the interest of privacy and to protect the amenity and living conditions of adjacent properties and in accordance with policies EN1, SP1 and DM1 of the Manchester Core Strategy (2012).

22. No removal of or works to any trees or shrubs or works to or demolition of buildings or structures that may be used by breeding birds shall take place during the main bird breeding season 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation for active birds' nests immediately before the vegetation is cleared and provided written confirmation

that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority.

Reason - In order to provide protection to nesting birds, pursuant to Policy EN15 of the Manchester Core Strategy (2012).

23. The development hereby permitted shall not be occupied until the bat and bird boxes have been installed accordance with details shown on the submitted plan Landscape Proposals (Drawing number 6765.03 Rev D).

The biodiversity enhancement measures shall be implemented in accordance with the approved scheme, prior to the occupation of the buildings, and retained at all times thereafter.

Reason - To ensure that landscape and ecology management is adequately in place and to ensure the longevity of new planting in different areas of the site and to comply with policy EN15 of the Manchester Core Strategy (2012).

24. a)The landscaping scheme for the development shall be implemented in accordance with Landscape Proposals ref: 6765.03 revD and the External Works plan ref: A 662 -P- 105 rev.F received 22nd June 2023. E

b)The approved landscaping scheme and arrangements for management and maintenance shall be implemented not later than 12 months from the date the buildings are first occupied. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective in the development, another tree or shrub of the same species and size as that originally planted shall be planted at the same place within the development.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Manchester Core Strategy (2012).

25. In this condition "retained tree" means an existing tree, shrub or hedge which is to be as shown as retained on the approved plans Tree protection plan (sheet 1 of 2) – Demolition Phase 6785 .04 rev.D ;Tree protection plan (sheet 2 of 2) – Construction Phase 6785 .05 rev.D received 2nd June 2023, specified in condition no.2 and particulars; and paragraphs (c) and (d) below shall have effect until the expiration of 5 years from the date of the first occupation of the development for its permitted use.

- (a) Finalised details of how the how the foundations will be constructed to prevent damage or loss to the retained trees and their root systems in proximity to the apartment block, block 8 terrace of houses and the detached house adjacent to no.14 Merwood Road . shall be submitted to and approved in writing prior to the implementation of the foundations to those dwellings . The development shall be implemented in accordance with the approved scheme.
- (b) A tree consultant must be present during excavations of the foundations to supervise and ensure the roots are protected. The development shall be

carried out in accordance with the agreed method statement. All tree work should be carried out by a competent contractor in accordance with British Standard BS 3998 "Recommendations for Tree Work".

- (c) No retained tree shall be cut down, uprooted or destroyed, nor shall any retained tree be topped or lopped other than in accordance with the approved plans and particulars, without the written approval of the local planning authority. Any topping or lopping approved shall be carried out in accordance with British Standard 5387 (Trees in relation to construction)
- (d) If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the local planning authority.
- (e) The erection of fencing for the protection of any retained tree shall be undertaken in accordance with the approved plans and particulars before any equipment, machinery or materials are brought on to the site for the purposes of the development, and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the written consent of the local planning authority.

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Manchester Core Strategy (2012).

26. a) The development shall be implemented in full accordance with the measures set out in Chapter 9 of the Victoria Park Environmental Standards Statement Ref: 2482-CWC-XX-XX-RP-0002 datd 31.05.2023 received 2nd June 2023.

b) A post construction review certificate/statement shall be submitted for approval for the development, within a timescale that has been previously agreed in writing for the development , to the City Council as Local Planning Authority.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Manchester Core Strategy (2012) and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

27. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification) no part of the development shall be used for any purpose other than the purpose(s) of Class C3(a) of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended) (or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification).

Reason - In the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1

and H11 of the Manchester Core Strategy (2012) and the guidance contained within the National Planning Policy Framework.

28. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification) the apartments hereby approved shall be used only as private dwellings (which description shall not include serviced apartments/apart hotels or similar uses where sleeping accommodation (with or without other services) is provided by way of trade for money or money's worth and occupied by the same person for less than ninety consecutive nights) and for no other purpose (including any other purpose in Class C3(a) of the Schedule to the Town and Country Planning (Use Classes) Order 1987 as amended by The Town and Country Planning (Use Classes) (Amendment) (England) Order 2010, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification) other than the purpose(s) of Class C3(a). For the avoidance of doubt, this does not preclude two unrelated people (who are not part of a single household or otherwise) sharing a property.

Reason - To safeguard the amenities of the neighbourhood by ensuring that other uses which could cause a loss of amenity such as serviced apartments/apart hotels do not commence without prior approval; to safeguard the character of the area, and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Manchester Core Strategy (2012) and the guidance contained within the National Planning Policy Framework.

29. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no garages, extensions, porches, roof alterations or outbuildings shall be erected onto the dwellings within the development hereby approved other than those expressly authorised by this permission.

Reason - To safeguard the amenities of the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

30. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no windows shall be inserted into the elevations of the dwellinghouses hereby approved other than those shown on the approved drawings specified in condition 2.

Reason - In the interests of residential amenity pursuant to policy SP1 and DM1 of the Manchester Core Strategy (2012).

31. Notwithstanding the Town and Country Planning (General Permitted Development) Order 2015 as amended by the Town and Country Planning (Permitted Development and Miscellaneous Amendments) (England) (Coronavirus) Regulations 2020 or any legislation amending or replacing the same, no further

development in the form of upward extensions to the buildings shall be undertaken save where it is authorised by the granting of planning permission.

Reason - In the interests of protecting residential amenity and visual amenity of the area in which the development is located pursuant to policies DM1 and SP1 of the Manchester Core Strategy (2012).

32. a) The wall and railing boundary treatment to the Daisy Bank Road Road frontage, and the railing above plinth design treatment to the front garden of Block nos . 5,6, 7 and 8, shall be implemented in accordance with External Works details plan ref: A662-P-920 . Finalised details of the railings shall be submitted approved in writing by the City Council as local planning authority, prior to implementation.

b) Notwithstanding the boundary treatment details annotated on the External Works plan ref: A 662 -P- 105 rev.F received 22nd June 2023, ; finalised details of the boundary treatment to rear gardens and the eastern and southern boundary of the site , including finalised location and elevational designs shall be submitted approved in writing by the City Council as local planning authority, prior to implementation.

The approved scheme shall be implemented in full before the dwellings are first occupied and retained as such thereafter.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located in order to comply with Policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012), and the guidance provided within the Guide to Development in Manchester Supplementary Planning Document and Planning Guidance.

33.a) Prior to above ground works , details of a Local Benefit Proposal, in order to demonstrate commitment to recruit local labour for the duration of the construction of the development, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved document shall be implemented as part of the construction of the development.

In this condition a Local Benefit Proposal means a document which includes:

- i) the measures proposed to recruit local people including apprenticeships
- ii) mechanisms for the implementation and delivery of the Local Benefit Proposal
- iii) measures to monitor and review the effectiveness of the Local Benefit Proposal in achieving the objective of recruiting and supporting local labour objectives

(b) Within one month prior to construction work being completed, a detailed report which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority.

Reason - The applicant has demonstrated a commitment to recruiting local labour pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy (2012).

Informatives

1. The applicant is advised that any requirements for licensing, hoarding / scaffolding, building maintenance and any associated temporary traffic management arrangements will need discussion and agreement with the council's Highways Applications and Network Resilience teams via Contact Manchester (Tel. 0161 234 5004).

The following residents, businesses and other third parties in the area were consulted/notified on the application:

**Highway Services
Environmental Health
Neighbourhood Team Leader (Arboriculture)
MCC Flood Risk Management
Greater Manchester Police
Greater Manchester Ecology Unit**

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties

Relevant Contact Officer :	Sue Wills
Telephone number :	0161 234 4524
Email :	sue.wills@manchester.gov.uk



Application Number	Date of Appln	Committee Date	Ward
136541/FH/2023	7 April 2023	6 July 2023	Chorlton Park Ward

Proposal Installation of a rear dormer together with 4 x no. roof lights to front elevation and replacement glazing to roof of existing single storey side and rear extension.

Location 35 Whalley Avenue, Chorlton, Manchester, M21 8TU

Applicant Mr Gareth & Mrs Sophie Toms

Agent Mrs Paula Butterfield Groves, Butterfield Architecture Ltd

Executive Summary

The applicant is seeking permission for the installation of a rear dormer together with four rooflights to the front elevation and replacement glazing to the roof of the existing single storey side and rear extension to provide additional living accommodation for a family dwellinghouse. The property is not listed, nor is it located within a conservation area.

Five neighbouring dwellings were notified of the proposed development and eight letters of objection were received. The key issues that were raised were concerns relating to the proposal's impact upon visual and residential amenity. These are fully considered within the main body of the report.

Description

The application site is located along a residential street consisting of terraced dwellinghouses. Whalley Avenue is located off Sandy Lane and is bordered to the south by Cleveleys Avenue Allotments. Situated within the Chorlton Park ward of Manchester, the property benefits from its proximity to Chorlton District Centre which possesses an abundance of bars, restaurants, and shopping facilities, as well as outdoor green spaces.

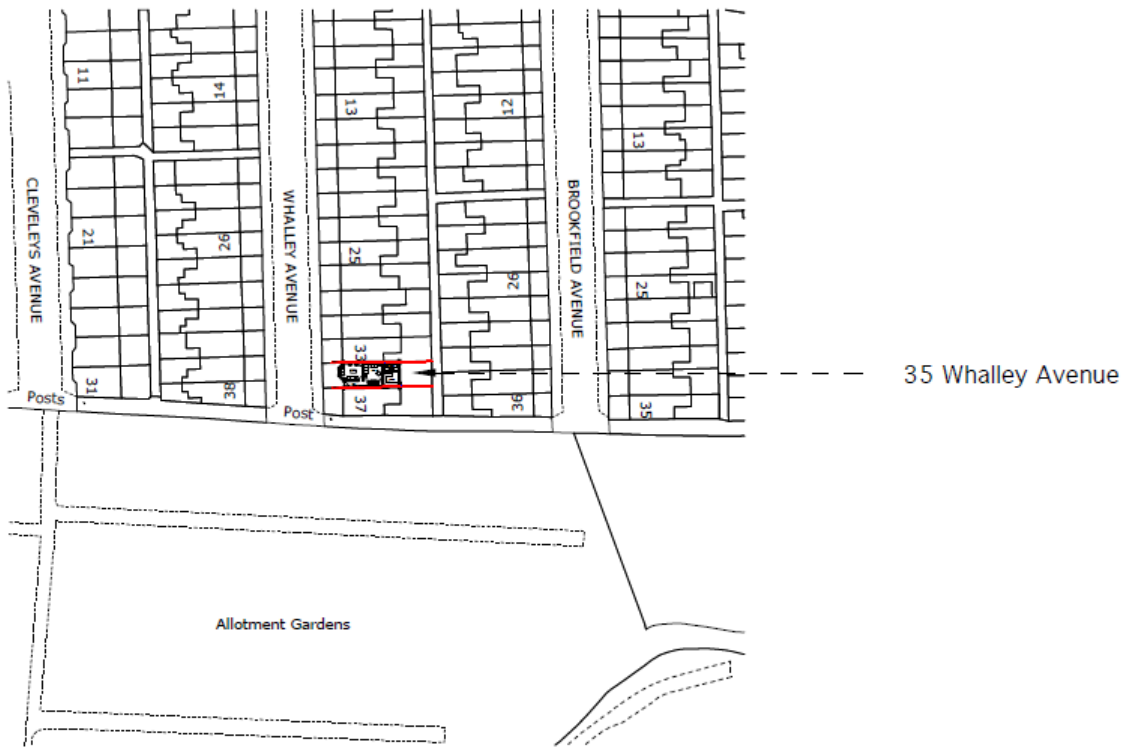


Figure 1. Submitted location plan with site edged in red.

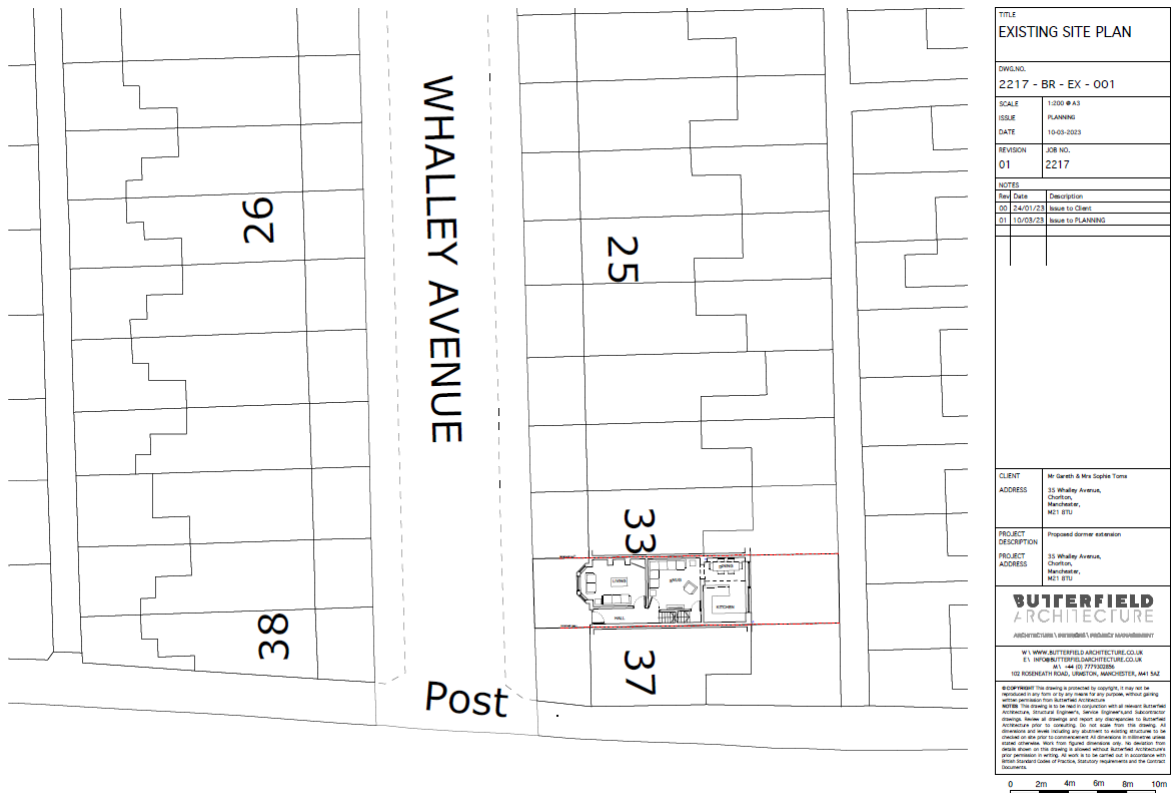


Figure 2. Submitted plan of the proposed site of development, edged in red.

The application relates to a two-storey mid-terraced C3 dwellinghouse of redbrick with a slate tile dual pitch roof. The property is painted white at ground floor level on its principal elevation and features white k-rend and timber panelling to its rear. The property also features white uPVC windows to the front, including a ground-floor bay, black aluminium windows to the rear, a front porch canopy, a modest front garden, a two-storey rear outrigger, a single storey side and rear extension and a paved rear garden / yard.

Both the front and rear gardens feature boundary treatment with the front garden being bordered by a low-level redbrick wall, gateposts and black iron gate and the rear garden being bordered by a mid-level redbrick wall to the shared boundaries and a high-level redbrick wall to the rear alleyway. The neighbouring plots on Whalley Avenue are of a similar size and shape as the application site.

The applicant is seeking permission for the installation of a rear dormer together with four rooflights to the front elevation and replacement glazing to the roof of the existing single storey side and rear extension. The works would provide additional living accommodation in the form of a third bedroom, ensuite half-bathroom, storage area and study / fourth bedroom.

Planning History

118793/FH/2018 - Erection of a single-storey rear extension and elevational alterations to rear – **Approved** (14.03.2018).



Figure 3. Principal elevation of property.



Figure 4. Rear view of property.



Figure 5. Rear garden.



Figure 6. Rear roof of property.

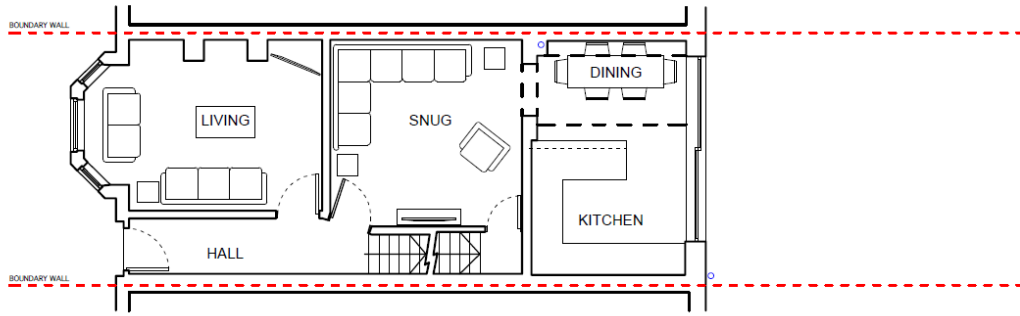
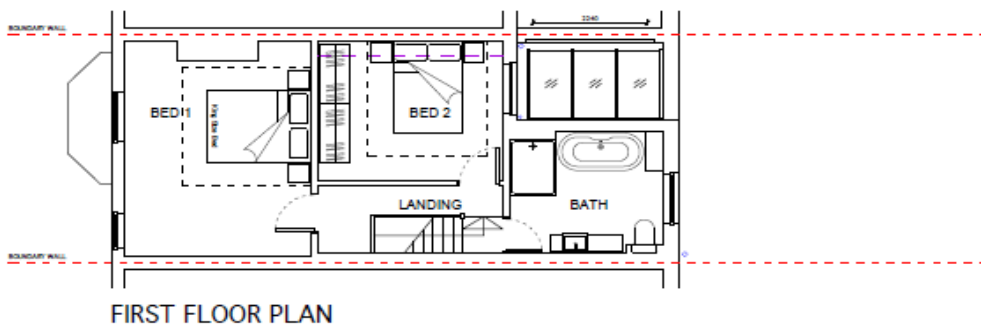
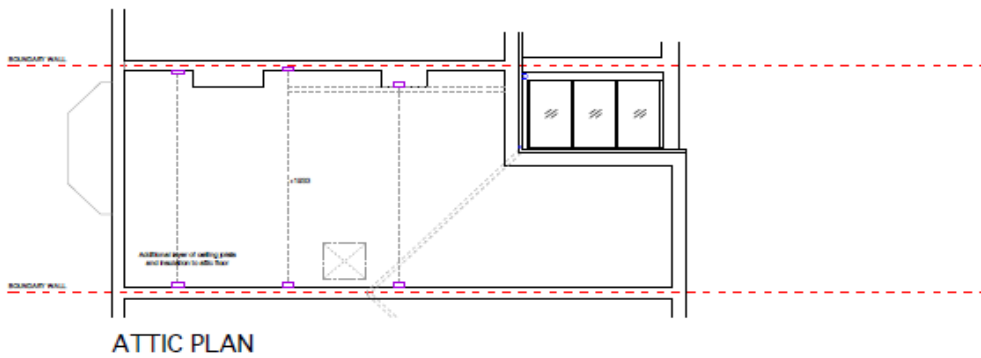


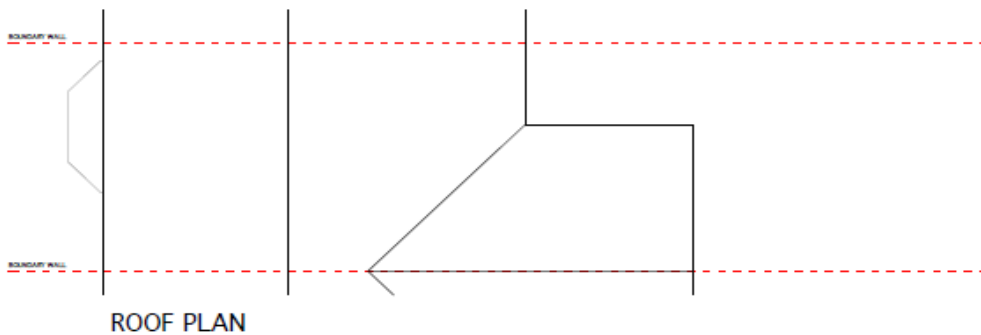
Figure 7. Submitted existing ground floor plan.



FIRST FLOOR PLAN



ATTIC PLAN



ROOF PLAN

Figure 8. Submitted existing first floor, attic, and roof plan.

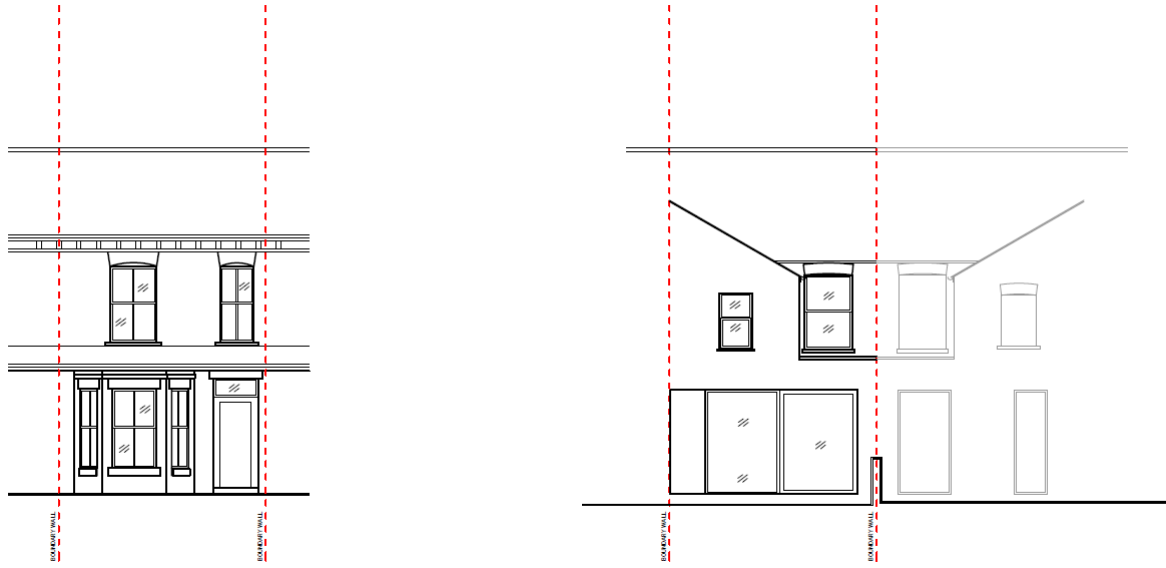


Figure 9. Submitted existing front and rear elevations.

Consultations

Eight letters of objection were received from seven local residents from six different address points. Their concerns are summarised as follows:

- Inappropriate form – a box dormer occupying the main roof slope would be more acceptable than an L-shaped dormer occupying the outrigger roof slope.
- Scale and massing of the dormer would not be suitable for the property or the surrounding street.
- Concerns relating to loss of light (due to the raised outrigger ridge line) which would negatively affect the rear gardens of neighbouring properties, as well as properties located to the immediate north.
- Loss of light would result in increased energy use for those properties affected due to an increased demand for light and heat, negatively impacting finances as well as the environment.
- Privacy issues for the opposing properties along Brookfield Avenue as well as neighbouring terraces.
- Concerns relating to works to the party wall.
- The dormer would not be in-keeping with others present in the area. No other property possesses an L-shaped dormer on Whalley Avenue or Brookfield Avenue.
- The dormer would not preserve or reinforce the local distinctiveness of Whalley Avenue and the surrounding avenues in this area of Chorlton Park.
- The design is not sympathetic to surrounding properties which are of traditional Victorian brick with sash style windows. Its contemporary design would not be in-keeping or appropriate, representing a visually obtrusive addition to the property.
- Would introduce an intrusive roof line, negatively affecting the rear appearance of the properties along Whalley Avenue.
- The dormer would represent a discordant addition to the house and area.

Policies

The Core Strategy Development Plan Document (2012-2027):

The "Core Strategy" was adopted by the City Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) as the document that sets out the long-term strategic planning policies for Manchester's future development. A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents. Relevant policies in the Core Strategy are detailed below:

Policy SP1: Spatial Principles – Development in all parts of the City should make a positive contribution to neighbourhoods of choice including creating well designed places that enhance or create character and protect and enhance the built and natural environment.

Policy DM1: Development Management - This policy states that all development should have regard to the following specific issues for which more detailed guidance may be given within a supplementary planning document: -

- Appropriate siting, layout, scale, form, massing, materials, and detail.
- Impact on the surrounding areas in terms of the design, scale, and appearance of the proposed development. Development should have regard to the character of the surrounding area.
- Effects on amenity, including privacy, light, noise, vibration, air quality, odours, litter, vermin, birds, road safety and traffic generation. This could also include proposals which would be sensitive to existing environmental conditions, such as noise.
- Accessibility: buildings and neighbourhoods fully accessible to disabled people, access to new development by sustainable transport modes.
- Community safety and crime prevention.
- Design for health.
- Adequacy of internal accommodation and external amenity space.
- Refuse storage and collection.
- Vehicular access and car parking.
- Effects relating to biodiversity, landscape, archaeological or built heritage.
- Green Infrastructure including open space, both public and private.
- The use of alternatives to peat-based products in landscaping/gardens within development schemes.
- Flood risk and drainage.
- Existing or proposed hazardous installations.
- Subject to scheme viability, developers will be required to demonstrate that new development incorporates sustainable construction techniques.

Unitary Development Plan for the City of Manchester (1995):

The Unitary Development Plan for the City of Manchester was adopted in 1995 and has largely been replaced with the policies contained within the Core Strategy. However, there are a number of policies that are extant and are relevant to consideration to the proposed extension to a residential dwellinghouse.

Policy DC1 of the Unitary Development Plan seeks to accommodate the demand for more living space, while at the same time ensuring that the amenities of neighbours are protected, and that the overall character of the surrounding area is not harmed. It relates specifically to residential extensions and the relevant criteria from this policy include:

DC1.1 The Council will have regard to:

- a. The general character of the property
- b. The effect upon the amenity of neighbouring occupiers
- c. The overall appearance of the proposal in the street scene;
- d. The effect of the loss of any on-site car-parking

DC1.2 states extensions will be allowed subject to:

- a. They are not excessively large or bulky (for example, resulting in structures which are not subservient to original houses or project out too far in front of the original buildings)
- b. They do not create a loss of sunlight/daylight or privacy
- c. They are not out of character with the style of development in the area
- d. They would not result in the loss of off-street parking Policy

DC1.3 states that Notwithstanding the generality of the above policies, the Council will not normally approve:

- a. rearward extensions greater than 3.65m (12 ft) in length;
- b. 2-storey extensions with a flat roof, particularly those which would be visible from the public highway;
- c. 2-storey extensions to terraced properties which occupy the full width of the house;
- d. flat roofed extensions to bungalows;
- e. extensions which conflict with the Council's guidelines on privacy distances (which are published as supplementary guidance).

DC1.4 In considering proposals for 2-storey side extensions, the Council will have regard to the general guidance above and also to supplementary guidance to be issued. In particular, the Council will seek to ensure that:

- a. the development potential of the gap between detached and semi-detached houses is capable of being shared equally by the owners or occupiers of the two properties concerned;
- b. the actual or potential result of building the extension will not be the creation of a terracing effect, where this would be unsympathetic to the character of the street as a whole;
- c. the actual or potential result of building the extension will not be the creation of a very narrow gap between the properties, or any other unsatisfactory visual relationships between elements of the buildings involved.

As a guide, and without prejudice to the generality of this policy, the Council will normally permit 2-storey house extensions which, when built, would leave a minimum of 1.52m (5 ft) between the side wall and the common boundary, and which meet the other requirements of this policy. Proposals which cannot meet these requirements will be judged on their merits, but with weight being given to (a) and (c) above.

DC1.5 The Council will consider on their merits exemptions to the above policies in

the case of applications from disabled people who may require adaptations to their homes.

Guide to Development In Manchester:

The Guide aims to support and enhance the on-going shaping of the City by providing a set of reasoned principles which will guide developers, designers and residents to the sort of development appropriate to Manchester. It seeks to retain the essential distinctiveness of its character areas, whilst not precluding new development.

National Planning Policy Framework (2021):

The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England and how these should be applied. The NPPF was updated in July 2021 and provides a framework within which locally prepared plans for housing and other development can be produced. Planning law requires that applications for planning permission be determined in accordance with the development plan, i.e., the Core Strategy Development Plan Document and accompanying policies, unless material considerations indicate otherwise. The NPPF is a material consideration in planning decisions.

Paragraph 11 states that plans and decisions should apply a presumption in favour of sustainable development which for decision-taking this means:

- approving development proposals that accord with an up-to-date development plan without delay; or
- where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting permission unless:
 - i. the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or
 - ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

Other Legislative requirements:

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Issues

Policies SP1 and DM1 of the Manchester Core Strategy seek to ensure that new development enhances or creates character, protects, and enhances the built environment; and that the design, scale, and appearance of the proposed development is appropriate to its context. Policies DC1.1, DC1.2, DC1.3 and DC1.4 of the Unitary Development Plan for the City of Manchester relate specifically to residential extensions and set out a number of criteria against which proposals for extensions will be assessed. Although these latter policies are now of some age, they are consistent with the guidance in the National Planning Policy Framework which seeks a high standard of design in new developments to ensure a good standard of amenity for all existing and future occupiers.

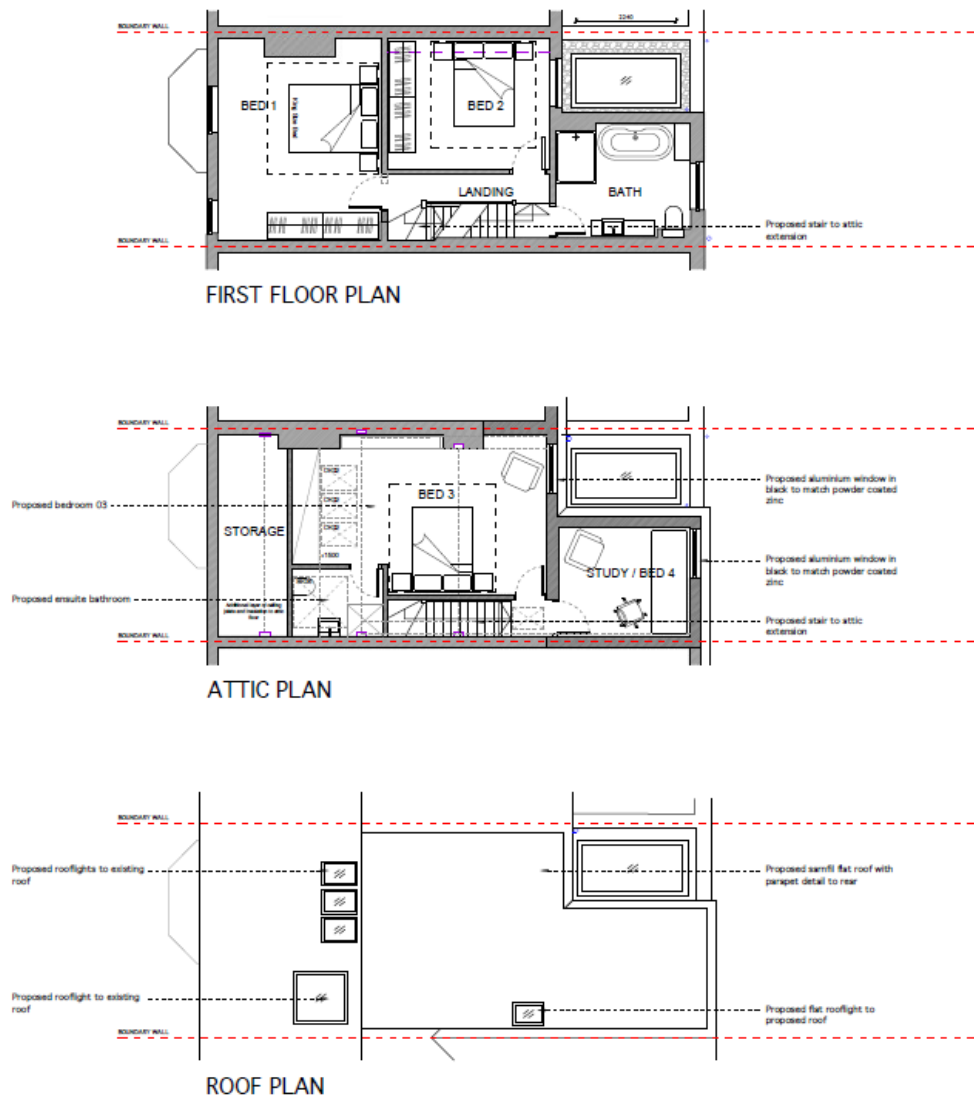


Figure 10. Submitted proposed first floor, attic and roof plan.

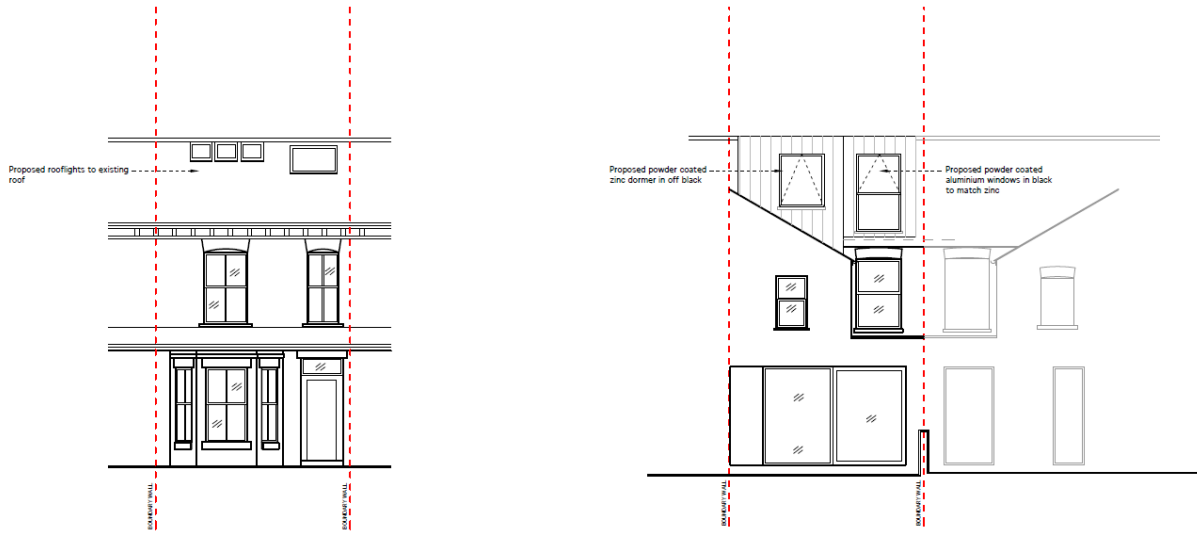


Figure 11. Submitted proposed front and rear elevations.

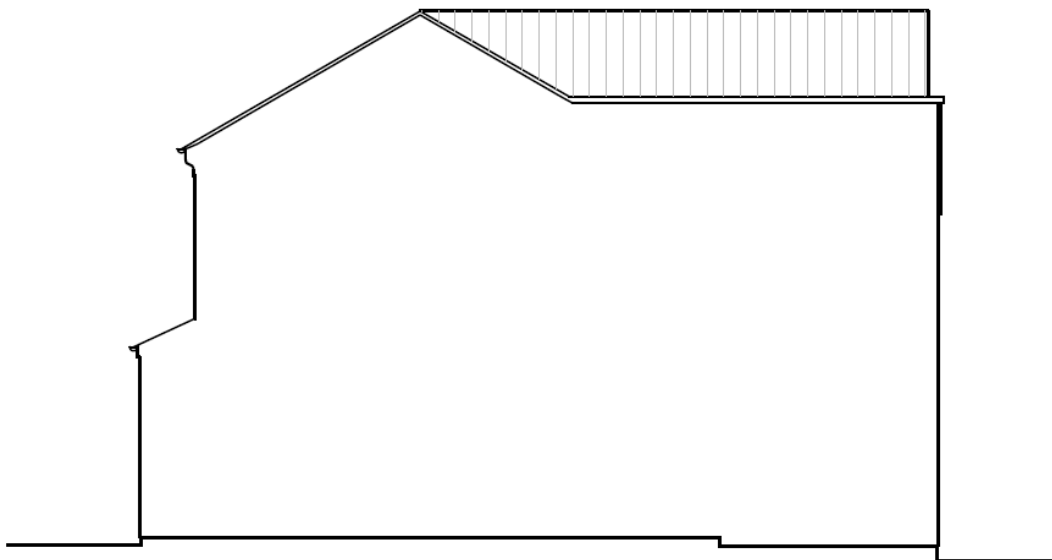


Figure 12. Submitted proposed south side elevation.

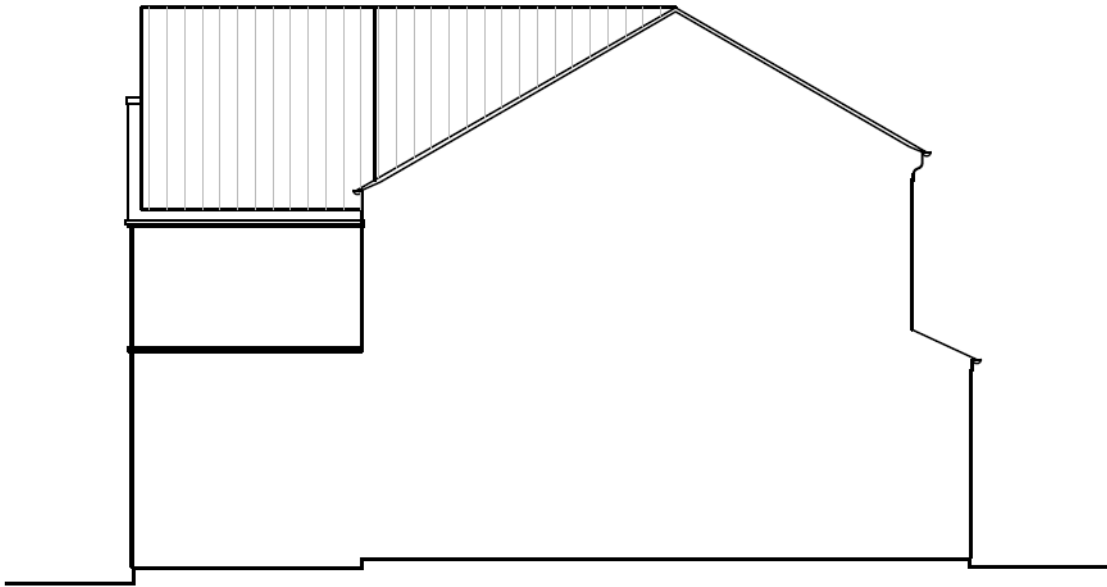


Figure 13. Submitted proposed north side elevation.

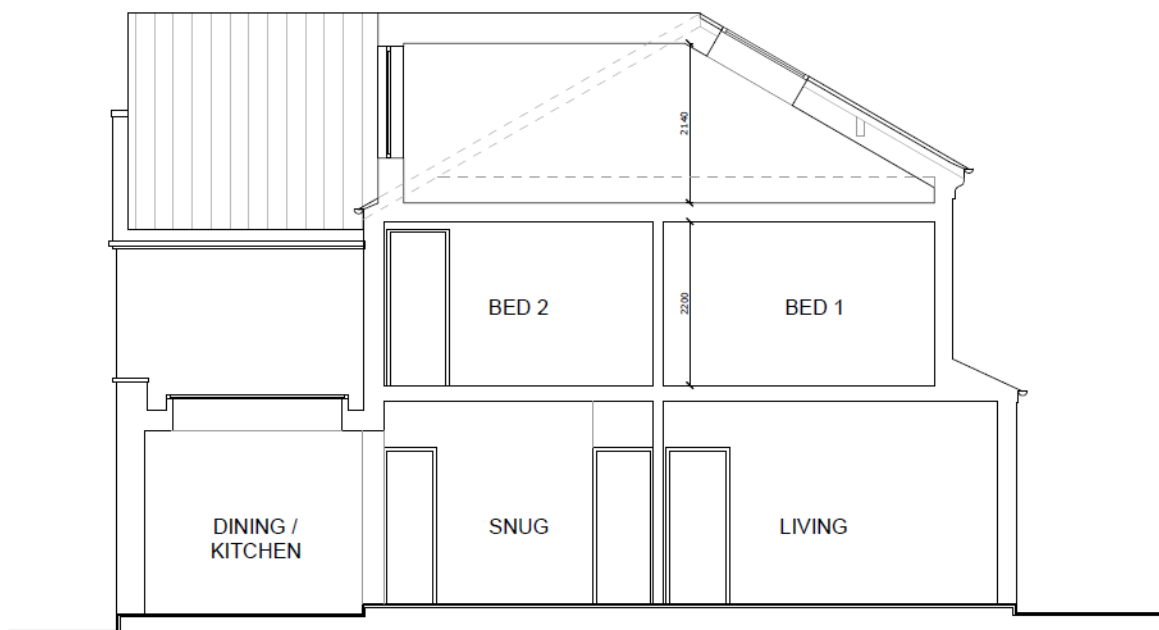


Figure 14. Submitted proposed section.

Principle – the principle of installing a rear dormer together with four rooflights to the property’s front elevation and replacement glazing to the roof of the existing single storey side and rear extension in order to provide additional living space for a family dwellinghouse is generally acceptable given that many individuals prefer to extend and alter their homes to meet changing household needs rather than move. However, consideration must be given to the proposal’s siting, scale and massing,

appearance and impact upon visual amenity, residential amenity, waste storage and parking.

Siting, scale, and massing – the L-shaped dormer would be sited within the main rear slope and outrigger slope of the property, providing an additional roof volume of approximately 25.97 cubic metres. Within the main roof slope, the dormer would be set at the same height as the existing ridge line, projecting 4.328m rearwards, with a width of 4.366m, height of 2.481m and would leave a gap of approximately 0.346m to the eaves. The portion of the dormer which would extend onto the property's outrigger roof slope would project 7.66m rearwards, with a width of 2.589m and height of 2.9m. This portion of the dormer would also be set at the same height as the ridge line of the main roof, however, would extend 1.288m above the ridge line of the outrigger roof. A gap of approximately 0.315m would be maintained to the eaves. The proposed dormer would also leave a gap of 0.216m to the boundary shared with No.33, a gap of 0.194m to the boundary shared with No.37 and would be set in 0.196m from the edge of the outrigger.

Local residents raised concerns in relation to the dormer's 'inappropriate' siting, scale, and massing. However, a dormer window of the same size could be erected under permitted development rights, with the choice of materials being the only issue which means that planning permission is required. This form of roof intervention is commonplace at the rear of houses with other smaller dormer window extensions within this row of properties. The scale and mass is considered to be acceptable.

Appearance and visual amenity – the property is not located within a conservation area, nor is it a listed building. The street on which it sits is comprised of opposing rows of terraced properties which present a relatively uniform appearance in terms of their street-facing elevation and possess a variety of more individualised, heterogenous residential extensions and additions to the rear. It should also be noted that there is a particular precedent for rear dormers along this street as well as the surrounding terraced streets including: Cleveleys Avenue, Brookfield Avenue, Beechwood Avenue, and Limley Grove.

The proposed rear dormer would feature a sarnfil flat roof with parapet detailing to the rear and one flat skylight and would be faced with powder coated zinc cladding in off-black with black aluminium framed windows (to match those already existing on the rear elevation). The side elevations of the dormer are to be blank and featureless, whilst its rear elevation would feature two windows.

A number of local residents raised concerns relating to the dormer's proposed materials, suggesting that the zinc cladding would not be sympathetic to the appearance of the host dwellinghouse or its surrounding properties. These residents felt that the design of the dormer would not be in-keeping with other dormers present in this locale, which are predominantly box-shaped, rather than L-shaped, and suggested that the proposed dormer, by reason of its 'intrusive' roof line would represent a discordant, visually obtrusive addition to the property which would negatively affect the visual amenity and character of the area

However, it is the Council's view that the design of the dormer is relatively standard for developments of this type as many dormers utilise contemporary materials in

order to modernise the appearance of a property. It should also be noted that the rear elevation of the host dwellinghouse already presents a more modern appearance through the implementation of planning permission 118793/FH/2018. The contemporary design of the proposed dormer would therefore be in-keeping with the current appearance of the rear of the property.

Moreover, though the properties along Whalley Avenue currently possess box-shaped dormers rather than L-shaped dormers, comparable dormers do exist elsewhere in this locality, for example those present at No.12 Beechwood Avenue, No.22 Beechwood Avenue, No.9 Brookfield Avenue, No.42 St Annes Road, No.45 St Annes Road and No.14 Limley Grove, as well as a number of L-shaped dormer approvals under planning applications 106721/LP/2014/S1, 114838/FH/2016, 131529/FH/2021, including one at No.24 Whalley Avenue under 132092/FH/2021. Therefore, the proposed would not be out-of-keeping.

Lastly, though the off-black zinc cladding would not match the existing slate roof of the property, it is considered acceptable as the dormer would be sited at the rear of the property and, as such, would not be readily visible within the street scene or public realm, other than at specific viewpoints within Cleveleys Avenue Allotments. The overall character of the area should therefore not be significantly harmed by these works. It should also be noted that the application site is not situated within a conservation area, nor is it a listed building.

The design and appearance of the proposed development therefore accords with Core Strategy policies DM1 and EN1 and chapter 12 of the NPPF as its impact upon visual amenity is considered to be minor.

Residential amenity – due to the siting, scale, and orientation of the proposal as well as the existing precedent of similar works in this area, it is not considered that the proposed dormer would cause undue harm to residential amenity in terms of loss of light, overshadowing, overlooking or loss of privacy, particularly given that the only windows are to be rear-facing.

A number of local residents raised concerns relating to the rear dormer's windows and their potential to overlook neighbouring gardens and properties, thus infringing upon their privacy. However, it is considered that, due to their setback from the eaves, these windows would provide no further degree of overlooking than any existing rear windows at first floor level and, as such, should not compromise the privacy of neighbouring occupants to the degree that would warrant refusal of this application.

Local residents also raised concerns relating to the dormer's potential to cause an undue loss of light for neighbouring residents and the resulting financial and environmental impact such a loss would cause. In terms of loss of daylight and overshadowing, it is understood that such affects would be limited given the orientation of the property and the proposal's siting on the roof (at the same height as the existing ridge line of the main roof). Thus, whilst there may be some overshadowing and loss of daylight at certain times of the day, this is not considered to be significant enough to warrant the refusal of this application, particularly given

the fallback permitted development position whereby a dormer window extension with the same dimensions could be erected under permitted development rights.

Waste storage – the current waste storage arrangements would be unaffected by this proposal.

Parking – the current parking arrangements would be unaffected by this proposal.

Other matters – the proposal also includes the installation of four rooflights to the front facing roof of the property. This alteration is considered to be permitted development not requiring planning permission and, as such, has not been considered in the assessment of this proposal.

The proposal also includes replacement glazing to the roof of the existing single storey side and rear extension (approved under 118793/FH/2018). This alteration is considered acceptable given its minimal impact upon visual and residential amenity as well as its similarities to the current roof of the extension.

A local resident raised concerns relating to the proposal's potential to affect the party wall. It should be noted that any potential damage caused would be regarded as a civil legal matter governed by separate legislation and would not represent a material planning consideration, which would warrant the refusal of this application.

Conclusion – given the above issues, it is considered that the proposed dormer and additional alterations are acceptable in terms of their principle, siting, scale and massing, appearance, and impact upon existing levels of visual and residential amenity as well as waste storage and parking as they broadly comply with policy DC1 of the UDP, policies DM1, SP1, EN1 and EN19 of the Core Strategy and chapter 12 of the NPPF. This is furthermore supported by the proposal being near compliant with permitted development regulations.

Other Legislative Requirements

Equality Act 2010

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land, and business assets. In taking account of all material

considerations, including Council policy as set out in the Core Strategy and saved polices of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation: Approve

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. No significant problems have arisen during the consideration of this application and the application has been determined in a timely manner, in accordance with the policies with the Development Plan.

Condition(s) to be attached to decision for approval OR Reasons for recommendation to refuse

1) The development must be begun not later than the expiration of three years beginning with the date of permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Act Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the documents labelled:

- 'PROPOSED FIRST FLOOR, ATTIC & ROOF PLAN', numbered '2217-BR-PR-101 REV 02', stamped as received by the City Council as Local Planning Authority on the 22nd March 2023.

- 'PROPOSED SECTION A-A', numbered '2217-BR-PR-300 REV 02', stamped as received by the City Council as Local Planning Authority on the 22nd March 2023.

- 'PROPOSED FRONT AND REAR ELEVATIONS', numbered '2217-BR-PR-200 REV 03', stamped as received by the City Council as Local Planning Authority on the 7th April 2023.

- 'PROPOSED SOUTH ELEVATION', numbered '2217-BR-PR-201 REV 03', stamped as received by the City Council as Local Planning Authority on the 7th April 2023.

- 'PROPOSED NORTH ELEVATION', numbered '2217-BR-PR-202 REV 03', stamped as received by the City Council as Local Planning Authority on the 7th April 2023.

Reason - To ensure the development is carried out in accordance with the approved plans, pursuant to Policies SP1 and DM1 of the Core Strategy.

3) The materials to be used on the external surfaces of the development hereby permitted shall match those specified on the submitted application form and approved drawings.

Reason - To ensure the appearance of the building to be developed is not adversely affected by the materials to be used in the construction of the development, pursuant to saved policies DC1.1, DC1.2 and DC1.4 of the Unitary Development Plan for the City of Manchester and policy DM1 of the Manchester Core Strategy.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 136541/FH/2023 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

37 Whalley Avenue, Chorlton, Manchester, M21 8TU
33 Whalley Avenue, Chorlton, Manchester, M21 8TU
36 Brookfield Avenue, Manchester, M21 8TX
34 Brookfield Avenue, Manchester, M21 8TX
32 Brookfield Avenue, Manchester, M21 8TX

A map showing the neighbours notified of the application is attached at the end of the report.

Relevant Contact Officer : Holly Wright
Telephone number : 0161 219 6381
Email : holly.wright@manchester.gov.uk



Application Number	Date of Appln	Committee Date	Ward
134707/FH/2022	15 May 2023	6 July 2023	Withington Ward

Proposal Erection of a two-storey rear extension to provide additional living accommodation.

Location 1D Cotton Lane, Manchester, M20 4GL

Applicant Mrs Mary Adey, 12A Carwood Avenue, Stockport, SK7 2PX

Agent Mr Brendan Cunningham, CPC Ltd, 823 Wilmslow Road, Manchester, M20 2SN

Executive Summary

The proposal relates to the erection of a two-storey rear extension to provide additional living accommodation.

The application site is a semi-detached dwelling house in use as a House of Multiple Occupancy (HMO) with 5no. bedrooms. The HMO was registered in 2019 and has been in use as a lawful HMO for at least 10 years.

The proposal adds no additional bedrooms and only increases the size of shared living space and 1no. first floor bedroom.

The proposal was subject to consultation by way of 8no. letters to adjacent properties. 2no. objections from neighbours were received focussed on the perceived anti-social behaviour of the current occupiers.

The proposal also received an objection from 2 Ward Councillors citing complaints made by residents about anti-social behaviour.

It is considered that the principle of allowing residents to extend their homes to provide enlarged or improved accommodation is generally acceptable if there is no overriding, undue impact upon either the character of the property or upon the residential and visual amenity of nearby residential occupiers. These issues are dealt with in detail within the main report,

Description of the site/building

The proposal is related to a semi-detached dwelling on Cotton Lane. The property has hardstanding for vehicle parking to the front and landscaping to the front and rear. The property features an existing attached garage.

It is of note that the property is registered as a House of Multiple Occupancy (HMO) by the applicant.



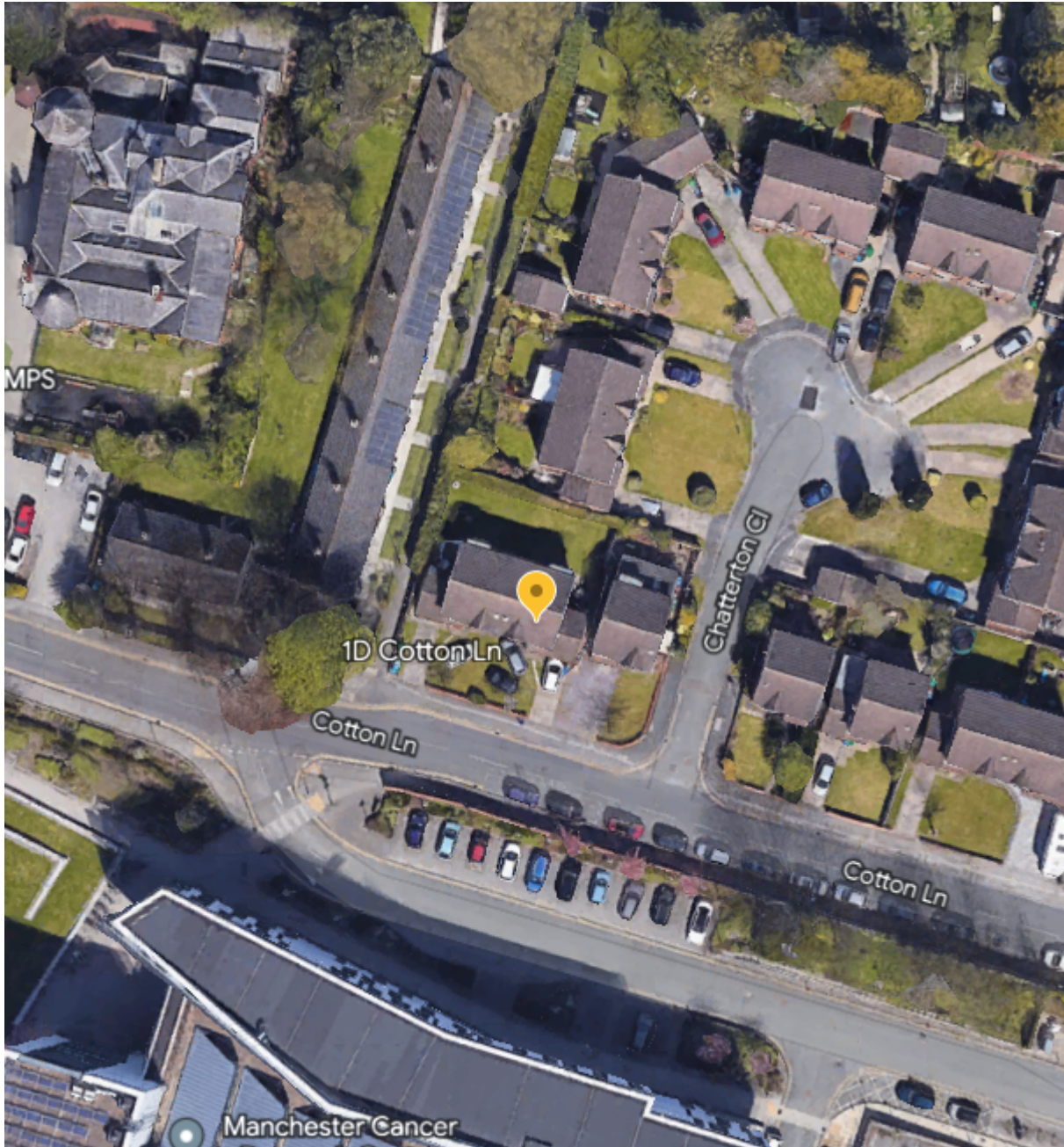
Figure 1. View from Cotton Lane



Figure 2. View from Chatterton Close

Description of area

The property is in a mixed residential/commercial area of the Withington Ward of Manchester. The property sits opposite the Manchester Cancer Research Centre and is close to the nearby Christie Hospital, Manchester Muslim Preparatory School, and local shops on Wilmslow Road.



Description of proposed development

Erection of a two-storey rear extension to provide additional living accommodation in association with existing use of the property as a House in Multiple Occupation (HMO). The applicant has submitted evidence to demonstrate that the lawful use of the property is that of a small-scale HMO incorporating five bedrooms.

Ground Floor

This proposal would project from the rear elevation of the existing dwellinghouse by 3m.

The proposal spans 3.50m in the middle of the rear elevation leaving 3.319m to the boundary with 1C and 1.810m to the detached elevation and adds a total of 9.15m² to the ground floor to create an enlarged kitchen/diner.

The proposal adds a set of French doors on the side elevation of the new extension facing into 1C Cotton Lane and incorporates a sloping tiled roof from the shallower first floor extension.

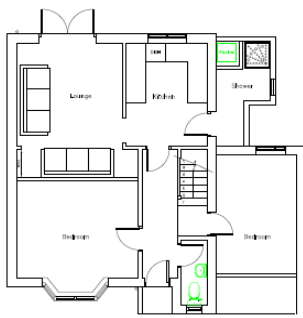
First Floor

The proposal adds 0.450m to the existing bay window extension to form an extension projecting 1.35m from the existing rear elevation.

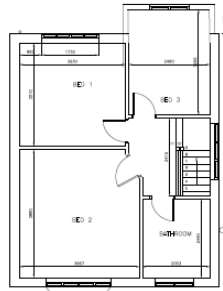
The first-floor proposal spans 3.05m wide from the detached side of the property leaving 3.32m to the boundary of 1C. This is in line with the ground floor extension.

The proposal replaces the current bay window with a new window to the extended rear of the first floor.

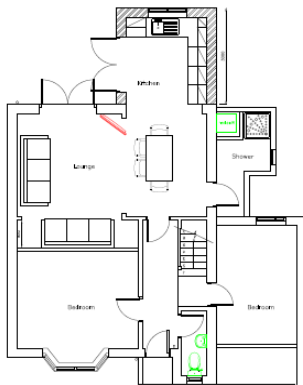
SCALE 1:50 @ A1



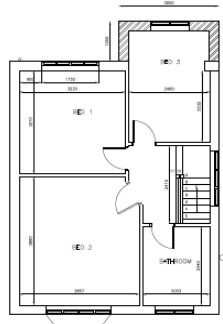
EXISTING GROUND FLOOR PLAN



EXISTING FIRST FLOOR PLAN

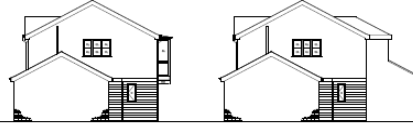


PROPOSED GROUND FLOOR PLAN



PROPOSED FIRST FLOOR PLAN

SCALE 1:100 @ A1



CPC	CPC Limited COMMERCIAL PROPERTY CONSULTANTS LTD. 10 COTTON LANE LONDON E16 1JH	Existing / Proposed Layouts and Elevations 10 Cotton Lane	Date	07/22	Drawing No.	Rev
			Drawn	BC	101	A
			Scale	1/40/00		

Relevant Planning History

078571/FH/2006/S1

Erection of first floor extension to form bay window at the rear

Decision: Approved 23/05/2006

It is of note the adjacent property 1C Cotton Lane is also an HMO. The owner of 1C Cotton Lane applied for the erection of a two-storey rear extension to form additional living accommodation which was refused planning permission on 28/09/2023 (134352/FH/2022) due to the proposed development creating additional bed spaces within an HMO which could potentially increase occupancy with a consequential increase in noise, waste and parking and its effects on the neighbouring residential occupiers.



Figure 3. Existing and Proposed plans for 134352/FH/2022 at 1C Cotton Lane

The applicant appealed to the Planning Inspector (APP/B4215/D/22/3312395) and the application was allowed with the Inspector stating:

“I therefore conclude on the main issue that the proposed extension would not cause harm to the living conditions of neighbours because there would be no increase in bed spaces. Consequently, there would be no conflict with policies SP1 and DM1 of the Manchester Local Development Framework, Core Strategy Development Plan Document (2012) because the proposal would not result in increased occupancy and therefore harm amenity, or have an effect on the wellbeing of residents”

Publicity

Local Residents

Occupiers of properties surrounding the application site were notified of the proposal as required.

Two comments objecting to the application were received from notified neighbours and can be summarised as follows:

1no. comment objecting to the application was received from a neighbour. The grounds for objection focussed on:

- Anti-social behaviour of current tenants including loud parties which take place within the garden and can last all night

- Concern that the proposal will increase the occupancy and result in more noise and disturbance
- General noise and disturbance associated with the existing occupiers and impact from other HMOs in the area
- Impacts on local families and older neighbours
- Disruption from building work

Ward member comments

Cllr. Becky Chambers and Cllr. Chris Wills co-signed an objection which can be summarised as follows –

- Regularly receive complaints about this property and other HMOs within the area relating to noise, poor waste disposal and drug use within the garden which results in odours to neighbouring gardens. The proposal would exacerbate these problems
- An appeal was allowed at 1C Cotton Lane, despite objections, This is an identical application by the same landlord and would lead to overcrowding and problems for the tenants and neighbours.

Site Notice & Press Advertisement

No site notice or advertisement was placed due to the nature and scale of the development. This is in line with other proposals of this type not in a Conservation Area.

Statutory and non-statutory consultees

No statutory and non-statutory consultees were consulted due to the nature and scale of the development. This is in line with other proposals of this type not in a Conservation Area/flood risk zone or with other planning constraints.

Policy

Section 38 (6) of the Town and Country Planning Act 2004 states that applications for development should be determined in accordance with the adopted development plan unless material considerations indicate otherwise. The adopted development plan consists of the Core Strategy (adopted 2012) and the saved policies of the Unitary Development Plan. Due consideration in the determination of the application will also need to be afforded to national policies in the National Planning Policy Framework (NPPF) which represents a significant material consideration.

Core Strategy Development Plan Document 2012 -2027 ("the Core Strategy")

The Core Strategy Development Plan Document 2012 -2027 ("the Core Strategy") was adopted by the City Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) as the document that sets out the long-term strategic planning policies for Manchester's future development. Several UDP policies have been saved until replaced by further development plan documents

to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents.

Relevant policies in the Core Strategy are detailed below

Policy SP 1 Spatial Principles

Spatial Principles

The key spatial principles which will guide the strategic development of Manchester to 2027 are:

- The Regional Centre will be the focus for economic and commercial development, retail, leisure and cultural activity, alongside high-quality city living.
- The growth of Manchester Airport will act as a catalyst for the regional economy and will also provide the impetus for a second hub of economic activity in this part of the City.
- Beyond these areas, the emphasis is on the creation of neighbourhoods of choice, providing high quality and diverse housing around district centres which meet local needs, all in a distinct environment. The majority of new residential development in these neighbourhoods will be in the Inner Areas, defined by the North Manchester, East Manchester and Central Manchester Regeneration Areas.
- The City is covered by regeneration areas including the City Centre. All development should have regard to the character, issues and strategy for each regeneration area as described in the North, East, Central and South Manchester and Wythenshawe Strategic Regeneration Frameworks and the Manchester City Centre Strategic Plan.
- The City's network of open spaces will provide all residents with good access to recreation opportunities. The River Valleys (the Irk, Medlock and Mersey) and City Parks are particularly important, and access to these resources will be improved.
- New development will maximise the potential of the City's transport infrastructure, in particular promoting walking, cycling and use of the public transport. The extension to the Metrolink network through the Oldham and Ashton lines will create key corridors for new development.

Core Development Principles

Development in all parts of the City should: -

- Make a positive contribution to neighbourhoods of choice including: -
 - creating well designed places that enhance or create character.
 - making a positive contribution to the health, safety, and wellbeing of residents
 - considering the needs of all members of the community regardless of age, gender, disability, sexuality, religion, culture, ethnicity, or income.
 - protect and enhance the built and natural environment.

- Minimise emissions, ensure efficient use of natural resources and reuse previously developed land wherever possible.
- Improve access to jobs, services, education and open space by being located to reduce the need to travel and provide good access to sustainable transport provision.

Policy H6 South Manchester

South Manchester is identified as providing 5% of new residential development over the plan period. It identifies that high density development will generally only be appropriate within the district centres of Chorlton, Didsbury, Fallowfield, Levenshulme, and Withington, as part of mixed-use schemes. “Outside of district centres priorities will be for housing which meets identified shortfalls, including family housing and provision that meets the needs of elderly people, with schemes adding to the stock of affordable housing”. The proposals are outside of the district centre, it is considered that as the scheme would deliver affordable housing it is considered to comply with the requirements of H6 and that the proposals would assist in meeting identified shortfalls of housing types within South Manchester. This matter is considered in more detail within the issues section of this report.

Policy EN1 Design Principles and Strategic Character Areas.

Southern Character Area

There is a notable presence of older development (1750-early 1900) at key junctions and along historic radial routes such as Oxford Road/Wilmslow Road, often but not exclusively associated with District Centres. The largely flat terrain has enabled the subsequent infilling of land between these routes with a more regular layout of predominantly residential development up to the 1960s. Much of this subsequent development was associated with the purpose-built radial parkways, serving the City Centre. Princess Parkway now forms the principal road link between the City Centre and the Airport. The area contains pockets of formal open space including some significant historic parks.

- New development needs to retain the identity and focus of activity associated with the historic District Centres.
- Where appropriate development along the radial routes such as Princess Parkway should be commensurate in scale with the prominence of its location.

Policy DM 1 Development Management

This policy sets out the requirements for developments and outlines a range of general issues that all development should have regard to. Of these the following issues are or relevance to this proposal:

- appropriate siting, layout, scale, form, massing, materials, and detail.
- design for health.
- adequacy of internal accommodation and amenity space.
- impact on the surrounding areas in terms of the design, scale and appearance of the proposed development.
- that development should have regard to the character of the surrounding area.

- effects on amenity, including privacy, light, noise, vibration, air quality and road safety and traffic generation.
- accessibility to buildings, neighbourhoods, and sustainable transport modes.
- impact on safety, crime prevention and health; adequacy of internal accommodation
- external amenity space, refuse storage and collection, vehicular access, and car parking; and impact on biodiversity, landscape, archaeological or built heritage, green
- Infrastructure and flood risk and drainage.

The application is considered in detail in relation to policy DM1 within the issues section below.

Manchester Unitary Development Plan (UDP) Saved Policy DC1 – Residential Extensions

Policies DC1.1, DC1.2, DC1.3 and DC1.4 of The Unitary Development Plan for the City of Manchester 1995 (UDP) relate specifically to residential extensions and set out several criteria against which proposals for extensions will be assessed. Although these latter policies are now of some age, they are consistent with the guidance in the National Planning Policy Framework (NPPF) which seeks a high standard of design in new developments which provides a good standard of amenity of all existing and future occupiers.

The National Planning Policy Framework (NPPF)

The National Planning Policy Framework (July 2021) sets out Government planning policies for England and how these are expected to apply. The NPPF seeks to achieve sustainable development and states that sustainable development has an economic, social, and environmental role. The NPPF outlines a “presumption in favour of sustainable development”. This means approving development, without delay, where it accords with the development plan and where a planning application conflicts with an up-to-date development plan (including any neighbourhood plans that form part of the development plan), permission should not usually be granted.

Local planning authorities may take decisions that depart from an up to date development plan, but only if material considerations in a particular case indicate that the plan should not be followed.

The NPPF has been adhered too and considered at all stages of the proposal.

Other Material Considerations

Guide to Development in Manchester Supplementary Planning Document and Planning Guidance (April 2007)

This Supplementary Planning Document supplements guidance within the Adopted Core Strategy with advice on development principles including on design, accessibility, design for health and promotion of a safer environment. The design, scale and siting of the proposed development is considered in more detail within the issues section of this report.

Manchester Residential Quality Guidance (July 2016) (MRQG) –

This document provides specific guidance on what is required to deliver sustainable neighbourhoods of choice where people will want to live and also raise the quality of life across Manchester.

The South Manchester Strategic Regeneration Framework (2007)

The South Manchester SRF was adopted prior to the preparation of the Core Strategy policies, however, it formed an important document in the formulation of the priorities for South Manchester that were subsequently contained in a number of the subsequently adopted policies particularly in relation to housing priorities.

The SRF set out that the key characteristics of South Manchester that shaped the vision and objectives for the SRF are based on a number of key facts one of which relates to the pressure for development and densification which threatens the inherent urban character of the area that makes it attractive in the first place.

The SRF also commented that there had been a trend for large villa/family housing conversions for flats and offices placing a further restriction on the supply of larger accommodation.

One of the key issues identified in the SRF was to provide a wider choice of housing for attracting and retaining residents and that future housing developments need to focus on providing high-quality family accommodation.

Issues

Principle

Policies SP1 and DM1 of the Manchester Core Strategy 2012 (Core Strategy) seek to ensure that new development enhances or creates character, protects, and enhances the built environment; and that the design, scale and appearance of the proposed development is appropriate to its context. Policies DC1.1, DC1.2, DC1.3 and DC1.4 of The Unitary Development Plan for the City of Manchester 1995 (UDP) relate specifically to residential extensions and set out several criteria against which proposals for extensions will be assessed. Although these latter policies are now of some age, they are consistent with the guidance in the National Planning Policy Framework (NPPF) which seeks a high standard of design in new developments which provides a good standard of amenity of all existing and future occupiers.

The principle of allowing residents to extend their homes to provide enlarged or improved accommodation is generally acceptable if there is no overriding, undue impact upon either the character of the property or upon the residential and visual amenity of nearby residential occupiers.

In this instance the principle of the proposal is considered acceptable since this is a lawful HMO, and the proposed extension would not increase occupancy. and that there are no other factors of sufficient weight in amenity terms that would warrant refusal of the application.

Appropriate siting, layout, scale, form, massing, materials, and detail

Erection of a two-storey rear extension to provide additional living accommodation.

The proposal uses brick and tile to match the materials of the existing property

The proposal has been sited and designed to ensure that it is subservient to the original dwelling house, is not excessively large or bulky.

Overall, it is considered that the proposed development has appropriate siting, layout, scale, form, massing, materials, and detail and is therefore in accordance with the development management principle.

Impacts on Privacy and Loss of Light

The proposal includes large, glazed doors on the side elevation facing into 1C Cotton Lane. Given the existing boundary treatment together with the gap to the shared boundary, it is not anticipated that there would be any undue overlooking, or loss of privacy given by the installation of doors.

The proposal includes a small extension to the existing first floor rear bay window including a new window facing rearward towards 1 Chatterton Close. As the existing window already faces the side elevation of the adjacent property the replacement window is unlikely to add any undue harm from overlooking.

The extension would not be visible from the public highway and therefore would not feature within the street scene. Extensions are a common addition to many houses within the area and the proposal is considered be appropriate in terms of its design, bulk and mass and would not form an unduly intrusive feature within the character of the area in general.

On balance, the extension is considered comparable in terms of size and its position to others found in the immediate area, therefore it is felt that the proposal can be accommodated at the application property.

The design of the proposal would be in keeping with the existing and surrounding properties in the area and in terms of the proposed materials it is intended to use matching brick as specified in the application form.

Overall, it is considered that the proposed development has minimal impact on the surrounding areas in terms of the design, scale, and appearance and is therefore in accordance with the development management principle.

The extension is set away from the boundaries with neighbouring houses and gardens and it is considered that there would not be any undue loss of daylight or sunlight to those properties.

Residential Amenity

As discussed above the proposal has minimal effect on privacy given the existing boundary treatment together with the gap to the shared boundary.

No additional bedrooms are being created within the property, with the plans showing an extension to the shared kitchen living space and bedroom 3 only. The applicant has confirmed that the proposal would not increase occupancy but is to improve the existing accommodation. There are five existing bedrooms within the property, and this constitutes a small-scale House in Multiple Occupation. As stated above, the applicant has demonstrated that the use as an HMO has been in operation for over ten years. There is also a license which restricts tenancy to not more than 5 occupants and/or 5 households.

It is of note the adjacent property 1C Cotton Lane is also a HMO. The owner of 1C Cotton Lane applied for the erection of a two-storey rear extension to form additional living accommodation which was refused planning permission on 28/09/2023 (134352/FH/2022) due to the proposed development creating additional bed spaces within a HMO which could potentially increase occupancy with a consequential increase in noise, waste and parking and its effects on the neighbouring residential occupiers.

The applicant appealed to the Planning Inspector (APP/B4215/D/22/3312395) and the application was allowed with the Inspector stating:

“I therefore conclude on the main issue that the proposed extension would not cause harm to the living conditions of neighbours because there would be no increase in bed spaces. Consequently, there would be no conflict with policies SP1 and DM1 of the Manchester Local Development Framework, Core Strategy Development Plan Document (2012) because the proposal would not result in increased occupancy and therefore harm amenity, or have an effect on the wellbeing of residents”

On the basis that the proposal does not propose an increase in occupancy above and beyond the existing with only a small increase in floorspace it is not considered that there would be any additional impacts from comings and goings or with other associated activity, on neighbouring occupiers. Conditions are also recommended to ensure that the number of bedrooms or occupiers is not increased as a result of the grant of planning permission. It is acknowledged that there are existing concerns in relation to the levels of activity associated with the use of the application property and that there are other HMOs within the immediate area. However, as stated above, the proposal would not increase the capacity and therefore there are no unduly unacceptable impacts associated with the proposed development which would warrant a refusal of planning permission.

Refuse storage and collection.

The proposal has no effect on the existing refuse storage and collection at the property.

Vehicular access and car parking

The proposal has no effect on the existing vehicle access and car parking at the property and would not increase occupancy. .

Construction Management

Any impacts associated with construction would be short lived and would not be material considerations which could be used to refuse planning permission. If there are any issues relating to noise or pollution associated with the construction phase, then these would be controlled through separate legislation such as the Environmental Pollution Act.

Flood risk and drainage.

The site does not sit in a flood risk area and therefore no flood statement or assessment was required.

The proposal has a negligible impact on drainage using the existing drainage on site.

Conclusion

The proposal seeks a relatively small extension to the property which doesn't seek to increase the occupancy but improve the standard of living conditions for existing tenants.

It is noted that HMO's are often causes of noise and disturbance to neighbouring residents with the added coming and goings however, in this case, the extension is unlikely to contribute to any increase in noise and disturbance, maintaining the current occupancy level.

The Planning Inspector has already allowed a similar proposal at the adjacent property and although applications stand on their own merit, it is likely a similar opinion would be brought on the proposal.

On balance the proposal accords with all national and local planning policies and guidance.

Recommendation: Approve

Article 35 Declaration

Positive and proactive working - Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. No significant problems have arisen during the consideration of this application.

Other Legislative Requirements

Equality Act 2010

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Human Rights Act 1998 considerations

This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Condition(s) to be attached to decision for approval OR Reasons for recommendation to refuse

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2. The development hereby approved shall be carried out in accordance with the following drawings and documents:
 - a. *Drawing – Location Plan, 'N/A' numbered 102, as received 15/05/2023*
 - b. *Drawing – Existing & Proposed, PLANS AND ELEVATIONS numbered 101 A, as received 15/05/2023*
 - c. *Design & Access Statement, as received 16/08/2022*

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy

3. The materials to be used on the external surfaces hereby permitted shall match those as specified on the application form in type, size, colour and texture.

Reason - To ensure the appearance of the building to be extended is not adversely affected by the materials to be used in the construction of the structure, pursuant to policy DM1 of the Manchester Core Strategy.

4. The extension hereby approved shall be used for an extended bedroom and kitchen area, as shown on the plans *Drawing – Existing & Proposed, PLANS AND ELEVATIONS numbered 101 A*, and the application property shall be restricted to a five-bedroom House in Multiple Occupation with no more than five permanent occupiers at any one time (as stated in the HMO licence 248463/OL2)

Reason – To ensure the residential amenity of the local residents as pursuant to policies SP1 and DM1 of the Core Strategy.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 134707/FH/2022 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

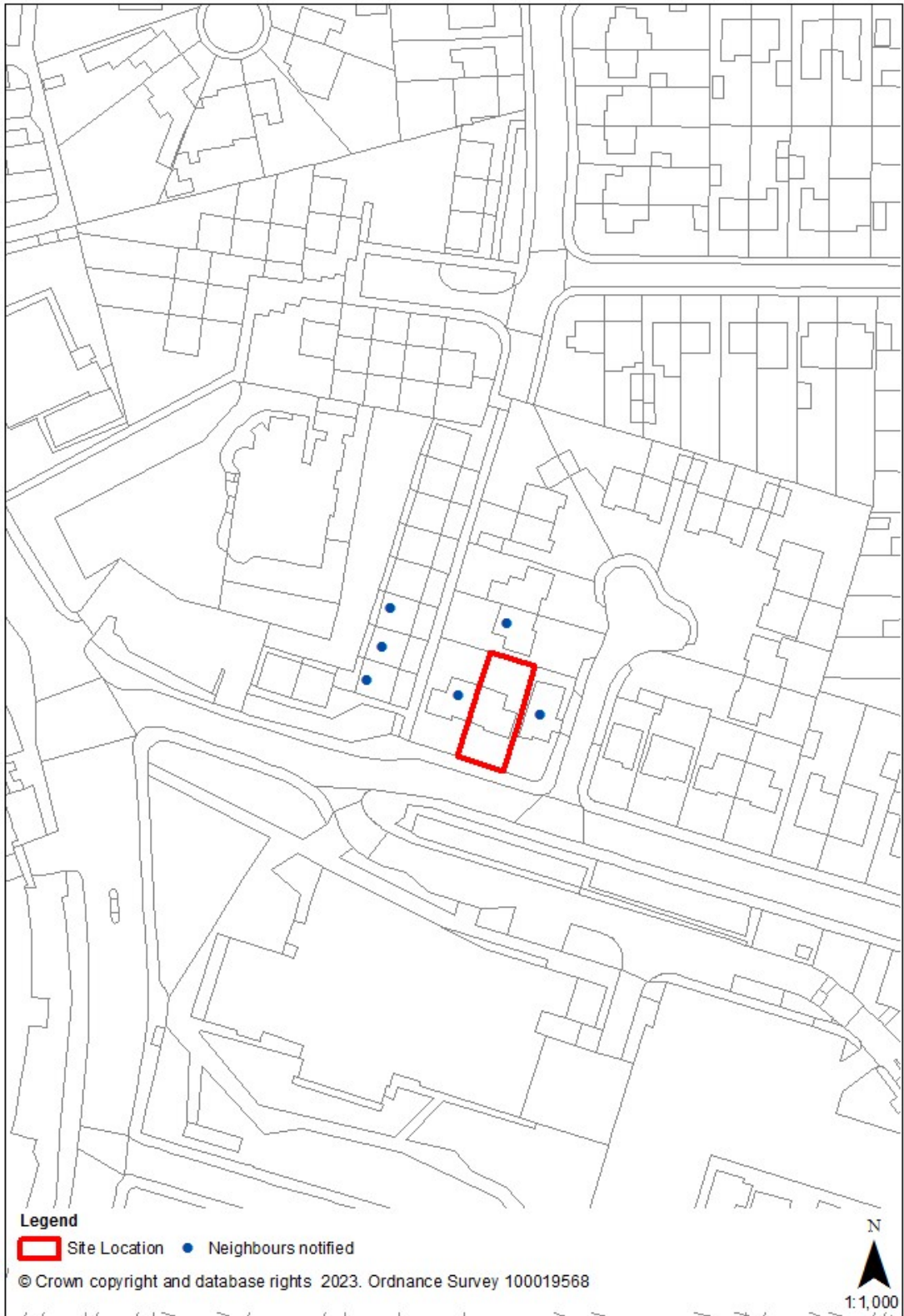
1E Cotton Lane, Manchester, M20 4GL
 1C Cotton Lane, Manchester, M20 4GL
 30 Harcombe Road, Manchester, M20 4AT
 34 Harcombe Road, Manchester, M20 4AT
 32 Harcombe Road, Manchester, M20 4AT
 1 Chatterton Close, Manchester, M20 4HX

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Cllr. Becky Chambers (Withington Ward)
 Cllr. Chris Wills (Withington Ward)

Relevant Contact Officer : Joshua Lee
Telephone number : 0161 219 2851
Email : joshua.lee@manchester.gov.uk



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Application Number	Date of Appln	Committee Date	Ward
136171/FO/2023	17 Feb 2023	6 Jul 2023	Clayton & Openshaw Ward

Proposal Erection of 24 dwellinghouses and cottage flats (Class C3) with associated car parking, landscaping and the creation of a new vehicular access

Location Land Bounded By Brigham Street, Meech Street And The Rear Of Connie Street, Manchester

Applicant Mr Robert Pleasance , One Manchester

Agent Mr Mark Trayhorn, Triangle Architects

Executive summary

Proposal

The proposed development forms part of the first phase of Project 500, which seeks to provide 378 new, low carbon affordable homes on 27 sites owned by the Council across the city. Many of the sites are located in north and east Manchester and new development would respond to high demand for affordable housing. Project 500 is being delivered by registered providers.

This application relates to an area of grassed open space with a broadly rectangular configuration covering an area 0.56 hectares. The application site has been previously developed and is in a residential area. The site is enclosed by a mix of low railings and higher railings to neighbouring alleyways. Two storey terraced housing frontages to Connie Street and Meech Street respectively is located to the west and south of the site. Pockets of open space separate the site from two storey housing to the north. Two storey houses and flats are located to the east of the site along Brigham Street. The site is owned by the Council but the applicant is engaged in discussion to secure its acquisition.

Objections

One objection has been received and raises concerns regarding additional noise and disturbance and traffic generation and additional on-street car parking. There are concerns that the development would undermine the privacy of existing residents and potentially increase the occurrence of anti-social behaviour. There is also concern that ground disturbance during construction may increase the risk of rodent infestation.

Key Issues

The proposed development would be delivered by a social housing provider with a commitment to providing affordable housing. The development would provide a mix

of accommodation that would respond the local need for family type accommodation and individuals requiring smaller properties.

The development would deliver 24 affordable with the following tenures:

- Four, 2 bed apartments for Social Rent (17%)
- Seven, 3 bed 4 person houses for Shared Ownership (29%)
- Three, 4 bed 5 Person houses for Shared Ownership (12%)
- Ten, 2 bed 3 Person houses for Affordable Rent (42%)

The development would involve the loss of open space. However, the site is a Brownfield site being cleared of housing in the past and has been identified for potential residential development. The loss of open space is considered to be justified on the basis of existing local open space provision and the positive benefits of delivering high quality housing.

Description

The proposed development forms part of the first phase Project 500, which seeks to provide 378 new, low carbon affordable homes on 27 sites owned by the Council across the city. Many of the sites are located in north and east Manchester and new development would respond to high demand for affordable housing. Project 500 is being delivered by registered providers.



Fig.1 – Views of the application site from Brigham Street

The application site relates to an area of grassed open space with a broadly rectangular configuration covering an area 0.56 hectares. The application site has been previously developed and is in a residential area. The site is enclosed by a mix

of low railings and higher railings to neighbouring alleyways. Two storey terraced housing with frontages to Connie Street and Meech Street are located to the west and south of the site. Pockets of open space separate the site from two storey housing to the north. Two storey houses and flats are located to the east of the site along Brigham Street. The site has been identified as amenity green space in the Council's open space audit. Limited tree cover is situated along the western and southern perimeters of the site. The site is owned by the Council and the applicant is engaged in discussion to secure its acquisition.

The proposed development would comprise:

- i. The formation of a new access road facilitated through the opening and extension of Stanton Street moving adjacent to the western site boundary and terminating at a turning head at the northern end of the site. Four apartments (in two separate units) and 3 pairs of semi-detached houses would be presented to the access road. Adjacent to the turning head, a pair of semi-detached houses with a staggered configuration would be formed.
- ii. The east of the site, six pairs of semi-detached houses would be formed with frontages to Brigham Street.



Fig.2 - Proposed site layout

iii. A band of open space would bisect the centre of the site running from south to north. This space would be demarcated to form garden areas relating to the respective plots.

iv. The proposed units would be supported with in-curtilage car parking with direct access to the access road and Brigham Street respectively. Shallow garden areas would be formed to individual plots with the street boundaries defined by low walls and boundaries. Garden areas would be defined by close boarded fencing.



Fig. 3 – Street elevation to Brigham Street

v. The proposed units would all comprise of two storeys with pitched roofs set between gable elevations. The elevation would incorporate light brown brickwork with contrasting dental courses and contrasting dark grey vertical cladding panels to plots 13, 15, 23 and 34. Thin profile grey roof tiles would be used throughout the development.

vi. The following house types have been proposed and the achieved internal space has been specified:

- Type A – 10 x two bedroom, three person (70.6 sq. metres)
- Type B – 7 x three bedroom, four person (87.9 sq. metres)
- Type C – 2 x four bedroom, five person (102.4 sq. metres)
- Type C (Variant) 1 x four bedroom, five person (100.2 sq. metres)
- Flat Type D1 – 4 x two bedroom, three person (61.9. sq. metres)

Typical house and apartment types are shown below:



Fig. 4 – Typical house type (Type A)



Fig. 5 - Flat Type D1 and D2

vii. The proposed 24 affordable units would deliver the following tenures:

- Four, 2 bed apartments for Social Rent (17%)
- Seven, 3 bed 4 person houses for Shared Ownership (29%)
- Three, 4 bed 5 Person houses for Shared Ownership (12%)
- Ten, 2 bed 3 Person houses for Affordable Rent (42%)

Consultations

Local residents – One objection has been received and raises concerns regarding additional noise and disturbance and traffic generation and additional on-street car parking. There are concerns that the development would undermine the privacy of existing residents and potential increase the occurrence of anti-social behaviour. There is also concern that ground disturbance during construction may increase the risk of rodent infestation.

Highway Services – The following comments have been received:

- i. The site is in an established residential area and is accessible by bus with services running along Greenside Street. It is not anticipated the development would generate highway safety or network capacity concerns.
- ii. The application has been supported with a transport statement and an assessment of accessibility and trip generation. It has been demonstrated that the development would not generate a significant level of peak-hour vehicle trips. Furthermore, pedestrian, cycle and public transport trips would be accommodated within the existing surrounding infrastructure. The development would be supported with proposed dropped kerbs and tactile paving at the Staton Street, Brigham Street and Meech Street junctions. The assessment demonstrates that the site is located in an accessible location with community facilities and services situated within walking distance.
- iii. The amount of car parking within individual plots is acceptable. It is recommended that individual driveway parking spaces achieve dimensions 3.0 metres x 6.0 metres.
- iv. Each residential unit should include an electric vehicle charging point with a minimum charging a capacity of 7kW) is required.
- v. Clarification is sought regarding the capacity of the proposed sheds to accommodate secured cycle storage for each of the proposed houses and apartments.
- vi. The proposed boundary walls and railings to the street are considered to be acceptable.
- vii. The proposed 2 metre wide footway is acceptable.
- viii. The development should be related to an off-site highways works condition. The identified 2.4 metre x 25 metre visibility splay to the Meech Street and Staton Street junction should be protected by a double-yellow waiting restriction (traffic regulation order) part of the S278 highway works agreement. The full extent of the highways to be adopted has been identified and related works, including dropped kerbs and tactile paving, will also need to be implemented through appropriate highways agreements.
- ix. The refuse vehicle tracking is acceptable.
- x. The development should be related to a construction management plan condition.

Environmental Health – The following recommendations have been received:

- i. The development should be related to a construction management plan.
- ii. The submitted waste and recycling measures should be related to the development by condition.

- iii. To safeguard air quality, a condition should be included to ensure the provision of electric vehicle (EV) charging points.
- iv. The submitted land condition survey has been assessed and details of further ground investigations referenced within it will need to be submitted for consideration. To ensure the comprehensive assessment of ground conditions and the remediation of contamination issues an appropriate condition has been requested.
- v. An acoustic insulation condition is requested to ensure that the development is appropriately attenuated from noise from industrial uses to the west of the application site.

Neighbourhood Team Leader (Arboriculture) – No objection to the removal of trees that have been identified as having limited amenity value. Whilst the site is relatively constrained, it is recommended that appropriate tree species are planted in rear gardens to offset tree loss. This request would be responded to through the recommended landscaping condition.

Greater Manchester Police – No objection subject to the development being implemented in accordance with recommendation contained in the submitted in the crime impact statement and the attainment of Secured by Design accreditation.

Greater Manchester Ecology Unit – The following comments have been received:

- i. The development should be implemented in accordance with the details of the submitted ecology report, including the recommendation for the safeguarding of any protected species and their habitats.
- ii. A condition is requested to ensure trees and shrubs in the site are removed outside the bird nesting season, unless the absence bird habitats within the trees is confirmed by a specialist consultant.
- iii. A plant species, i.e., Cotoneaster, has been identified within the site. This plant and its seed can become potentially invasive if disturbed or inappropriately removed and transportation from the site. To address this issue, a condition has been included to ensure appropriate measures for the mitigation, treatment and removal of any invasive specified identified within the application site.
- iv. Proportionate bio-diversity enhancement measures are recommended in the form of native tree and shrub planting and the provision of bat and bird boxes.

Issues

National Planning Policy Framework (NPPF) - This Framework came into effect on 27th March 2012 and was amended and updated in July 2021. It sets out the Government's planning policies for England and how these are expected to be applied. It defines the Government's requirements for the planning system 'only to the extent that it is relevant, proportionate and necessary to do so'. It provides a mechanism through 'which local people and their accountable councils can produce their own distinctive local and neighbourhood plans, which reflect the needs and priorities of their communities'. The Framework re-iterates that planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The statutory status of the development plan remains as the starting point for decision making. However, paragraph 10 states that 'at the heart of the Framework is a presumption in

favour of sustainable development.' In 'decision-taking', this means that development proposals that accord with the development plan should be approved without delay unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

i. Chapter 5 Delivering a sufficient supply of homes - States that to support the Government's objective of significantly boosting the supply of homes, a sufficient amount and variety of land can come forward where it is needed. It requires that land with permission is developed without unnecessary delay. It also requires that the needs of groups with specific housing requirements should be addressed and this should be reflected in the consideration of the size, type and tenure of housing needed for different groups in the community. Chapter 5 states that the provision of affordable housing should be related to major developments and that the re-use of brownfield sites should be encouraged. The importance of small and medium sized sites to housing requirements is acknowledged along with delivery of development that supports housing for first time buyers. In this case, it is considered that the size the proposed development would positively contribute to the quality and supply of housing in the local area and secure the development of a brownfield site. It would also help to diversify size and character of accommodation in the local housing market and deliver high quality design with capability of achieving sustainable future occupation. The applicant has indicated that the development would deliver a mix of affordable housing. The development would thereby accord with chapter 5.

ii. Chapter 8: Promoting healthy and safe communities - States that planning decisions should aim to achieve healthy, inclusive, accessible and safe places, where crime and disorder (and the fear of crime) do not undermine the quality of life or community cohesion. The development would present habitable room windows to the street, which would reflect the characteristics of neighbouring terraced houses and aid natural surveillance. The development would present opportunities for the incorporation of appropriate security measures and site management.

With regard to open space and recreation, chapter 8 states that existing open space, should not be built on unless:

- a) An assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements.
- b) The loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location.

In this case, the application site relates to a previously developed brownfield site, that has been landscaped but identified for future residential development. The site is located immediately to the north of Openshaw Park, which incorporates a children's play area, multi-use sports area (MUGA), 5-a-side football, football pitch, tennis courts and a teen shelter. It is considered that the loss of the site as open space would be offset by the quality of the facilities available in the neighbouring park. Each property would also incorporate garden areas, which would be beneficial to residential amenity and would provide appropriate setting to the proposed houses and satisfactory private amenity space. The development would thereby accord with chapter 8.

iii. Chapter 9: Promoting sustainable transport - States that in assessing specific applications for development, it should be ensured that:

- a) Appropriate opportunities to promote sustainable transport modes can be - or have been - taken up, given the type of development and its location;
- b) Safe and suitable access to the site can be achieved for all users.

It is considered that, given the magnitude of the development, the generation of traffic and vehicular movement would be predictable and capable of being accommodated within the local highway infrastructure. The development would be supported with off-site car parking and cycle storage. The site is in a sustainable location benefitting from access to the local bus network. The development would thereby accord with chapter 9.

iv. Chapter 11: Making effective use of land - States that planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions. The use of previously developed or 'brownfield' sites is encouraged. Chapter 11 also states that planning decisions should support development that makes efficient use of land, whilst considering:

- a) The identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it.
- b) The importance of securing well-designed, attractive and healthy places.

It is considered that chapter 11 has been positively responded to as the development would secure the re-use of a brownfield site and secures housing with a high quality of urban design and potential for sustained occupation. The scale, proportions and character development would also be appropriately related to the character of existing built form in the surrounding area.

v. Chapter 12: Achieving well-designed places - States that good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. It is considered that the development would accord with the objective of ensuring that the development functions effectively and adds to the overall quality of the area. It would deliver a high quality of architecture and an appropriate layout and landscaped setting. The relationship to the street would establish a sense of space and satisfactory building types. Indicative material specifications give confidence regarding the quality of the appearance of the development with final details to be approved through a related condition.

vi. Chapter 14: Meeting the challenge of climate change, flooding and coastal change - States that new development should:

- a. Avoid increased vulnerability to the range of impacts arising from climate change;
- b. Help to reduce greenhouse gas emissions, such as through its location, orientation and design.

It further states that all plans should apply a sequential, risk-based approach to the location of development - taking into account the current and future impacts of climate change to avoid where possible, flood risk to people and property.

The applicant has provided an environmental assessment that predicts that the development would achieve a 46% reduction in carbon emissions above the requirements of Part L of building regulations. The development has been related to a fabric first approach with U-values, design air permeability and ventilation targets that would potentially exceed building regulations. The above would be supplemented with measures to secure sustainable drainage and design measures to protect existing ecology, enhance biodiversity and provide cycle storage for residents. Air source heat pumps would also be incorporated into the development. These measures are capable of being related to the development by condition to secure compliance with chapter 14.

Planning Practice Guidance (PPG) - On 6 March 2014 the Department for Communities and Local Government (DCLG) launched this planning practice guidance web-based resource. The PPG seeks to both simplify and clarify planning guidance easier and simpler. It is intended to be read in conjunction with the National Planning Policy Framework (NPPF) and is relevant to key planning issues of significance to applicants and local authorities. In the following assessment of the proposed development has been given to the following aspects of the PPG:

- i. Consultation and pre-decision matters - The PPG reasserts that local planning authorities are required to undertake a formal period of public consultation, prior to deciding a planning application. All necessary local consultation has been undertaken, including the posting of site notices and a press advertisement;
- ii. Design - Good quality design is an integral part of sustainable development. It is considered that the development presents a contemporary interpretation of family type housing that balances the functional requirements with a coherent and cohesive design. The above objectives would thereby be accorded with.
- iii. Flood Risk Planning and Flood Risk - The potential for increase flood risk has been appropriately assessed and necessary conditions recommended to secure satisfactory drainage.
- iv. Health and well-being - States those local planning authorities should ensure that health and wellbeing, and health infrastructure are considered in planning decision making. In this case, the development would provide a high quality of design that would positively benefit the appearance of the area.
- v. Air quality - The PPG states the relevance of air quality to a planning decision will be dependent upon the proposed development and its location. Consideration should be given to the likelihood that the development would:
 - a. Significantly affect traffic in the immediate and wider vicinity of development;
 - b. Introduce new point sources of air pollution;
 - c. Expose people to existing sources of air pollutants. This could be by building new homes, workplaces or other development in places with poor air quality;

- d. Give rise to potentially unacceptable impacts during construction for nearby sensitive locations;
- e. Affect biodiversity.

The development would be related to proportionate car parking with impacts offset through the provision of cycle storage and the provision of electric vehicles charging points for each house and apartment. Impacts, including those affecting air quality, would be managed during the construction phase through the construction management plan. Proportionate ecology enhancement would also be secured.

vi. Land affected by contamination - States that the contaminated land regime under Part 2A of the Environmental Protection Act 1990 provides a risk based approach to the identification and remediation of land where contamination poses an unacceptable risk to human health or the environment. The application has been accompanied with a ground condition survey and it is considered that identified land contamination issues will be addressed through details required by condition.

vii. Noise - Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. The PPG sets out the potential approaches to responding to noise and appropriate mitigation, which have been applied in the consideration of the proposed development. The development is located in an established residential area with a commensurate immediate noise climate. However, to address any potential impacts from industrial units to the west of the site an acoustic condition has been recommended.

viii. Travel plans and traffic / transport assessments - The PPG has been related to the traffic and highways issues and potential measures to reduce reliance on private car usage, including cycle storage provision. It is considered that the local highway network can accommodate the additional traffic generated by the development.

Manchester's Local Development Framework: Core Strategy - The Core Strategy Development Plan Document 2012 -2027 ('the Core Strategy') was adopted by the Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) as the document that sets out the long term strategic planning policies for Manchester's future development. A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents.' The following policies are relevant to the proposed development:

Policy SP 1 (Spatial Principles) - Specifies the Core Development Principles for parts of the City. In this case the relevant principles relate to the extent to which the development:

- a. Makes a positive contribution to neighbourhoods of choice including the creation of well designed places that enhance or create character; making a positive

contribution to the health, safety and well-being of residents, considering the needs of all members of the community regardless of age, gender, disability, sexuality, religion, culture, ethnicity or income and to protect and enhance the built and natural environment;

- b. Minimise emissions, ensure efficient use of natural resources and reuse previously developed land wherever possible;
- c. Improve access to jobs, services, education and open space by being located to reduce the need to travel and provide good access to sustainable transport provision.

The proposed development would contribute positively to the formation of a sustainable community, which would be beneficial to the quality of the living environment experienced by existing and future residents. This would be achieved through the development of a high quality and sustainable design that would respond to the on-going need for housing in the local area. The development would secure the re-use of a brownfield site. Appropriate crime reduction and security measures would be capable of being incorporated into the development. Policy SP1 would therefore be positively responded to.

Policy EN1 (Design Principles and Strategic Character Areas) - States that opportunities for good design to enhance the overall image of the City should be fully realised to reinforce and enhance the local character and context of the development site. In this case, it is considered that the composition of the development would be appropriately related to the height and scale of development to the local area. The design has been informed by surrounding roofscapes and the coherent use of materials would give the development an appropriate identity. Policy EN1 would thereby be accorded with.

Policy EN4 (Reducing CO2 Emissions by Enabling Low and Zero Carbon Development) - Has been related to the assessment of the submitted statement detailing measures to reduce CO2 emissions and to secure energy efficiency. The development has been related to 'fabric-led' design approach that would accord with the principles of the energy hierarchy in line within policy EN 4, which is considered that, due to the proposed high quality of energy efficient design, the development would deliver effective measures to reduce carbon emissions that respond to the physical constraints of the site and magnitude of development. On this basis, policy EN4 would be accorded with.

Policy EN 6 (Target Framework for CO2 reductions from low or zero carbon energy supplies) – States that applications for residential development of 10 or more units need to achieve a reduction in carbon emissions that exceed Part L of building regulations. In this case, the submitted environmental standards statement indicates that the achieved level of carbon emissions would significantly exceed Part L of the 2013 building regulations. Policy EN6 would thereby be accorded with.

Policy EN 7 (Energy Infrastructure opportunities) – The proposed development would incorporate air source heat pumps and would thereby meet the policy objective of securing low carbon decentralised energy generation.

Policy EN 8 (Adaptation to Climate Change) - States that all new development will be expected to be adaptable to climate change in terms of the design, layout, siting and function of both buildings and associated external spaces. In this case of this application reference has been given to the adaptability of the development to climate change with reference to:

- i. Minimisation of flood risk by appropriate siting, drainage, and treatment of surface areas to ensure rainwater permeability.
- ii. The need to control overheating of buildings through passive design.
- iii. The opportunity to provide linked and diverse green space to enhance natural habitats, which will assist species adaptation.

As stated, the development has been related to a satisfactory environmental statement that demonstrates measures to reduce carbon emissions. Opportunities for landscaping and biodiversity enhancement would be realised as part of the development. Conditions would secure the delivery of sustainable drainage and its management. Policy EN8 has would thereby be accorded with.

Policy EN9 (Green Infrastructure) - States that new development will be expected to maintain existing green infrastructure in terms of its quantity, quality and multiple function. Where the opportunity arises and in accordance with current Green Infrastructure Strategies, the Council will encourage developers to enhance the quality and quantity of green infrastructure, improve the performance of its functions and create and improve linkages to and between areas of green infrastructure. Where the benefits of a proposed development are considered to outweigh the loss of an existing element of green infrastructure, the developer will be required to demonstrate how this loss will be mitigated in terms of quantity, quality, function and future management. As stated, the applicant has demonstrated that the loss of the open space would not undermine local provision and policy EN9 would be positively responded to.

Policy EN9 has been related to the character, use and quality of the existing green space and the local provision of open and formalised recreational space. The site has been used as an open, recreational space but has not been marked out as sports pitch or used for the siting of play equipment. The land is a brownfield site that has been identified for potential residential development. The locality would continue to be served by Openshaw Park to the south and areas of more informal open and green space to the north. It is therefore considered that future residents would have access to recreational and open space and the development would be suitably related to policy EN9.

Policy EN 10 (Safeguarding Open Space, Sport and Recreation Facilities) - States that the Council will seek to retain and improve existing open spaces, sport and recreation facilities to the standards set out above and provide a network of diverse, multi-functional open spaces. Policy EN10 states that the development of open space will only be permitted where it can be demonstrated that the land does not fulfil and unsatisfied need for such space. Given the amount of retained local open space provision, it is considered that the benefits of the development would outweigh its loss. The development would therefore be appropriately related to policy EN10.

Policy EN 14 (Flood Risk) - States that in line with the risk-based sequential approach, development should be directed away from sites at the greatest risk of flooding and towards sites with little or no risk of flooding. The submitted details and characteristics of the development have been assessed and it is considered that identified additional requirements for the provision of sustainable drainage and its management can be addressed through condition. On this basis, it is considered that policy EN14 can be complied with.

Policy EN15 (Biodiversity and Geological Conservation) – States that developers will be expected to identify and implement reasonable opportunities to enhance, restore or create new biodiversity. Ecological issues affecting the site have been appropriately assessed through the submitted statement and its recommendations relating to biodiversity enhancement and landscaping are capable of being delivered as part of the development.

Policy EN 16 (Air Quality) – Requires improvements to air quality that are sought as part the consideration of development. It also requires that measures are put in place to minimise and mitigate the impact of traffic generation from the development. In this case, the most significant impacts would be experienced during the construction phase, which would be managed through a construction management plan. The level of traffic movement during the operation phase would be limited and impact reduced through the provision of EV charging points and opportunities for the adoption of more sustainable transport modes. Policy EN16 would thereby be responded to positively.

Policy EN18 (Contaminated Land and Ground Stability) - States that any proposal for development of contaminated land must be accompanied by a health risk assessment. This application has been accompanied by a Desk Study and Geo-Environmental Assessment, which has been assessed by Environmental Health whose recommendations have given confidence that identified issues can be addressed through the recommended contaminated land condition thereby securing compliance with policy EN18.

Policy EN19 (Waste) - Requires consideration of the submitted details relating to determine if the applicant has satisfactorily demonstrated how:

- i. Both construction and demolition waste will be minimised and recycled on site wherever possible;
- ii. The sustainable waste management needs of the end user will be met.

Policy EN19 has been related to the proposed waste management arrangements, which are acceptable in terms of capacity. These details are related to the development by condition, including requirements for bin enclosures. It is considered that appropriate waste and recycling management details can be delivered as part of the development to secure compliance with policy EN19.

Policy H1 (Overall Housing Provision) - Relates to the City Council strategy for the delivery of new housing between March 2009 and March 2027. The proposed development responds positively to policy H1 by:

- i. Contributing to the creation of mixed communities by providing house types to meet the needs of a diverse and growing Manchester populations.
- ii. Supporting growth on previously developed sites in sustainable locations.
- iii. Ensuring that the design and density of development contributes to the character of the local area.
- iv. Making appropriate provision for parking cars and appropriate levels of sound insulation.
- v. Being designed to give privacy to its residents and neighbours.

It is considered that the development would be positively related to the quality, supply and appearance of the housing stock in the local area. Appropriate car parking would be provided and residential amenity maintained. Policy H1 would thereby be accorded with.

Policy H 4 East Manchester – States that East Manchester, over the lifetime of the Core Strategy, will accommodate around 30% of new residential development. It also states that priority will be given to family housing. The development would respond positively to policy H4.

Policy H 8 (Affordable Housing) - Policy H8 sets the requirements for affordable housing or an equivalent financial contribution. It is relevant in this case as development would exceed the 15 or more threshold for affordable housing provision. Policy H8 requires that developers use a 20 % target for the incorporation of affordable housing. The applicant proposes that the houses would be available for affordable rent. A condition has been included to ensure that a mechanism for the future maintenance of affordable housing provision following the implementation of the development. On this basis, policy H8 would be accorded with.

Policy H11 (Houses in Multiple Occupation) - Has been related to a condition to ensure that the development is maintained as Class C3 accommodation and to prevent future occupation as small-scale houses in multiple occupation (Class C4). This requirement would ensure that a sustainable supply of family housing and appropriate mix housing types and tenure is maintained in accordance with policy H11.

Policy T1 (Sustainable transport) – Relates to the delivery of sustainable, high quality, integrated transport system, which encourages a modal shift away from car travel to public transport, cycling and walking and prepare for carbon free modes of transport. In this case, the development would be supported with satisfactory car parking provision and arrangements for cycle storage. The site is in a sustainable location with access to local bus routes. The development thereby responds positively to policy T1.

Policy T2 (Accessible areas of opportunity and need) - States that the Council will actively manage the pattern of development to ensure that new development: is located to ensure good access to the City's main economic drivers, including the regional centre and to ensure good national and international connections; is easily accessible by walking, cycling and public transport; connecting residents to jobs, centres, health, leisure, open space and educational opportunities. Given the

sustainable location of the application site, in terms of access to public transport, policy T2 would be accorded with.

Policy DM1 (Development Management) - States that all development should have regard to the following specific issues for which more detailed guidance may be given within a supplementary planning document. Relevant considerations in this case are:

- a. Appropriate siting, layout, scale, form, massing, materials and detail
- b. Impact on the surrounding areas in terms of the design, scale and appearance of the proposed development and its relationship to the character of the surrounding area.
- c. Effects on amenity.
- d. The accessibility to buildings and neighbourhoods, in relation to inclusive access and sustainable transport modes.
- e. Community safety and crime prevention.
- f. Design for health.
- g. Adequacy of internal accommodation and external amenity space.
- h. Refuse storage and collection.

The following assessment demonstrates how the development would accord with policy DM1 points a - h (inclusively). It is considered that the impact of the development on residential amenity would be suitably mitigated and managed through the details of the application and related conditions. Policy DM1 would thereby be accorded with.

Policy PA 1 (Developer Contributions) – States that the nature and scale of any planning obligations will be related to the form of development and its potential impact upon the surrounding area. In this case, the development would deliver affordable housing and its delivery would be addressed by condition. Any financial contribution required to address highways issues would be dealt with separately through any required s38 / s278 agreements.

Unitary Development Plan (saved UDP) Policies -The following Unitary Development Plan saved policies are relevant to the assessment of the development:

Part 2

Policy DC7 (New Housing Development) - States that the Council will negotiate with developers to ensure that new housing is accessible at ground floor level to disabled people, including those who use wheelchairs, wherever this is practicable. Policy DC7 also requires that a satisfactory quality of development is achieved. In this case, satisfactory inclusive access would be achieved and the layout of the proposed houses and the quality of design would accord with residential quality guidance. Policy DC7 would thereby be complied with.

Policy DC26 (Development and noise) - The following elements of policy DC26 are considered to be relevant:

Policy DC26.1 has been related to the proposals contribution to the local noise environment and how existing noise sources, including noise from neighbouring industrial / commercial uses, may impact on the proposed housing.

Policy DC26.4 requires that where an existing noise source might result in an adverse impact upon a proposed new development, or where a new proposal might generate potentially unacceptable levels of noise, consideration is given to measures to deal with it satisfactorily.

Policy DC26.5 has been related to the assessment of the development, in terms of measures to minimise the impact of noise on future residents, including the incorporation of noise insulation.

The development site is in an established residential area, and it is considered that the activity associates with its occupation would be absorbed into the local noise environment. Given the distance of the site from the industrial units and the screening afforded by existing houses, it is considered that any related noise impacts are capable of being addressed through the recommended condition to secure compliance with policy DC26.

Guide to Development in Manchester: Supplementary Planning Document and Planning Guidance - The Guide aims to support and enhance the on-going shaping of the city by providing a set of reasoned principles which will guide developers, designers and residents to the sort of development we all want to see in Manchester. The following paragraphs are of relevance and have been responded to appropriately:

- i. Section 2 Design
- ii. Section 3 Accessibility
- iii. Section 4 Environmental Standards
- iv. Section 7 Housing Density and Mix
- v. Section 8 Community Safety and Crime Prevention

Providing for Housing Choice Supplementary Planning Document (SPD) and Planning Guidance (adopted 2nd September 2008) - This document provides planning guidance about the mix of new housing provision required in Manchester to meet the requirements of the City's planning policies and government guidance about planning policies for housing provision and the delivery of affordable housing. It states that everyone living in Manchester should have the opportunity live in a decent, affordable and accessible home. Furthermore, the range of available housing should support the City's economic growth and develop and sustain neighbourhoods that attract families and workers. In this case, the proposed development would exceed the 15 unit threshold for the provision of affordable housing. The development would provide affordable housing through the delivery of:

Four x 2 bed, 3 person apartments for social rent
 Ten x 2 bed, 3 person houses for affordable rent
 Seven x 3 bed, 4person houses for shared ownership and
 Three x 4 bed, 5 person houses for shared ownership.

The mechanism for maintaining at least 20% of these units as affordable accommodation in perpetuity is set out in the recommended condition. In these circumstances it is considered that the above guidance would be accorded with.

Manchester Residential Quality Guidance (MRQG) - Sets out the direction for the delivery of sustainable neighbourhoods of choice where people will want to live. It also seeks to raise the quality of life across Manchester. To achieve the City's target of carbon neutrality by 2050, residential schemes will also need to incorporate the most appropriate and up to date technologies to significantly reduce emissions. The guidance is therefore relevant to all stages of the development process, including construction and operational management. The proposed house types and apartments would meet the relevant space criteria as set out in the guidance. The quality of the design and layout of the development and quality of design would positively contribute to the character of the local area, encourage sustained long-term occupation and thereby secure compliance with the MRQG.

Manchester's Great Outdoors - A Green and Blue Infrastructure Strategy for Manchester (GI) (2015 -25) adopted July 2015 - The strategy sets out a framework to guide the maintenance of and access to green space and blue space, i.e. waterways, canals and rivers etc., within the city. The strategy builds on the investment to date in the city's green infrastructure (GI) and the understanding of its importance in helping to create a successful city.

The GI strategy recognises the significance of East Manchester to the delivery of new housing. It also acknowledges that East Manchester has the highest proportion of brownfield sites within the city. These sites may provide opportunities for the formation of 'meanwhile' temporary green spaces. However, ultimately many of the brownfield spaces will be rationalised for development and the opportunities for high quality multifunctional green and blue spaces to be delivered as part of attractive new neighbourhoods. The application site has been identified as an amenity green space in the most recent Open Space Audit. However, it shares the characteristics of a 'meanwhile site' and has been identified as a housing capacity site as part of the Strategic Housing Land Availability Assessment (SHLAA). The applicant has provided an open space assessment that identifies open and recreational space in the locality, which include Openshaw Park situated immediately to the south of the site. The development would also deliver garden areas, landscaping and proportionate biodiversity enhancement, which would contribute positively to the quality of the local environment. In these circumstances, it is considered that the proposal would accord with the criteria for the development of open space referenced in the GI strategy.

Positive and proactive engagement with the applicant - An amendment to the DMO, which came into effect on 1st December 2012, requires every decision notice relating to planning permission and reserved matters application to include an explanation as to how the local planning authority have worked with the applicant in a positive and proactive manner based on seeking solutions to problems which arise during the determination of the planning application.

In this case, officers engaged in pre-application discussions with the applicant's agent and thereby established the parameters of the development and identified

pertinent material considerations. These discussions informed the subsequently submitted planning application. Further discussions were undertaken during the consideration of the development to secure appropriate changes to the proposed layout and positive responses to statutory consultation. It is considered that the submitted information has facilitated the appropriate assessment of the development.

Principle of the development – The development would contribute to the delivery and supply of affordable housing through the Project 500 programme. The application site has been previously developed and identified as being suitable for residential development. It is not considered that the development would unduly harm the local supply of open, green and recreational space. The magnitude of the development has been appropriately related to the constraints and configuration of the site. The nature of the proposed use would be acceptable within the context of a residential area. The design of the development has been suitably informed by the scale and character of surrounding housing. The quality of the elevational treatment would positively contribute and enhance the appearance of the streetscene. The internal layout would also encourage sustained occupation. The principle of the development is considered to be acceptable.

Publicity and local consultation – The application was the subject of the required neighbour and statutory consultation, press notices and display of site notices. In addition, the applicant engaged in local pre-application consultation event that provided local residents with an opportunity to view and comment on the proposal. Seven residents provided written comments (six were supportive and one objection was received).

Affordable housing contribution - An affordable housing statement has been submitted that confirms:

i. The development would deliver 24 affordable units in total, comprising:

Four 2 bed apartments for social rent
 Seven 3 bed 4 person houses for shared ownership
 Three 4 bed 5 Person houses for shared ownership
 Ten 2 bed 3 Person houses for affordable rent

ii. The development would be related to 100% funding via a Homes England grant.

iii. The shared ownership products provide the ability to diversify the affordable home ownership offer and attract new residents into an area of close City location. As part of shared ownership residents can purchase an equity share in the property with a portion of rent at 2.75% of unsold equity with an option to staircase in future years.

iv. At least 20% of these homes will be held in perpetuity as affordable units subject to the statutory rights of the future residents.

The above measures give certainty regarding the delivery and future maintenance of affordable housing provision. The affordable housing statement has been linked to the development by a recommended condition.

Magnitude of development – It is considered that the development would provide an appropriate mix of house types and apartments with the density of the new built form being relieved by satisfactorily sized garden areas.

Siting – The proposed houses and apartments would be appropriately related to Brigham Street through the formation a formal building line with a distance of 13 metres maintained between existing and proposed principal elevations. The new access road and footpath would adjoin shallow front garden areas with boundary treatments to satisfactorily demarcate public and private spaces. A minimum distance of 16 metres would be maintained between the principal elevations of the proposed houses presented to the new access road and the rear elevations of houses with frontages to Connie Street. The existing houses to the south of the site do not incorporate extensions at first floor level. A distance of 10.3 metres would be maintained between the nearest existing houses (41 to 43 Meech Street) and the blank elevation to proposed Plot 12. A distance of 15 metres would be maintained between proposed plots 13 and 15 and the nearest existing house (35 Meech Street). These distances would be appropriate given the configuration of proposed windows and arrangement of solid brickwork. Where windows are incorporated into the rear elevations of existing neighbouring ground floor extensions views towards the site would be screened by boundary walls and new fencing. A distance of 12 metres would be maintained across the site between the blank elevations of the proposed units at plots 13 and 15 and windows to plots 11 and 12 thereby ensuring that there would be no undue overlooking. Given the constraints of the site and characteristics of development in the area, it is considered that the proposed layout and siting of individual units would be acceptable.

Height, scale and massing – The proposed development would be limited to two storeys. The housed with frontages to Brigham Street would have a common eaves and ridge line height, the articulation provided through the arrangement of windows and inclusion of textured brickwork panels. Similarly, three pairs of semi-detached houses and the apartment would have common ridge and eaves heights. The juxtaposition of units at plots 23 and 24 would result in a staggering of the ridge line with eaves maintained at a common height. The elevation treatment would be supplemented with the inclusion of contrasting cladding panels to relieve the massing of the brickwork. It is considered that the composition of the development would be proportionate and suitably informed by the scale of neighbouring built form.

Future use of the development - A condition is recommended to ensure that the proposed houses are maintained as single occupancy accommodation and to prevent future occupation as small-scale houses in multiple occupation (Class C4) proposed housing is retained as Class C3 accommodation. This approach would ensure that the development contributes to a sustainable supply of family housing and appropriate mix housing type and tenure.

Permitted development restrictions - In order to maintain and safeguard residential amenity and in response to the constraints of the substantive site, it has been considered appropriate to restrict the permitted development rights that would otherwise be available to householders. Conditions have therefore been recommended in the interests of residential amenity and to ensure that the following

development is not undertaken without the permission of the City Council as local planning authority:

- i. The erection of extensions, porches and outbuildings.
- ii. The replacement of authorised boundary treatments.
- iii. The insertion of additional windows.

Design - The proposed development presents contemporary interpretation of semi-detached housing that has been informed by the characteristics of surrounding built form. The use of light brown brickwork and grey roof tiles throughout the development would secure visual cohesion. Matching brickwork would also be incorporated into the street boundary walls. Brickwork forming the elevations would be relieved by the configuration of windows, feature brick panels and incidental inclusion of dark grey horizontal cladding panels. The proposed design is considered to be acceptable and final details of the materials specifications would be agreed by condition. Details of the composition of the streetscene elevations and boundary treatment is shown at Fig. 6.



Fig.6 – Street elevations and boundary treatment

Inclusive access – The development would provide level thresholds to the front and rear entrances to each of the proposed houses. Suitably wide door sets would be provided. Satisfactory turning areas would be achieved within hallways to facilitate wheelchair access. The larger house Type C would have the potential for future adaptation. Level thresholds would be formed to the proposed apartments, with appropriate turning space in the hallways of the ground floor units. Proposed pavements would secure satisfactory widths and tactile paving has been indicated at crossing points. The proposed access arrangements are considered to be acceptable.

Residential amenity – The quality of proposed accommodation would encourage sustained occupation. Satisfactory amenity space would be proposed with appropriate separation between proposed houses and also, neighbouring properties. Impact of noise would be commensurate with residential occupation and any impacts

for neighbouring noise generating uses would be capable of being addressed by condition.

Residential space standards – All of the proposed units would meet or marginally exceed the relevant space criteria as specified in the Manchester Residential Quality Guidance. The achieved internal space would encourage the sustained occupation of the development.

Impact on local open space provision – The application site has been identified as an amenity green space in the most recent Open Space Audit. However, the application relates to a previously developed brownfield site that has been identified for future residential development. In addition to pockets of green space, the site is located in close proximity to Openshaw Park, which incorporates open spaces and more formal play and sports areas. The loss of the site as open space would be offset by the quality of these facilities in the neighbouring park. Each property would incorporate garden areas, which would be beneficial to residential amenity and would provide appropriate settings to the proposed houses and satisfactory amenity space. The loss of the open space is therefore justified.

Crime and Security – The development would provide satisfactory natural surveillance of the street and boundary treatment would satisfactorily define public and private space and the secure enclosure of garden areas. A condition has been recommended requiring that development meets the requirements of the submitted crime impact statement.

Boundary treatment – The demarcation of the public and private realm would be satisfactorily defined by low walls and railings to an overall height of 900 mm along the street boundaries. Driveway demarcation would be achieved through the positioning of 900 mm railings along the shared boundary. Gate access to rear gardens would be incorporated into 1800 mm close boarded fencing, which would also extend along shared boundaries between individual gardens. The boundary treatment would effectively define the respective plots, enhance the security of the site and reduce the risk of crime. Notwithstanding the above, a condition has been included to ensure the approval of fencing to the northern and southern boundaries. The proposed arrangements would demarcate gardens using existing railing fencing, which would not secure necessary privacy for future residents.

Reducing carbon emissions and sustainable design – A statement has been submitted that relates the development to a 'fabric first approach', i.e., the design would incorporate measures to maximise environmental performance and thermal insulation through use of appropriate components and material as part of the development. The conditioned implementation of the environmental standards statement would provide a series of measures to enhance the environmental performance of the development. Internal fixtures would secure water efficiency and a sustainable drainage system would be incorporated into the development. Air source heat pumps would be provided. Measures to reduce waste and encourage recycling at the construction and occupational phases have been identified (in environmental standards statement and construction and residential waste management plans). Air quality would also be maintained through the implementation of a construction management plan and the provision of EV charging points during

the occupational phase. The site benefits from sustainable transport links, i.e., bus routes to the city centre and local centres. The provision of landscaping would also help to offset carbon dioxide emissions. Based on the above measures, it is predicted that a 46% improvement above Part L of the 2013 Building Regulations would be achieved. This would exceed the reduction in carbon emissions currently required by planning policy and is therefore acceptable.

Car parking and highways issues – There are no traffic regulation orders on the adjacent Brigham Street. There is some on-street car parking on Brigham Street due to the absence of car parking spaces relating to neighbouring terraced houses. However, it is not considered that the siting of driveways relating to the development would impact on the capacity of the neighbouring street to accommodate on-street car parking. The paired arrangement of driveways would reduce disruption of the footway to Brigham Street and the new access road. The development would provide 100% in curtilage car parking for two and three bedroom units and 200% car parking for four bedroom units due to the size of driveways. This level of car parking is considered to be acceptable given the characteristics of the area, access to public transport and provision of cycle storage. The submitted transport assessment incorporates a road safety assessment that indicates that no accident ‘hotspots’ in the study area, including the application site. Trip generation forecasts also indicate a minimal level of traffic activity.

Cycle storage – A condition has been included to ensure the approval of the appearance and specification of the sheds identified within each plot, including appartement. The details will need to confirm that the shed have the capacity to provide cycle storage. On this basis, a minimum of 100% cycle storage would be secured with the capability of providing other ancillary storage.

Off-site highways works – A condition has been included to ensure that all necessary works to the adjacent highways that are related to the development are undertaken prior to the occupation of the development. It is considered that these measures would ensure that the development would unduly harm the operation of the surrounding highway.

Electric vehicle (EV) charging points - The applicant has confirmed that EV charging points to all houses and apartments. A condition has been recommended to ensure that, before the occupation of the development, EV charging points are installed to an approved specification (including charging capacity of 7kw/H) and subsequently maintained in situ.

Construction Management Plan (CMP) - Given the relationship of the site to neighbouring houses, construction work will need to be implemented in accordance with a management plan. A CMP condition has therefore been recommended, which includes measures for dust suppression and wheel washing.

Waste management – The waste and recycling arrangements would be accorded with Council guidance and each dwelling would be supported with:

- 1 No. 240l wheeled bin for refuse
- 1 No. 240l wheeled bin for paper/cardboard recycling

- 1 No. 240l wheeled bin for glass/cans/plastics recycling
- 1 No. 240l wheeled bin for garden/food waste

The following collection arrangements are proposed:

- General Waste (grey bin) - Fortnightly
- Green & Food Waste (green bin/kerbside caddy) - Fortnightly
- Recycling Glass, Plastic, Tins (brown bin) - Fortnightly
- Recycling Paper, Cardboard, Cartons (blue bin/bag) - Fortnightly

The above arrangements have been related to a condition. Bins to proposed houses would be located within rear gardens and screened by fencing and related enclosures would not be necessary. As the bins to the proposed apartments would be located in communal garden areas, the approval of storage enclosure details has been related to a condition that is required to safeguard residential amenity.

Noise – The application site is located in an established residential area and noise generated by the site, including traffic noise, would be proportionate to the magnitude of proposed residential occupation and would thereby have a limited impact on residential amenity. The industrial uses are located approximately 120 metres from the application site and screening is provided by existing housing. However, as a further safeguard to amenity an acoustic attenuation condition is recommended to identify measures that may be required to address noise and disturbance from neighbouring industrial uses.

Air quality – Impacts on air quality would be most noticeable during the construction phase so the development. However, these impacts would be managed through the construction management plan. As stated, that development would generate minimal traffic activity and any impact would be potential reduced through the provision of EV charging points and cycle storage. It is not considered that the development would unduly affect air quality during the operational phase of the development.

Land conditions - The applicant has submitted a ground condition survey that has been assessed and gives assurance that, subject to further information, issues relating to historic land contamination are capable of being addressed. A condition is recommended to ensure that submission of additional details pertaining to the report and to ensure the implementation of an appropriate remediation strategy and all necessary mitigation works.

Flood risk and site drainage - The application site is located within Flood Zone 1 and has low risk of flooding. The applicant has been advised of the conditions requested by Flood Risk Management Team are necessary to ensure the appropriate implementation of the development. It is considered that any additional risk of flooding can be addressed through a satisfactory sustainable drainage scheme related to the development by condition.

Ecology - The development has been related to an ecological assessment

The findings of the ecological assessment are accepted by the GM Ecology Unit, including its recommendation for the provision of the following:

- i. Safeguarding of protected species and habitats;
- ii. Bat and bird boxes;
- iii. Replacement tree planting;
- iv. Incorporation of native planting species within the landscaping scheme.

A condition has been recommended to ensure that the above measures are delivered as part of the development.

Tree removal and landscaping – In order to facilitate the development, including the formation of an access road, seven trees would need to be removed. Five replacement trees have been identified. However, further tree and shrub planting can be delivered through the recommended landscaping condition, including native species. One cherry tree would be retained in the south-west corner of the site in an area of communal open space. Given the configuration of the housing layout, the formation of a small area of open space cannot be avoided. The recommended layout condition therefore included a requirement to ensure its future maintenance. The submitted arrangements for the protection of the retained tree have also been related to the development by condition.

Local labour agreement - A specific local labour agreement condition has not been included in this case. Instead, contributions toward social value, including local labour opportunities, would be co-ordinated across the Project 500 programme and delivered through the development contract.

Broadband connectivity – The applicant has confirmed that the site would have access to local broadband infrastructure.

Conclusion – The development would secure the residential use of a previously developed parcel of land. It is considered that a satisfactory quality of design and the proposed internal space would meet the Council's space criteria. Any potentially harmful impact of the development on residential amenity has been either addressed through the submitted details or is capable of being addressed by condition. The development would positively contribute to the supply of affordable family housing in the local area and the loss of open space is justified due to local access to Openshaw Park. The proposed development is therefore considered to be acceptable.

Other Legislative Requirements

Equality Act 2010

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation Approve

Article 35 Declaration

In this case, officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Officers engaged in pre-application discussions with the applicant's agent and thereby established the parameters of the development and identified pertinent material considerations. These discussions informed the subsequently submitted planning application. Further discussions were undertaken during the consideration of the development to secure appropriate changes to the proposed layout and positive responses to statutory consultation. It is considered that the submitted information has facilitated the appropriate assessment of the development.

Condition(s) to be attached to decision for approval

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents stamped as received by the City Council as local planning authority on 6 February 2023, 17 February 2023 and 15 May 2023:

Planning application forms

Location Plan Ref: 20_051/01

Proposed Site Plan Ref: 20_051/02 Rev A

Roof plan Ref: 20_051/03 Rev A
 House Type A Ref: 20_051/20 Rev A
 House Type B Ref: 20_051/21 Rev A
 House Type C Ref: 20_051/22 Rev A
 Flat Types D1 and D2 Ref: 20_051/23 Rev A
 Proposed External Materials Ref: 20_051/25 Rev A
 Vehicle Tracking Ref: 20_051/10
 Boundary Treatment Details Ref: 20_051/30
 Brigham Street Development Openshaw (Part of MCC's Project 500 Phase 1) Design and Access Statement produced by Triangle Architects Ltd on behalf of One Manchester January 2023
 Brigham Street Development Openshaw Amenity Space and Recreational Provision Statement by Triangle Architects received 1 June 2023
 Brigham Street, Openshaw Transport Statement Ref: 230523/SK22337/TS01(-00) dated 23 May 2023 by SK Transport Planning Ltd
 Building for a Healthy Life Assessment (June 2020 Edition) Job Title: Brigham Street, Openshaw by Triangle Architects
 One Manchester Proposed Development at Brigham Street, Openshaw, Manchester, M11 2JJ Utility Feasibility Report Revision V0 by TDS relating to broadband connectivity

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester.

3) Before the commencement of above ground construction works, samples and specifications of all materials to be used on all external elevations of the development have been submitted to and approved in writing by the City Council as local planning authority. The development shall be fully implemented in accordance with the approved details, which shall be maintained in situ thereafter.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy for the City of Manchester.

4) The authorised development relates to the erection of: 10 x two bedroom, three person; 7 x three bedroom, four person; 3 x four bedroom, five person and 4 x two bedroom, three person (24 units in total (Class C3)).

Reason - To safeguard the amenities of the occupiers of occupiers of nearby residential properties pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester and saved policy DC26 of the Unitary Development Plan.

5) Upon occupation of the development, the detail of the One Manchester Affordable Housing Statement for Brigham Street stamped as received by the City Council as local planning authority on 16 June 2023 (and detailing the provision of a total of 24 affordable units consisting of Four, 2 bed apartments for Social Rent, Seven, 3 bed 4 person houses for Shared Ownership, Three, 4 bed 5 Person houses for Shared

Ownership and Ten, 2 bed 3 Person houses for Affordable Rent shall be fully implemented and maintained in situ at all times thereafter.

Reason - In order to provide affordable housing at the site in accordance with policy H8 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

6) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no part of the residential units shall be used for any other purpose (including any other purpose in Class C3 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 as amended by The Town and Country Planning (Use Classes) (Amendment) (England) Order 2015, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification) other than the purpose(s) of C3(a) not precluding occupation by two unrelated people sharing a property.

Reason - In the exceptional circumstances of a proliferation of HMOs restricting housing choice and adversely affecting sustainability and in the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through the provision of accommodation that is suitable for people living as families pursuant to paragraph 7.4 of the Guide to Development in Manchester: Supplementary Planning Document and Planning Guidance, the National Planning Policy Framework and policies SP1 and DM1 of the Core Strategy for the City of Manchester.

7) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no garages, extensions, porches or outbuildings shall be erected other than those expressly authorised by this permission.

Reason - In the interests of residential amenity pursuant to policy SP1 and DM1 of the Core Strategy for the City of Manchester.

8) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no windows shall be inserted into the elevations of the dwellinghouses hereby approved other than those shown on the approved drawings referenced: Proposed Site Plan Ref: 20_051/02 Rev A; Roof plan Ref: 20_051/03 Rev A; House Type A Ref: 20_051/20 Rev A; House Type B Ref: 20_051/21 Rev A; House Type C Ref: 20_051/22 Rev A and Flat Types D1 and D2 Ref: 20_051/23 Rev A.

Reason - In the interests of residential amenity pursuant to policy SP1 and DM1 of the Core Strategy for the City of Manchester.

9) Before the occupation of the development windows to bathrooms / wet rooms as so indicated on the approved drawings ref: Proposed Site Plan Ref: 20_051/02 Rev A; Roof plan Ref: 20_051/03 Rev A; House Type A Ref: 20_051/20 Rev A; House Type B Ref: 20_051/21 Rev A; House Type C Ref: 20_051/22 Rev A and Flat Types

D1 and D2 Ref: 20_051/23 Rev A shall be obscure glazed to a specification of no less than level 5 of the Pilkington Glass Scale or such other alternative equivalent and shall remain so in perpetuity.

Reason - To protect the amenity and living conditions of adjacent residential property from overlooking or perceived overlooking and in accordance with policies SP1 and DM1 of the Core Strategy for the City of Manchester.

10) Prior to the commencement of the development hereby approved, a detailed construction management plan outlining working practices during the construction phase of the development shall be submitted to and approved in writing by the local planning authority, which for the avoidance of doubt should include:

- i. Display of an emergency contact number;
- ii. Details of wheel washing;
- iii. Compound locations where relevant;
- iv. Location, removal and recycling of waste;
- v. Routing strategy and swept path analysis;
- vi. Parking of construction vehicles and staff;
- vii. Sheeting over of construction vehicles.
- vii. Parking of construction vehicles and staff;
- viii. Dust suppression measures.

Development shall be carried out in accordance with the approved construction management plan upon commencement of authorised works.

Reason - To safeguard the amenities of nearby residents and highway safety, pursuant to policies SP1, EN19 and DM1 of the Core Strategy for the City of Manchester.

11) Before the occupation of the development, a scheme shall be submitted to and approved in writing by the City Council as local planning authority confirming the specification of the electric vehicle (EV) charging points as shown drawing referenced: Proposed Site Plan Ref: 20_051/02 Rev A. The EV charging points shall achieve a minimum charging specification of 7kw/H and be installed in accordance with positions shown on the approved drawings before first occupation of the development and maintained in situ at all times thereafter.

Reason - In the interests of residential amenity and to contribute to the reduction of carbon emissions and improve air quality, pursuant to policies SP1, EN16 and DM1 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

12) The authorised development shall be undertaken in accordance with the waste management strategy comprising drawing and documents referenced:

Proposed Site Plan Ref: 20_051/02 Rev A;
Brigham Street Development. Openshaw, Waste Management Strategy, Brigham Street Development, Openshaw – Waste Management Strategy by Triangle Architects received 17 February 2023;

Waste Management Proforma Ref: 136171/FO/2023 dated 6 February 2023.

The agreed arrangements shall be implemented in full prior to the first occupation of the development and maintained in situ thereafter.

Reason - In the interests of residential amenity and to secure appropriate arrangements for the storage and collection of segregated waste and recycling, pursuant to policies SP1, EN19 and DM1 of the Core Strategy for the City of Manchester.

13) Before the first occupation of the development details of the elevational appearance of the bin storage enclosures relating proposed apartments (at Plots 23, 14 15 and 16) in the positions shown on drawing ref: Proposed Site Plan Ref: 20_051/02 Rev A shall be submitted to and approved in writing by the City Council as local planning authority. The approved details shall be implemented in full prior to the first occupation of the development and maintained in situ thereafter.

Reason - In the interests of residential amenity and to secure appropriate arrangements for the storage and collection of segregated waste and recycling, pursuant to policies SP1, EN19 and DM1 of the Core Strategy for the City of Manchester.

14) Before the first occupation of the development details of the elevational appearance of the gardens details as shown on drawing ref: Proposed Site Plan Ref: 20_051/02 Rev A, including confirmation of facilities for secure cycle storage with the sheds, shall be submitted to and approved in writing by the City Council as local planning authority. The approved details shall be implemented in full prior to the first occupation of the development and maintained in situ thereafter.

Reason - To ensure that adequate provision is made for cycle parking so that persons occupying or visiting the development have a range of options in relation to mode of transport in order to comply with policies SP1, T1 and DM1 of the Core Strategy for the City of Manchester.

15) Before occupation of the development, a scheme shall be submitted to and approved in writing by the City Council as local planning authority relating to the acoustic insulation of the residential accommodation against noise from industrial uses to the west of the application site. The approved noise insulation scheme shall be implemented before the development is occupied.

b) Prior to first occupation of the residential units, a verification report shall be required to validate that the work undertaken throughout the development conforms to the recommendations and requirements in the approved acoustic consultant's report. The report shall also undertake post completion testing to confirm that the internal noise criteria have been met. Any instances of non-conformity with the recommendations in the report shall be detailed along with any measures required to ensure compliance with the internal noise criteria.

Reason - To secure a reduction in noise from industrial uses to the west of the application site and in order to protect future residents from noise nuisance, pursuant to policies SP1, H1 and DM1 of the Core Strategy for the City of Manchester.

16) a) Before the development hereby approved commences, a report (the Preliminary Risk Assessment) to identify and evaluate all potential sources and impacts of any ground contamination, groundwater contamination and/or ground gas relevant to the site shall be submitted to and approved in writing by the City Council as local planning authority. The Preliminary Risk Assessment shall conform to City Council's current guidance document (Planning Guidance in Relation to Ground Contamination).

In the event of the Preliminary Risk Assessment identifying risks which in the written opinion of the Local Planning Authority require further investigation, the development shall not commence until a scheme for the investigation of the site and the identification of remediation measures (the Site Investigation Proposal) has been submitted to and approved in writing by the City Council as local planning authority.

The measures for investigating the site identified in the Site Investigation Proposal shall be carried out, before the development commences and a report prepared outlining what measures, if any, are required to remediate the land (the Site Investigation Report and/or Remediation Strategy) which shall be submitted to and approved in writing by the City Council as local planning authority.

b) When the development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the City Council as local planning authority.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Core Strategy for the City of Manchester.

17) No development shall take place until surface water drainage works, designed in accordance with Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards, have been submitted to and approved in writing by the Local Planning Authority.

Reason: To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution, pursuant to policies EN8 and EN14 of the

Core Strategy for the City of Manchester and the guidance within the National Planning Policy Framework.

18) Before the occupation of the development, details of the implementation, maintenance and management of the sustainable drainage scheme shall be submitted to and approved by the City Council as local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details at all times thereafter. Those details shall include:

- i. Verification report providing photographic evidence of construction as per design drawings;
- ii. As built construction drawings if different from design construction drawings;
- iii. Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

Reason: To manage flooding and pollution and to ensure that a managing body is in place for the sustainable drainage system and there is funding and maintenance mechanism for the lifetime of the development, pursuant to policies EN8 and EN14 of the Core Strategy for the City of Manchester and the guidance within the National Planning Policy Framework.

19) Before the occupation of the building, the car parking spaces and other hard surfaces relating to each plot shall be implemented in accordance with the details of drawing referenced: Proposed Site Plan Ref: 20_051/02 Rev A. Upon occupation of the building, the car parking area shall be made available for use in accordance with the authorised use only and maintained in situ thereafter.

Reason - In the interest of pedestrian and highways safety and to ensure the provision of appropriate car parking pursuant to policies SP1, T1 and DM1 of the Core Strategy for the City of Manchester.

20) Before the occupation of the authorised development, full details and specifications of all off-site highways works, including footpath reinstatement to Meech Street, Staton Street and Brigham Street, shall be submitted to and approved in writing by the City Council as local planning authority. The required works shall be fully implemented prior to the first occupation of any part of the development and maintained in situ thereafter.

Reason - In the interest of pedestrian and highway safety and in the interests of local amenity, as specified in policies SP1, T2 and DM1 of the Core Strategy for the City of Manchester and guidance contained within the National Planning Policy Framework.

21) The development hereby approved shall be implemented in full accordance with the measures as set out in document referenced: One Manchester Environmental Standards Statement Brigham Street, Openshaw, Manchester by Watt Energy and Consulting Engineers dated 26 April 2022, including: measures to secure predicted

carbon emissions and the attainment of specified environmental efficiency and performance.

Within 3 months of the completion of the construction of the development a verification statement shall be submitted to and approved in writing, by the City Council as local planning authority, confirming the incorporation of the specified measures at each phase of the construction of the development, including dated photographic documentary evidence of the implementation and completion of required works.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy for the City of Manchester and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

22) The authorised development shall be designed and constructed in accordance with the recommendations contained within sections 3.3 and 4 of document referenced: Crime Impact Statement: Brigham Street 20 No. Houses and 4 No. Apartments for One Manchester Version A: 17th October 2022 Reference: 2022/0453/CIS/01 by GM Police Design for Security. The development shall only be carried out in accordance with these approved details and occupation shall not commence until the City Council as local planning authority has acknowledged, in writing, that it has received written confirmation of 'Secured by Design' accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1, EN1 and DM1 of the Core Strategy for Manchester and to reflect the guidance contained in the National Planning Policy Framework.

23) Before the occupation of any part of the development the details of document referenced: Preliminary Ecological Appraisal Land at Brigham Street, Manchester, M11 2JH dated November 2022 Ref: P.1592.22 (Section 5.0 Evaluation and Recommendations) by Ascerta shall be supplemented by additional details relating to the provision and position of bat and bird boxes, which shall be submitted to and approved in writing by the City Council as local planning authority. The development shall be undertaken in accordance with the approved details before the occupation of the apartments and maintained in situ thereafter.

Reason - In order to enhance local biodiversity through the provision of wildlife habitats, pursuant to policy EN15 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

24) The development shall be implemented in accordance with the Arboricultural Impact Assessment Land at Brigham Street Manchester M11 2JH Ref: P.1592.22 dated October 2022, including arrangements for protection of tree during construction (6.0 Tree Protection Measures) and the retention of one Cherry tree (G1) as identified on drawings referenced P.1592.22.02. The retained tree (G1) shall not be removed without the prior written consent of the City Council as local planning authority.

Reason - In order to avoid damage to any retained trees adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy for the City of Manchester.

25) Before the occupation of the authorised development and notwithstanding the details of drawing referenced: Proposed Site Plan Ref: 20_051/02 Rev A, a scheme shall be submitted to and approved in writing by the City Council as local planning authority, detailing the inclusion of native tree and shrub planting within the site and arrangements for the future maintenance of communal hard and soft landscaping within external communal areas. The approved landscaping scheme shall be fully implemented during the first full planting scheme following the occupation of the development and maintained in situ thereafter in accordance with the approved landscape maintenance scheme. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that the development incorporates a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Core Strategy for the City of Manchester.

26) No removal or other work to trees and or hedges shall be undertaken in the main bird breeding season (March to July inclusive), unless nesting birds are found to be absent, by a suitably qualified person.

Reason - To safeguard local biodiversity and nature conservation pursuant to policy EN15 of the Core Strategy for the City of Manchester.

27) Before the commencement of the development, a scheme shall be submitted to and approved in writing by the City Council as local planning authority detailing the measures for safeguarding protected species throughout the construction phase as set out in document referenced: Preliminary Ecological Appraisal Land at Brigham Street, Manchester, M11 2JH dated November 2022 Ref: P.1592.22 (Section 5.0 Evaluation and Recommendations) by Ascerta. The approved scheme shall be implemented upon commencement of works and throughout the construction phase.

Reason - To safeguard local biodiversity and nature conservation pursuant to policy EN15 of the Core Strategy for the City of Manchester.

28) The development shall be implemented in accordance with the measures for managing the spread of invasive species within the site as set out in document referenced: Preliminary Ecological Appraisal Land at Brigham Street, Manchester, M11 2JH dated November 2022 Ref: P.1592.22 (Section 5.0 Evaluation and Recommendations) by Ascerta stamped as received by the City Council as local planning authority on 6 February 2023.

Reason - To prevent the spread of an invasive species (Cotoneaster), which has been identified on part of the application site and in the interests of residential amenity pursuant to policies SP1, EN18 and DM1 of the Core Strategy for the City of Manchester and The National Planning Policy Framework.

29) The development shall be implemented in accordance with document referenced: One Manchester Proposed Development at Brigham Street, Openshaw, Manchester, M11 2JJ Utility Feasibility Report Revision V0 by TDS relating to broadband connectivity (Section 5.4 Telecoms).

Reason – In the interests of residential amenity pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester and The National Planning Policy Framework.

30) Notwithstanding the detail of drawing referenced Boundary Treatment Details Ref: 20_051/30 and before the occupation of the development, details of fencing along the northern and southern boundaries shall be submitted to and approved in writing by the City Council as local planning authority. The approved details shall be fully implemented along with all other boundary treatments specified on drawing referenced Boundary Treatment Details Ref: 20_051/30 upon first occupation of the development and maintained in situ thereafter.

Reason – In the interests of residential amenity pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester and The National Planning Policy Framework.

31) The development shall be implemented in accordance with the Unexploded Ordnance Risk Assessment as set out in Section 5.0 (page 8) of document referenced Openshaw West Site Phase 1 Preliminary Risk Assessment by Curtins Ref: 077873-CUR-00-XX-RP-GE-032 Revision: V01 Issue Date: 28 April 2021.

Reason – In the interests of public safety and residential amenity pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 136171/FO/2023 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

Highway Services
 Environmental Health
 Neighbourhood Team Leader (Arboriculture)
 MCC Flood Risk Management
 Greater Manchester Police

United Utilities Water PLC

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Highway Services
Environmental Health
Neighbourhood Team Leader (Arboriculture)
Greater Manchester Police
Greater Manchester Ecology Unit

Relevant Contact Officer : Carl Glennon
Telephone number : 0161 234 4530
Email : carl.glennon@manchester.gov.uk

